

Agenda
Village of Homer Glen
PLAN COMMISSION
Thursday, January 15, 2026 – 7:00 p.m.
Village Board Room, 14240 W. 151st Street, Homer Glen

1. Call to Order.

2. Pledge of Allegiance to the Flag.

3. Roll Call.

4. Amendments to the Agenda.

5. Approval of the Agenda.

6. Minutes.

- a) December 18, 2025

7. Public Comment.

8. New Business and Possible Action.

- a) **Case No. HG-2526-SP, Runnings Outdoor Storage (Public Hearing):** A request for approval of an amendment to a Special Use Permit for Outdoor sales and storage in the C-3 General Business District at 15830 S. Bell Road, Homer Glen, Illinois. (PIN: 16-05-14-400-014-0000)

- b) **Case No. HG-2530-PUD, 13812 & 13830 S. Bell Road – Car Wash, Gas Station, Restaurant, Convenience Store (Continued Public Hearing):** A request for approval of [1] a Special Use Permit for a Major Change to the PUD for Menards Subdivision with requested exceptions, [2] a Special Use Permit for a car wash (classified as an automobile repair, service and body shop use), [3] a Special Use Permit for a drive-through establishment, [4] a Special Use Permit for an automobile service station (gas station), [5] a Special Use Permit for 24-hour operation; and [6] Site Improvement Plans for a unified development for certain real property located in the C-2 Local Business District at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois. (PINs: 16-5-02-213-002-0000, 16-05-02-213-005-0000, 16-05-02-213-004-0000)

9. Village Board & Committee Updates.

10. Other Business.

11. Attendance Confirmation.

- a) Attendance Confirmation for February 5, 2026 PC Meeting

12. Adjournment.

DISABLED: Any individual requiring special accommodations as specified by the Americans with Disabilities Act is requested to notify the Village Manager of Homer Glen at 708-301-0632 at least 24 hours in advance of the meeting date.

RECORDING MEETINGS: The Public May Record All Meetings Required to Be Open under the Open Meetings Act, Subject to Reasonable Rules Approved by the Village.

Plan Commission

Minutes of the Meeting on
December 18, 2025

DRAFT

Village of Homer Glen
14240 W 151st Street, Homer Glen, IL 60491
Community Room

1. Call to Order.

The meeting was called to order at 7:03 pm by Chairman Hand.

2. Pledge of Allegiance to the Flag. The Pledge was led by Commissioner Marshall.**3. Roll Call.**

Members present: Commissioner Foley, Chairman Hand, Commissioner Foley, Commissioner Marshall, Commissioner Stanly, Commissioner Mesaf, and Vice Chair McGary

Also present were Planning Director Gruba, Planner Udarbe, and Assistant Planner Kahn.

Commissioner Mesaf arrived at 7:05 pm.

4. Approval of Amendments to the Agenda

None.

5. Approval of the Agenda

Not called because there were no changes to the agenda.

6. Minutes.

- a) **December 4, 2025:** Commissioner Stanly made a motion to approve the minutes from December 4, 2025, which was seconded by Commissioner Marshall. A voice vote was taken, all were in favor, zero (0) opposed and the motion carried.

7. Public Comment.

Commissioner Hand asked if there was anyone that wanted to speak on anything that was not on the agenda. There were none.

8. New Business and Possible Action

- a) **Case No. HG-2530-PUD, 13812 & 13830 S. Bell Road – Car Wash, Gas Station, Restaurant, Convenience Store (Public Hearing):** A request for approval of [1] Major Change to the PUD for Menards Subdivision with requested exceptions, [2] Special Use Permits for a car wash (auto service), a drive-through, and an automobile service station (gas station), and [3] Site Improvement Plans for certain real property located in the C-2 Local Business District at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois. (PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000)

Planner Kahn explained that this case is for new buildings for a car wash, gas station, restaurant, and convenience store within the Menards PUD. However, staff recommended tabling the public hearing to the Plan Commission meeting on January 15, 2026, because it was discovered that there were some inaccuracies in the published public hearing notice.

Director Gruba mentioned that this case was approved in 2023, nothing was constructed, and the PUD expired in April of this year. The applicant is applying for the same PUD request. The public hearing did not include all the correct information including the legal description. The notice did not include the (remnant of) Lot 4 where a monument sign is proposed along with light poles, even though there will not be buildings constructed on this lot. The applicant wanted to request hours of operation outside of normal business hours (6 am until 11 pm). Besides the car wash,

the other uses would be open outside of normal business hours which means they need to request an additional Special Use Permit which wasn't included in the public hearing notice.

Director Gruba stated that for the protection of the Village and the applicant, staff recommends that the Plan Commission keep the public hearing open and continue it at the January 15, 2026 meeting.

Even when the project was originally approved in 2022 under ORD 22-038 (amended in 2023), the plans were changing. Director Gruba and Planner Kahn were trying to figure out the latest plans. Staff wants to make sure that the correct exhibits are brought to the public hearing.

Chairman Hand asked for clarification on the item title. Director Gruba said that the item title and motions will be updated for the January 15, 2026 meeting. Planner Kahn mentioned that the 2023 staff report did not list the exact times that the businesses were open. She wanted clarity from the applicant on the hours of operation to determine whether or not a Special Use Permit would be applicable; this will be reflected at the January 15 public hearing.

Chairman Hand asked if the owners intend to be open 24 hours. Planner Kahn mentioned that every business but the car wash (gas station, convenience store, and restaurant) intends to be open outside of the normal business hours. Planner Kahn mentioned that the 2023 staff report stated that the businesses could potentially be open for 24 hours but not provide specific times.

Commissioner Stanly made a motion to open the public hearing, seconded by Commissioner McGary. A voice vote was taken, all voting in favor, zero (0) opposed. The public hearing was opened.

Petitioner Comment:

Mark Daniel, the applicant, mentioned that he brought the plan sheets for anyone to review. He is representing NSS group with Daniel Law. Mr. Daniel mentioned that the original PUD materials [from the early 2000s] can't be found. There is an existing sign easement since around 2004 so we will re-notice to be clear that there will be a sign on that adjacent parcel. Mr. Daniel mentioned that the C-store and gas station will be open 24 hours. He mentioned that the restaurant will likely not be open 24 hours. Mr. Daniel said that they submitted the plans that they thought were approved to avoid having the Village Engineer and consultant re-review the plans. They had approved engineering but they didn't get the building permitted. Mr. Daniel said that he believed the Photometric Plans were compliant. Mr. Daniel asked to continue this public hearing to the January 15, 2026 and that he will be available then.

Chairman Hand asked if there is any public comment. There was none.

Plan Commission Discussion:

No commissioners had questions.

Commissioner Stanly made a motion to keep open and continue the public hearing until January 15, 2026, seconded by Commissioner McGary. A voice vote was taken, all voting in favor, zero (0) opposed.

The public hearing will be kept open until the January 15, 2026 Plan Commission Meeting to continue the Public Hearing for case number HG-2530-PUD.

9. Village Board and Committee Updates

Director Gruba mentioned that the Board approved the Walden Road variance for impervious surface. The Board also approved a Minor Change to the PUD for Goodings Grove townhomes for larger decks and patios; they are already pouring the foundation for Lot 25. They want to get the model homes open by the Spring. The Board also approved 10 minor text amendments to the Zoning Ordinance. Staff intends to bring more text amendments to the Plan Commission in the near future.

10. Other Business

- a) **Case No. HG-2519-ASPV, 151st Street & Bell Rd – Dunkin’ (Workshop):** Discussion on concept plans for [1] a Preliminary Plat of Subdivision; [2] a Map Amendment from the A-1 Agriculture District to C-2 Local Business District; [3] Special Use Permit for a Drive-through establishment associated with a permitted use; [4] Special Use Permit to operate outside of normal nonresidential operating hours; [5] Site improvement plans, and; [6] Sign Variances for certain real property located in the A-1 Agriculture District (PINs: 16-05-11-400-026-0000 & 16-05-11-400-017-0000).

Planner Udarbe listed the 6 zoning requests for the project and explained this is a proposal for a relocation of the Dunkin’ located in the Speedway on 151st and Bell Road into a new 965sf standalone building adjacent to the Speedway on Bell Road. This will be a drive-through use only. The purpose of the workshop is to review the proposed plans, provide comments, and ask questions, and no public comment has been received since it hasn’t been officially noticed.

Planner Udarbe went through the lots that are vacant in the agriculture zone. They are requesting a map amendment to the C-2 Local Business District. The Comprehensive Plan designates this as commercial with environmental features. This is a densely forested lot with a wetland buffer. There is a shared access drive currently in place.

Both existing lots currently don’t meet lot requirements for the A-1 district. There are two proposed lots (lot 1 and lot 2). They are requesting a map amendment from A-1 to C-2. They are requesting 2 Special Use Permits for a drive-through and operating outside of normal business hours.

Planner Udarbe showed the proposed site plan and updated renderings that the Dunkin’ team submitted on December 16, 2025. They are proposing a ~965sf building. For the staff review, the primary building conforms with the C-2 District. The updated December 16 plans increased the parapet height, but this still meets code requirements.

For traffic and pedestrian circulation, the subject property is undeveloped but has an existing cross-access easement to utilize access to Bell Road in the SE corner. Per the traffic study, there are no roadway improvements or traffic control adjustments that would be required. Per updated Will County DOT and Village agreements, the sidewalk will not be installed in the Bell Road ROW even though it’s on the Dunkin’ site plan. There are minimal parking requirements because there are no seats within the building. They have 8 spaces including 1 ADA space where only 5 spaces are required. Director Gruba mentioned that the public is not allowed in the building since this would only be for employees. Planner Udarbe continued, noting that the parking spaces on a 45° angle and trash enclosure encroach in the 25’ landscape buffer. The proposed landscape islands do not meet the correct width for the code requirements. The applicant can either request a variance or revise the plans. The Board may also consider an alternate to traditional landscape islands like the rain garden.

For the landscape plan, they are proposing turf grass in the wetland buffer but staff will later recommend plants that are more appropriate for a wetland. For parkway tree planting requirements, they would either plant trees in the right-of-way of Bell Road (a county road) or the wetland buffer. The preserved trees may count towards to the parkway tree planting requirements.

The applicant also proposes foundation plantings and a rain garden that go beyond our code. An Effective Screen Planting Easement, a 15' easement between residential and commercial, does not apply here because they don't directly abut a residential district. We don't have quantity standards for the landscaping requirements; staff will need to do full review of the landscape plan to determine compliance.

The PowerPoint shows the updated December 16 signage plans. Elevations match the 3-D renderings. The sign package proposes 1 new ground sign, 4 wall signs, directional signs, and other accessory signs. The requested sign variances are to allow for the monument sign to encroach in the clear vision area and to allow 2 wall signs in the south elevation where only 1 is permitted. Non-conformities will require revisions or additional variance requests to clarify signage plan, the sign on the north side does not front a road or private circulation, no masonry provided around the monument sign base, and remove advertisement logo on directional sign.

Ancillary canopy signs need revisions or variances to remove an "order here" and the blade projecting sign on the drive through clearance bar.

The lighting plan (updated on 12/16/2025) is still non-compliant. The north and west property lines exceed the permitted 0.1 max foot candle. The south property line exceeds 0.5 from commercial to commercial. The drive-in and drive-through canopy exceeds the maximum 15-foot candles. This may change based on further review. The Village requires full compliance with photometrics.

For the exterior materials, the new rendering still doesn't meet code requirements for the area of the first floor. The parapet height increased but still meets code.

Tree preservation applies here because of the subdivision request. The tree survey contains ~1,352 total tree includes on the Dunkin' site. Planner Udarbe showed a marked up tree removal and tree preservation plan. They are proposing to remove 62% of healthy, protected trees. There are 3 protected trees that would be removed (these are healthy, non-exempt trees over 24 DBH); they total 81" caliper inches of protected trees. Either they have to mitigate by planting around 65 trees on site or pay \$20,250 for restitution. Planner Udarbe mentioned that she is concerned about the proximity of some of the healthy trees to the proposed development. The arborist is recommending removal of invasive species as part of this project.

Stormwater Management does apply. They likely will encroach the 75' wetland buffer which requires variance from Board.

Applicant Comment

Philip Wolf, the applicant, is the engineer for the project. Randy Bafia (Keystone), John Gallagher (the property owner), and Bill Dale (Dunkin') were also present.

Four or five months ago, preliminary stormwater was approved by Brett Westcott. The site is very wooded. Phil Wolf mentioned that the buildable area is north of where the parking spaces are located. They have tried to minimize the impact of tree removal. They added a rain garden because it's a sensitive area that will store rain and remove pollutants.

Questions from Commissioners

Commissioner Stanly asked the owner about the difference between a drive-through and sit-down Dunkin'. Bill Dale, the Operations Officer for the Franchisee, said Dunkin' has been (post-COVID) trying to minimize the footprint of the business while still giving something to the community. They don't think that the drive-through will have a negative impact on the business.

Commissioner Stanly asked what type of trucks would be used for deliveries, if this would be after hours, and the size of the truck. Bill Dale said that they would be typical semi-truck for deliveries. The time of delivery depends on the coordinated schedule of the distribution center. They try to stay away from busy hours from 6-10am.

Commissioner Marshall asked if the 14 drive-through spots in the queue would be adequate. The applicant said that that's adequate.

Commissioner Marshall asked if the property would be sold or leased to Dunkin'. John Gallagher said that lot 1 will be sold to Dunkin' Franchise. Lot 2 will maintain ownership so there is still access off of Bell Road.

Commissioner Marshall asked if the proposed Dunkin' will have similar hours of operation as the new Dunkin' on 159th Street near Ace Hardware. Planner Udarbe said that she believed that the hours will be similar, both opening at 5am.

Commissioner Marshall asked about the variance for exterior materials. He asked if the cement panels are brick and pressed so they look like a brick façade but they are not. Planner Udarbe mentioned that there are specific standards for concrete panels with the Building Department. Randy Bafia, the applicant with Keystone, mentioned that these are fiber cement panels, and the rest of the siding is wood grain which is Dunkin' standard custom design. Planner Udarbe mentioned that this is a building code requirement. The Building Manager reviewed the materials and mentioned that they did not comply with Building Codes; he also mentioned that fiber cement is the most common material that comes before the Village Board. Ultimately, building material variances are up to the Board.

Chairman Hand asked if the elevations are similar to the Dunkin' on 159th Street. Planner Udarbe mentioned that the other Dunkin' has glass in the front since it is a walk-in building.

Chairman Hand asked whether or not the door is glass. Randy Bafia mentioned that the door is not glass because it is for employees only. Chairman Hand thought that the door should be glass and that there wouldn't be much to see inside because it's a kitchen and workspace.

Planner Udarbe mentioned that the Dunkin' on 159th Street is primarily masonry, brick and glass with fiber cement accents. Planner Udarbe said that she can bring in the renderings from the other Dunkin' to the next public meeting since Chairman Hand wants to see a comparison of the two buildings.

Commissioner Marshall asked if all the signage will be illuminated. It seems that they all will be illuminated.

Commissioner Marshall asked about the requirements of landscape islands. Our code requires a mix of landscape material and curbs. Commissioner McGary asked if there's a reason for one island being 6' wide and the other being 9' wide, then that would make sense to construct it that way. Director Gruba mentioned that the narrowness of the islands is in our Village Code

Commissioner Marshall asked if there are any other special considerations, other than wetlands in the north, such as run-off. Planner Udarbe mentioned that we have preliminary engineering, but we still need a full review with staff and consultants. The applicant's engineer, Philip Wolf, mentioned that all run-off from the site goes to the rain garden first which then drains to the detention basin which outlets to the southwest. They aren't running anything back to the wetland.

Commissioner Marshall asked if the site is improved (utilities, wastewater, etc.). Philip Wolf said that the existing water and sewer is in the ROW.

Chairman Hand asked about the water and sewer and how the wetlands would be impacted. Based on Brett Westcott's comments, they need to see if there's an impact the wetland buffer; this would require a variance from the Board.

Chairman Hand asked if the building would be visible when people are driving on Bell Road. Randy Bafia mentioned that that is why they wanted a sign variance for the ground sign. Director Gruba pointed out that all the dead and invasive trees would be removed, so only 4 mature trees would remain. Director Gruba thought that the building would be more visible after the tree removal. Chairman Hand mentioned that he'd prefer to see an updated rendering.

Chairman Hand asked if there will be a left turn lane on Bell Road after construction. Vice Chair McGary asked if customers will be able to turn north out of the Dunkin'. Planner Udarbe will look at Bell Road widening plans on the Will County plans.

Chairman Hand asked if there will be extra traffic with a standalone location. The applicant expects additional traffic.

Commissioner Stanly agreed with Chairman Hand about adding a glass window in the front. He asked if there is any way to extend the window on the side to make it look more pleasing. The applicant mentioned that from an architectural standpoint, they can make an adjustment. Operationally, it wouldn't make an impact or be an issue. Commissioner Stanly mentioned that customers might be confused and may try to enter through the door. Randy Bafia mentioned that they could extend the window by about 6'.

Chairman Hand asked about how the applicant plans to comply with our Lighting Code. Randy Bafia mentioned that their lighting has complied in other communities.

Planner Udarbe asked if there were any comments about the sign variance for encroaching in clear vision. Chairman Hand thinks that this sign is necessary. Planner Udarbe asked about the variance for the two signs on the south elevation. The Commissioners had no comments. Planner Udarbe asked about the variance for the wall sign on the north side where a wall sign is not allowed by right. Vice Chair McGary said that that sign wouldn't be needed since it's facing trees, but she wouldn't be opposed to it for customers in the drive-through.

The masonry is missing from the base, so the monument sign would need a variance. Their base would be a metal panel. Randy Bafia, the applicant, mentioned that they could change this to masonry to match the building. They will submit revised plans.

Commissioners mentioned that they like the accessory, directional signs. Planner Udarbe will update the variance factors to reflect the sign variances.

Planner Udarbe mentioned the transition yard encroachment – 25’ transition buffer--- between two incompatible uses (commercial and agriculture). The south side is not required to have a transition yard since it’s next to commercial. The other lots would stay as agriculture and would need the transition buffer. The Commissioners did not have a problem with this because of the needed parking.

Director Gruba asked if the detention pond on the west is a wet or dry pond. Philip Wolf said that the detention area to the west will be dry. The rain garden will have water-loving plants without standing water.

Director Gruba clarified the tree preservation ordinance. Any trees under 6” can be removed with no mitigation. Trees between 6-24” have to be mitigated if 75% of the trees are removed on the site. Mitigation must occur for any trees removed that are 2’ wide. Had they tripped the 75% threshold, the on-site mitigation or restitution would have been much higher. Chairman Hand said that they can do partial mitigation and partial restitution.

The next step is to submit revisions and return for a public hearing. The applicant wants to start in the spring. Dunkin’s lease expires in May 2026 in the Speedway. They have had the location since Sept 2023, but it’s been there long before they bought it from the previous owners.

Other Business (continued)

Staff is still working on the ComEd Substation, Dunn Farm, Hidden Valley, along with other projects. The Goodings Grove School is looking to do a parking lot expansion and will need to rezone.

11. Attendance Confirmation

Four commissioners confirmed that they are able to attend the January 15, 2026 meeting. Commissioner Mesaf said that he might be able to attend. Planner Kahn will check with Commissioners Lyons and Foley after the meeting.

12. Adjournment.

A motion was made to adjourn by Commissioner Stanly, seconded by Commissioner Marshall. A voice vote was taken with all in favor, zero (0) opposed and the meeting was adjourned at 8:27PM.

Minutes transcribed and respectfully submitted by:

Dana Kahn (Assistant Planner): _____

Approved Date: _____



Agenda Item Number: 8.a
Plan Commission Meeting Date: January 15, 2025
Subject: Case No. HG-2526-SP
Address: 15830 S. Bell Rd.

Item Title: Consider a request for approval of an amendment to a Special Use Permit for Outdoor sales and storage in the C-3 General Business District at 15830 S. Bell Road, Homer Glen, Illinois [Case No. HG-2526-SP].

Staff Contact: Assistant Planner Dana Kahn

Property Information

Location: 15830 S. Bell Rd
PIN: 16-05-14-400-014-0000
Property Size: 7.5-acre lot
Existing Zoning: C-3 General Business District
Adjacent Zoning: N: C-2 Local Business
E: C-4 Highway Commercial / C-3 General Business
S: R-4 Single-family Residential
W: R-6 Multifamily Residential

Background Information

The applicant, Runnings, is requesting approval to designate a portion of their existing parking lot as a display and sales area for retail sales of utility trailers at the subject property of 15830 S. Bell Road. See Attachment 6 with the Site Plan for Outdoor Storage and Sales prepared by Staff. Note that trailers have already been stored on-site since at least November 2025 without prior approval of an amended Special Use Permit. The subject property of 15830 S. Bell Road was granted approval of Special Use Permits for Outdoor Sales and Storage under ordinances ORD14-062 (Case no. HG1430-SV) and ORD23-006 (Case no. HG-2230-SP). The latter was an amendment to the 2014 ordinance. The business, previously Big R store, has since changed to Runnings department store in 2023. See Attachment 4 with the complete history of the property.

Runnings has outdoor merchandise including but not limited to 10 trailers, sheds, gates for farmers, bags of gravel/rock, wire mesh, storage materials for farmers & landscapers, farm-related equipment, and plants in the springtime. Approved under ORD23-006, Town and Country Sheds sell Everlast Portable buildings including, but not limited to, sheds, barns, garages, and chicken coops in a portion of the outdoor storage area.

Public Comment

Staff received in-person complaints from two residents on January 5, 2026. The residents told staff about a semi-truck in the eastern portion of the parking lot along Bell Road, that the merchandise on-site never seems to “move” and that the wall sign on the south side of the building appears exceptionally bright. Code enforcement has been in contact with the owners of the semi-trucks which was removed as of January 8, 2026 (see *Condition #8*), and staff is currently investigating the permitting of the south wall sign.

Conformance with Zoning Regulations

Outdoor Sales and Storage

According to the Use and Bulk Table for Nonresidential Districts under Table 2A, Outdoor sales and storage is listed as a Special Use in the C-3 General Business District. The regulations below show the applicable regulations set forth in code sections § 220-828 and § 220-829.

§ 220-828: Outdoor storage:

Note that staff's responses to the following regulations are in bold and italics.

- A. Outdoor storage is defined as the keeping, in an unroofed area, of any goods, material, merchandise, or raw materials in the same place for more than 48 hours. The area within any structure which does not include a roof and at least one wall shall constitute an "unroofed area." ***Runnings has merchandise in unroofed areas onsite every day.***
- B. No goods, material, merchandise or raw materials intended for sale at retail shall be stored or stacked so that the height of such stored goods, material, merchandise or raw materials exceeds 42 inches. Individual items whose height exceeds 42 inches may be stored but shall not be stacked so as to exceed the height of an individual item. ***Complies. §220-828 B applies since the applicant has a 4' and 5' fence around the merchandise.***
- C. No goods, material, merchandise or raw materials shall be stored in such a manner so as to restrict or otherwise affect surface water drainage. ***Complies. Merchandise is stored on existing asphalt, which is already impervious, and therefore will not affect surface water drainage.***
- D. All nonpackaged goods, material, merchandise or raw materials shall be stored in such a manner so as to prevent the dispersal of such goods, material, merchandise or raw materials by the elements, such as wind and rain. ***Complies. The majority of the merchandise is stored within a fence. Staff recommends that all materials, except for sheds, be stored inside the fenced area under Condition #6.***
- E. All goods, material, merchandise or raw materials shall be stored on a permanent durable surface. ***Complies. Merchandise is stored on asphalt parking lot.***
- F. Except for raw materials intended to be sold in bulk or merchandise which is not customarily offered for sale in containers or packages, all goods, materials, merchandise or raw material intended for retail sale shall be stored in containers or packages. ***Complies. It appears that merchandise is not stored in containers.***
- G. No goods, material, merchandise or raw materials shall be stored on a public sidewalk so as to impede the orderly flow of pedestrian movement or create an unsafe condition for pedestrians. ***Complies. No merchandise is stored on public sidewalks.***
- H. No goods, material, merchandise or raw materials shall be stored on any zoning lot:
 - 1. Within 15 feet of the ingress entrance or egress exit for that zoning lot to any public street or any ingress entrance or egress exit to any adjacent zoning lot; ***Complies. The outdoor storage areas are more than 15' away from entrances and exits of 159th Street and Bell Road along with the adjacent parcel 16-05-144-100-13-0000.***
 - 2. Within any side yard or rear yard setback or within 15 feet of any side or rear lot line, whichever is greater; provided, however, if said zoning lot has a zero side yard or rear yard setback and screening is installed along said setback, then such storage may occur adjacent to said screening; and ***The existing rear storage area does not meet***

the required rear setback of 20' from the adjacent residential properties. This encroachment is considered an existing, non-conforming use and does not need to be brought into compliance unless changes were made to this rear outdoor storage area.

3. In the front yard within 15 feet of the lot line on dedicated roads or within 65 feet of the center line on nondedicated roads. **Complies. The proposed outdoor storage areas are set back at least 15' from the dedicated ROW of S. Bell Road and 159th Street.**
 - I. Except as otherwise allowed pursuant to the terms of a special use permit, the total area of outdoor storage per zoning lot shall not exceed 500 square feet. **Existing outdoor storage exceeds the 500 square foot limit as granted under ORD23-006. The applicant wants to amend the Special Use Permit to expand outdoor storage and sales to approximately 52,084.37 sf total. See detailed Table of storage descriptions on Page 4.**
 - J. All storage areas for waste collection and solid waste dumpsters shall be confined in an enclosed area that is screened on all sides with opaque screening of at least six feet in height. All storage areas for waste collection and solid waste dumpsters for new construction shall be constructed of a masonry-type material. **N/A for this request.**
 - K. Vehicles stored for repair shall be located within an area screened on all sides with opaque screening of at least six feet in height. **N/A for this request.**
 - L. All screening shall be installed and maintained in a sound condition and shall be constructed so as to not restrict or otherwise affect the surface water drainage. Except as otherwise provided herein, all screening shall be of a material and color which are consistent and compatible with those of the principal structure on the zoning lot. Screening shall not obstruct sight distance to or from any public street so as to impede or otherwise create a hazard for vehicular or pedestrian traffic. **N/A for this request.**
 - M. Subsection B shall not apply to storage yards which are screened and enclosed by a fence or other structure of not less than six feet in height on all sides. **Complies. Attached to the southwest corner of the Runnings store, the applicant has outdoor storage within a fence that is greater than 6'. Only merchandise in this section with the fence taller than 6' can be stored higher than 42".**

Currently, Runnings has a fence around the majority of the merchandise. The storage area does have a 4' and 5' tall fence, which means that they can store items up to 42" because the fence is less than 6' in height on all sides. The merchandise stacked 3' tall outside of the fence (wire mesh and bags of rocks/gravel) does not exceed 46" in height. **Staff recommends that all outside storage materials (expect for sheds) in the main area be stored within the fenced area and not outside of the fenced area (See Condition #6).**

The proposal for outdoor storage exceeds 500 sf, but the applicant is requesting a total of 52,084.37 sf of storage. See Table on Page 4 with each storage area.



Site Photos of faded parking stalls with merchandise outside of the fence, encroaching on parking stalls



Description of Storage Area	Square Footage
[1] Storage area along east side of building	3,484.44 sf
[2] Storage area in middle of parking lot*	31,751.84 sf
[3] Storage area for sheds	6,990.73 sf + 3,262.42 sf = 10,253.15 sf
[4] Storage area connected to rear (SW corner of building)	6,694.77 sf
Total Storage Area	52,084.37 sf

*Applicant is requesting an expansion of storage in the middle of parking lot from 10,388 sf to 31,751.84 sf. The ~10,000sf of storage was previously approved in ORD23-006.

§ 220-829: Outdoor sales:

The applicant is requesting to amend the Special Use Permit granted in ORD23-006 to include the outdoor sales of trailers. Outdoor sales, including merchandising and promotional displays, are subject to the following with staff responses in bold and italics:

- A. The use shall be located so as to have direct vehicular access to a public street. **Complies. As seen in Attachment 6, the proposed storage allows for a clear drive aisle for accessing 159th Street and also Bell Road.**
- B. No sales or display activity shall be located on a public right-of-way. **Complies. The entire area of merchandise is located out of public ROW.**
- C. The location of materials for sale shall not be located so as to block visibility for vehicles or pedestrians on or off the lot in a manner that would create a safety hazard. **Complies. The location of the proposed merchandise is out of the required clear vision triangle on the corner of Bell Road and 159th Street.**
- D. All trash and debris are to be removed daily. **Staff noticed some litter accumulating around the fenced storage area. Staff recommends adding a condition of approval that the property be cleared of litter on a weekly basis. See Condition #7.**
- E. All signage shall conform to the standards set forth in § 220-1005. **As condition #4 to the special use approvals, the applicant needs to conform to signage standards. There are 5 prohibited signs on site. Staff has added this as a condition #5 of approval. See sign regulations below**

Signage

Runnings currently has 5 prohibited signs displayed without permits on site. The signs are attached to the fence around the outdoor storage area. Note that any signage requires permits and must comply with sign code (See Condition #4). According to § 220-1005 C(4)(b), prohibited placement of signs includes:

- “Signs affixed to fences, except “No Trespassing,” “No Parking/Towing,” and “Beware of Dog” signs in accordance with all other applicable sections on this article.”

Staff is recommending that Runnings removes any signs affixed to fences as a condition of this case (See Condition #5).

Site Photo of 5 Prohibited Signs from December 30, 2025



Site Photos of 5 Prohibited Signs (cont.)



Parking Adjustment

Note that per [§220-1004](#), the proposed outdoor storage and sales will require a parking reduction through the Village Board during the same meeting as HG-2526-SP. Runnings is deficient in parking stalls based on the business square footage. Previous ordinances and cases from 2014 and 2023 included “parking reduction variances”. However, the Zoning Ordinance allows parking variances to be granted as “parking adjustments”, which only require review and approval by the Village Board.

ADA Spaces are not blocked by any outdoor sales and storage. The applicant is blocking 9 parking stalls for utility trailers. Trailers are all under 3,000lbs.

Note that not all stalls are painted. **As condition #3 of approval, the applicant must re-strip any stalls that our faded and outside of the storage area.**

Site Photos of Stalls Needing Paint from December 23, 2025



From the applicant, James Sugrue (Store Manager):

“Currently the 231 parking spaces are more than sufficient for our customer count which is about 350 during the weekdays and 500 during the weekend.

Those numbers are split out during the course of the 12 hours we are open so there is always ample room for our customers.

When we start to pick up significantly more customer count, we will look at picking up more spaces where the sheds are and reduce the length of the outside storage.

I don't anticipate us needing those additional spaces for 2 to 3 years' time.”

Similar Special Use Permit Cases

- HG-1922-PS Gas N Wash Leonard McEnergy, 15930 W. 159th St (ORD19-043)
 - Approved SUP for Outdoor Storage in C-3 District with PUD
- HG-1706-PS: Leonard McEnergy, 159th St and Gougar Rd (ORD17-040)
 - Approved SUP for Outdoor Sales and Storage in C-3 District with a PUD
- HG-1502-SV: Peerless Fence, 15901 S. Parker Rd (ORD015-017)
 - Approved SUP for Outdoor Storage
- HG-1204-MS: Kane Brothers, 12137 W 159th St (ORD12-044)
 - Approved SUP for Outdoor Storage in I-1 District

Findings of Fact: Sections 220-1209D of the Code of the Village of Homer Glen states the required standards for making findings of fact for a Special Use Permit. The Code requires that the Plan Commissioners consider these standards in making its finding and determining a recommendation to send to the Village Board. Please refer to *Attachment 2* for the Findings.

Conformance with other Village Regulations

Exterior Construction Standards (Chapter 75, Article II): The Exterior Construction Standards do not apply to this request as the applicant is not proposing any exterior building changes.

Outdoor Lighting (Chapter 75, Article II): The Outdoor Lighting regulations do not apply to this request as the applicant is not proposing any new lights in the parking lot.

Conservation Design (Chapter 107, Article IV): The Conservation Design regulations do not apply to this request as this is not a residential subdivision.

Tree Preservation (Chapter 107, Article III): The Tree Preservation regulations do not apply to this request as the subject property is less than 5 acres and there are no significant trees on the subject property.

Subdivision & Stormwater (Chapter 138, Article I): The Subdivision and Stormwater regulations do not apply to this request as no site development is proposed.

Park Donation (Chapter 138, Article II): The Park Donation regulations do not apply to this request as the subject property is not a residential subdivision.

Conformance with Adopted Plans

Comprehensive Land Use Plan: The subject property is currently zoned C-3 General Business, and the 2005 Comprehensive Plan designates the property as Commercial.

Transportation Plan: The applicant is not proposing any changes to the existing ingress and egress as part of this proposal.

Motion for Consideration

Is there a motion to recommend _____ (approval / approval with conditions / denial) of an amendment to a Special Use Permit for Outdoor sales and storage, in accordance with the reviewed plans, public testimony and findings of fact, for certain real property located in the C-3 General Business Zoning District at the common address 15830 S. Bell Road, Homer Glen, Illinois [HG-2526-SP], subject to the following conditions:

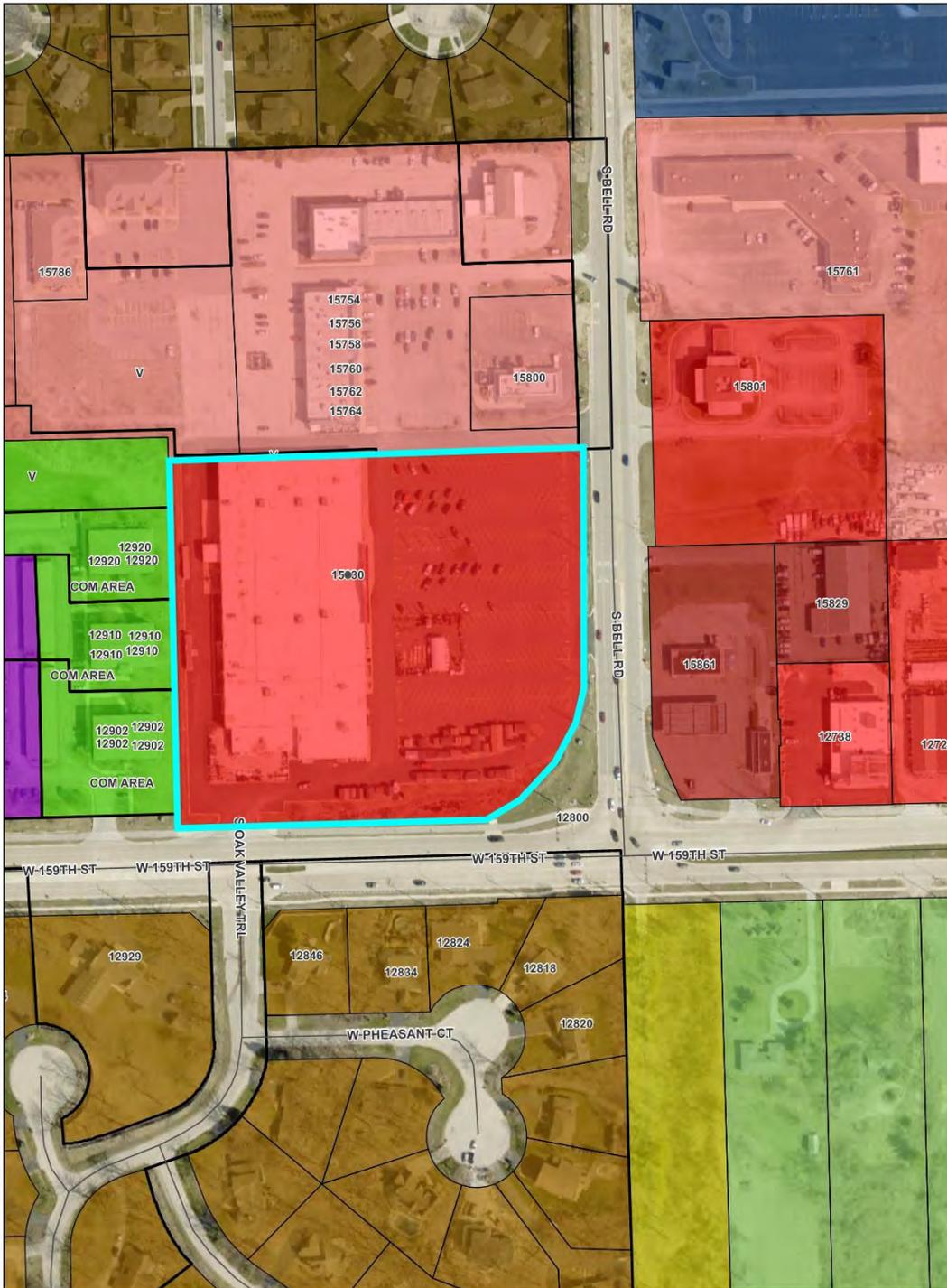
1. The existing fire lane must remain open and clear of any storage or vehicles;
2. The area in front of the hydrant on the southern edge of the parking lot must remain clear and accessible;
3. The 23 faded parking stalls located immediately south of the 31,752 square foot storage area shall be re-striped no later than August 1, 2026;
4. Any proposed signage for outdoor sales must conform to sign regulations, and applicant must submit permit applications for any future signage;
5. The existing prohibited signs on the fence around the 31,752 square foot outdoor storage area, as illustrated on pages 4 and 5 of the Plan Commission staff report, shall be removed per §220-1005 C(4)(b);
6. Merchandise and materials, except for the sheds, must be stored within the fenced area and cannot block parking stalls as shown on the site plan;
7. The property shall be cleared of litter and debris on a weekly basis by Runnings employees or contracted janitorial services;
8. Semi-truck tractors and trailers shall not be stored or parked on the property unless actively loading or unloading within the designated loading zone.

Attachments

1. Land Use Viewer
2. Findings of Fact – Special Use Standards
3. Site Photos
4. History of 15830 S. Bell Road – Outdoor Storage
5. Aerials of Outdoor Storage 2023 - 2025
6. Site Plan for Outdoor Storage and Sales (prepared by Staff)

Attachment 1 – Land Use Viewer

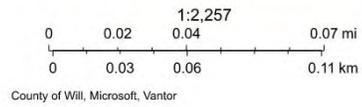
Land Use Viewer of Runnings Store 15830 S. Bell Rd



12/23/2025, 3:47:54 PM

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Subject Property



Village of Homer Glen

Attachment 2 – Findings of Fact
Standards for a Special Use Permit

Chapter 220-1209D of the Code of the Village of Homer Glen states the required standards for making findings of fact for a special use (and any amendments thereto). The following are the categories with the applicant's findings (*in italics*):

1. That the establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.

This proposal does not pose any risk to public welfare. All trailers will be displayed outdoors, positioned away from drive lanes, and securely anchored to prevent any movement toward public areas or parked vehicles.

2. The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.

We would merchandise our trailers for sale in a clean, organized simple pattern. This would make our lot look more full of merchandise to entice additional sales and tax revenue.

3. That the special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.

These trailers will be sold, and our inventory will be continuously updated with new models. We will exclusively offer new merchandise. This activity will serve as an indicator of a thriving neighborhood.

4. The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.

We are requesting a permit to sell trailers because current retail space cannot accommodate them. If approved, we will bring high-quality trailers to the community at competitive prices.

5. The proposed use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.

This is suitable because this is part of retail and commerce. These trailers are easily movable and are not a permanent structure.

6. That the establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

We will not be building a structure that impedes with any future development.

7. That the exterior architectural appeal and functional plan of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.

N/A, no utility work will be done.

8. That the adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.

N/A, no utility work will be required for this special use permit.

9. That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.

We will keep all trailers away from ingress and egress to our property and out of any drive or emergency lanes.

10. The proposed use has been considered in relation to the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the plan.

We are looking to add trailer sales to conform with our other 89 locations that also sell trailers. These create additional sales and tax revenue as well as foot traffic to our location. We cannot park several different models inside the building. A special use permit outside would be ideal for this type of merchandise retail.

Attachment 3 – Site Photos



**SITE PHOTOS FROM STAFF ON
11.19.25**



#





Site photos from staff December 2025





Site photos from staff December 2025

Attachment 4 - History of Outdoor Storage/Sales for 15830 S. Bell Rd (now Runnings)

- 10.6.14: Plan Commission public hearing [HG-1420-SV] for Big R
 - SUP for Outdoor Sales
 - Variance for Off-street parking
 - Wall Sign Variances
- 10.22.14: Board [HG-1420-SV]
 - ORD 14-061: SUP for Outdoor sales (can't infringe upon ADA spaces)
 - ORD 014-062: Variance for off-street parking from the required 368 spaces to 328 spaces
 - ORD 14-063: Wall Sign Variance for # of wall signs
 - ORD 14-064: Wall Sign Variance for amount of wall signs
- 1.5.23 & 1.19.23: Plan Commission public hearing [HG-2230-SP] for Town Country Sheds
 - SUP for Outdoor storage and sales
 - 1. Added storage and sales of sheds
 - Variance for Off-street parking
- 1.23.25: Board [HG-2230-SP]
 - ORD 23-006
 - 1. Amendment to SUP for Outdoor sales
 - 2. Special Use for Outdoor storage
 - 3. Variance for required parking from 368 to 295 spaces
 - 4 conditions
 - 1. Fire lane clear
 - 2. Hydrant clear
 - 3. If parking is an issue, they need to re-strip stalls
 - 4. Sign conformance for sheds
- 5.23.23: Runnings obtains business license
- 6.14.23: Runnings Commercial Occupancy Build-out
- 1.15.26: Plan Commission public hearing [HG-2526-SP] for Runnings
 - Amendment to SUP for Outdoor storage and sales (add trailers & expand outdoor storage)
 - Code citations: parking lot storage, temporary signs (no permits), semi-truck cab parked in front, semi-trailer storage behind the building, 5 signs affixed to fence, outdoor storage blocking parking stalls, blank parking stalls
- TBD 2026: Board [HG-2526-SP]
 - SUP for Outdoor Storage
 - Parking Adjustment

Attachment 6: Site Plan for HG-2526-SP Prepared by Staff January 2026 for Outdoor Storage and Sales on Runnings property at 15830 S. Bell Road



Commercial Real Estate
Due Diligence Management
3465 S. Arlington Road
Suite E #183
Akron, OH 44312
330-294-1077
www.annational.net

ALTA/NSPS Land Title Survey

Big R Portfolio
STORE Capital Client File No.: 7210/02-621.9

Surveyor Certification

15830 South Bell Rd
Homer Glen, IL 60491
County of Will

To: STORE Master Funding XV, LLC, a Delaware limited liability company, STORE Capital Corporation, a Maryland corporation, each of their respective successors and/or assigns; Chicago Title Insurance Company and American National, LLC.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 6a, 6b, 7a, 7b, 7c, 8, 9, 10a, 10b, 11, 14, 16, 17, 18, 20, 21 of Table A thereof. The network was completed on 6/16/16.

DAVID J. SHERILL
Land Surveyor Number 2534
In State of Illinois
Date of Plat or Map: 05-25-18
Date of last revision: 7-10-18
Network reference #20180616-013



Survey Prepared By:
Sherrill Associates, Inc.
3164 North Main Street
Edwardsville, IL 62025

Legal Description

PARCEL I: COMMENCING AT THE SOUTHEAST CORNER OF SECTION 14, TOWNSHIP 36 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS; THENCE NORTH ALONG THE EAST LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 14, SAID EAST LINE HAVING THE ASSUMED BEARING OF NORTH 0 DEGREES 00 MINUTES EAST, A DISTANCE OF 600.12 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 40.0 FEET TO A POINT; SAID POINT TO BE KNOWN AS THE POINT OF BEGINNING; THENCE SOUTH 0 DEGREES 00 MINUTES WEST A DISTANCE OF 332.74 FEET TO A POINT ON A CURVE TO THE RIGHT HAVING A RADIUS OF 271.50 FEET, A DEGREE OF CURVATURE OF 21 DEGREES 30 MINUTES; THENCE NORTH AN ANGLE OF 89 DEGREES 39 MINUTES; THENCE SOUTHERLY AND WESTERLY ALONG SAID CURVE TO THE RIGHT A LONG CHORD DISTANCE OF 278.49 FEET TO A POINT SAID LONG CHORD HAVING A BEARING OF SOUTH 41 DEGREES 45 MINUTES WEST; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 426.83 FEET TO A POINT; THENCE NORTH 0 DEGREES 00 MINUTES WEST A DISTANCE OF 556.57 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST A DISTANCE OF 614.02 FEET TO THE POINT OF BEGINNING.

PARCEL II: NON-EXCLUSIVE EASEMENT FOR STORM RETENTION PURPOSES AS CREATED IN WARRANTY DEED FROM COI, LTD., TO OTR, AN OHIO GENERAL PARTNERSHIP RECORDED DECEMBER 13, 1981 AS DOCUMENT NO. R81-2881, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF SECTION 14, TOWNSHIP 36 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS; THENCE NORTH ALONG THE EAST LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 14, SAID EAST LINE HAVING THE ASSUMED BEARING OF NORTH 0 DEGREES 00 MINUTES EAST, A DISTANCE OF 600.12 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 40.0 FEET TO A POINT; SAID POINT TO BE KNOWN AS THE POINT OF BEGINNING; THENCE SOUTH 0 DEGREES 00 MINUTES WEST, 10.0 FEET TO A POINT; THENCE NORTH 0 DEGREES 00 MINUTES WEST, 35.75 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST, 411.04 FEET TO A POINT ON THE EAST LINE OF THE WEST 250.0 FEET OF THE EAST 1/2 OF THE SOUTHWEST 1/4 OF SAID SECTION 14; THENCE NORTH 0 DEGREES 00 MINUTES WEST ALONG THE EAST LINE OF THE WEST 250.0 FEET OF THE SOUTHWEST 1/4 OF SAID SECTION 14, A DISTANCE OF 50.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST, 200.0 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 40.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST A DISTANCE OF 221.04 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 45.75 FEET TO THE POINT OF BEGINNING.

PARCEL III: LEASEHOLD EASEMENT AS CREATED BY A LEASE/EASEMENT AND RECIPROCAL AGREEMENT DATED JANUARY 15, 1981, BY AND BETWEEN FIRST NATIONAL BANK OF LANSING, TRUST NO. 2967, AS SELLER, PETERSON GASSER AND ASSOCIATES, INC., AS BUYER/LESSEE AND FIRST NATIONAL BANK OF LANSING, TRUST NO. 3036, AS ADJOINING OWNER/LESSOR, RECORDED MAY 1, 1981, AS DOCUMENT NO. R81-10735, AND RE-RECORDED NOVEMBER 19, 1981, AS DOCUMENT NO. R81-2891, AND AMENDED BY AMENDMENT RECORDED DECEMBER 7, 1981 AS DOCUMENT NO. R81-2892, AND AS ASSIGNED BY DEED AND ASSIGNMENT MADE BY PETERSON GASSER & ASSOCIATES, INC., A UTAH CORPORATION TO COI, LTD., A UTAH LIMITED PARTNERSHIP RECORDED DECEMBER 7, 1981 AS DOCUMENT NO. R81-2892, AND ALSO AS CREATED BY MEMORANDUM OF GROUND LEASE RECORDED AS DOCUMENT NO. R83-32779 DENISING THE FOLLOWING DESCRIBED PROPERTY: COMMENCING AT THE SOUTHWEST CORNER OF SECTION 14, TOWNSHIP 36 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS; THENCE NORTH ALONG THE EAST LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 14, SAID EAST LINE HAVING THE ASSUMED BEARING OF NORTH 0 DEGREES 00 MINUTES EAST, A DISTANCE OF 600.12 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 40.0 FEET TO A POINT; SAID POINT TO BE KNOWN AS THE POINT OF BEGINNING; THENCE SOUTH 0 DEGREES 00 MINUTES WEST A DISTANCE OF 332.74 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 426.83 FEET TO A POINT; THENCE NORTH 0 DEGREES 00 MINUTES WEST, 10.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES WEST, 411.04 FEET TO A POINT ON THE EAST LINE OF THE WEST 250.0 FEET OF THE EAST 1/2 OF THE SOUTHWEST 1/4 OF SAID SECTION 14; THENCE NORTH 0 DEGREES 00 MINUTES WEST ALONG THE EAST LINE OF THE WEST 250.0 FEET OF THE SOUTHWEST 1/4 OF SAID SECTION 14, A DISTANCE OF 50.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST, 200.0 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 40.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST A DISTANCE OF 221.04 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 45.75 FEET TO THE POINT OF BEGINNING.

PARCEL IV: TEMPORARY EASEMENT FOR DRIVEWAY PURPOSES, AS CREATED BY A LEASE/EASEMENT AND RECIPROCAL AGREEMENT DATED JANUARY 15, 1981, BY AND BETWEEN FIRST NATIONAL BANK OF LANSING, TRUST NO. 2967, AS SELLER, PETERSON GASSER AND ASSOCIATES, INC., AS BUYER/LESSEE AND FIRST NATIONAL BANK OF LANSING, TRUST NO. 3036, AS ADJOINING OWNER/LESSOR, RECORDED MAY 1, 1981, AS DOCUMENT NO. R81-10735, AND RE-RECORDED NOVEMBER 19, 1981, AS DOCUMENT NO. R81-2891, AND AMENDED BY AMENDMENT RECORDED DECEMBER 7, 1981, AS DOCUMENT NO. R81-2892, AND AS ASSIGNED BY DEED AND ASSIGNMENT MADE BY PETERSON GASSER & ASSOCIATES, INC., A UTAH CORPORATION TO COI, LTD., A UTAH LIMITED PARTNERSHIP RECORDED DECEMBER 7, 1981 AS DOCUMENT NO. R81-2892, AND ALSO AS CREATED BY MEMORANDUM OF GROUND LEASE RECORDED AS DOCUMENT NO. R83-32779 DENISING THE FOLLOWING DESCRIBED PROPERTY: COMMENCING AT THE SOUTHWEST CORNER OF SECTION 14, TOWNSHIP 36 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS; THENCE NORTH ALONG THE EAST LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 14, SAID EAST LINE HAVING THE ASSUMED BEARING OF NORTH 0 DEGREES 00 MINUTES EAST, A DISTANCE OF 600.12 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 40.0 FEET TO A POINT; SAID POINT TO BE KNOWN AS THE POINT OF BEGINNING; THENCE SOUTH 0 DEGREES 00 MINUTES WEST A DISTANCE OF 332.74 FEET TO A POINT; THENCE SOUTH 89 DEGREES 39 MINUTES WEST A DISTANCE OF 426.83 FEET TO A POINT; THENCE NORTH 0 DEGREES 00 MINUTES WEST, 10.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES WEST, 411.04 FEET TO A POINT ON THE EAST LINE OF THE WEST 250.0 FEET OF THE EAST 1/2 OF THE SOUTHWEST 1/4 OF SAID SECTION 14; THENCE NORTH 0 DEGREES 00 MINUTES WEST ALONG THE EAST LINE OF THE WEST 250.0 FEET OF THE SOUTHWEST 1/4 OF SAID SECTION 14, A DISTANCE OF 50.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST, 200.0 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 40.0 FEET TO A POINT; THENCE NORTH 89 DEGREES 39 MINUTES EAST A DISTANCE OF 221.04 FEET TO A POINT; THENCE SOUTH 0 DEGREES 07 MINUTES EAST A DISTANCE OF 45.75 FEET TO THE POINT OF BEGINNING.

EXCEPTING THEREFROM:
THAT PART OF THE EAST HALF OF THE SOUTHWEST QUARTER OF SECTION 14, TOWNSHIP 36 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF THE SOUTHWEST QUARTER OF SAID SECTION 14, THENCE SOUTH 88 DEGREES 02 MINUTES WEST (BEARINGS BASED ON ILLINOIS STATE PLANE COORDINATES, EAST ZONE, NAD83-2007) ALONG THE SOUTH LINE OF SAID SOUTHWEST QUARTER, 655.12 FEET TO THE EAST LINE OF THE WEST 681.00 FEET OF THE EAST HALF OF SAID SOUTHWEST QUARTER; THENCE NORTH 0 DEGREES 41 MINUTES 13 SECONDS WEST ALONG SAID LINE, 42.07 FEET TO THE NORTH RIGHT-OF-WAY LINE OF 159TH STREET AS DESIGNATED BY DOCUMENT NO. 489892 AND THE POINT OF BEGINNING; THENCE CONTINUING NORTH 0 DEGREES 41 MINUTES 13 SECONDS WEST, 10.00 FEET; THENCE NORTH 88 DEGREES 08 MINUTES 53 SECONDS EAST PARALLEL WITH THE SAID NORTH RIGHT-OF-WAY LINE OF 159TH STREET, 454.09 FEET TO THE WEST RIGHT-OF-WAY LINE OF BELL ROAD AS DESIGNATED BY DOCUMENT NO. 307887 BEING A CURVE CONCAVE TO THE NORTHWEST; THENCE SOUTHWEST 28.04 FEET ON SAID CURVE TO THE RIGHT HAVING A RADIUS OF 271.80 FEET; THENCE SOUTH 87 DEGREES 59 MINUTES 35 SECONDS WEST, A DISTANCE OF 28.02 FEET TO THE SAID NORTH RIGHT-OF-WAY LINE OF 159TH STREET; THENCE SOUTH 88 DEGREES 08 MINUTES 53 SECONDS WEST ALONG SAID NORTH RIGHT-OF-WAY LINE, 426.83 FEET TO THE POINT OF BEGINNING, IN WILL COUNTY, ILLINOIS.

THE ABOVE DESCRIPTION IS THE SAME AS THAT PROPERTY IN CHICAGO TITLE INSURANCE COMPANY COMMITMENT NO. 180022309, DATED MAY 22, 2018.

ZONING INFORMATION

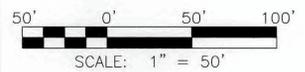
ZONED C-3 GENERAL BUSINESS DISTRICT
MINIMUM LOT SIZE: 20,000 S.F.
MINIMUM LOT WIDTH: 100 FEET
MAXIMUM DENSITY: 75% LOT COVERAGE/0.5 F.A.R.
MAXIMUM BUILDING HEIGHT: 35 FEET

SETBACKS
FRONT: 80 FEET NON-DEDICATED RIGHT-OF-WAY; 45 FEET DEDICATED;
30 FEET LANDSCAPED
STREET SIDE: 80 FEET NON-DEDICATED RIGHT-OF-WAY; 45 FEET DEDICATED; 30 FEET LANDSCAPED
INTERIOR SIDE: 20 FEET/50 FEET ABUTTING RESIDENTIAL
REAR: 20 FEET/50 FEET ABUTTING RESIDENTIAL

PARKING: 4 SPACES PER 1,000 SQUARE FEET OF GROSS FLOOR AREA
ZONING INFORMATION PROVIDED BY
GLOBAL ZONING CONFORMANCE SUMMARY REPORT
JOB #GZ 5068
DATED JULY 5, 2018

Legend of Symbols & Abbreviations

⊠	CUT "X" FOUND	○	IRON PIN SET
●	IRON PIN FOUND	—(R)—	OVERHEAD ELECTRIC
—	EXISTING STRUCTURE	—(M)—	CORRUGATED METAL PIPE
+	POWER POLE	—(C)—	RIGHT OF WAY
—	GUY WIRE	—(T)—	TRANSFORMER
⊕	LIGHT STANDARD	—(R)—	CONCRETE
⊖	WATER METER	—(M)—	CL
⊕	WATER VALVE	—(T)—	PL
⊕	FIRE HYDRANT	—(R)—	(TYP)
⊕	GAS METER	—(M)—	(R)
⊕	GRATED INLET	—(M)—	(M)
⊕	MANHOLE		
⊕	SIGN		
⊕	HANDICAP		
⊕	PIPE BOLLARD		
⊕	CURB INLET		
⊕	CABLE SPLICE BOX		
⊕	TELEPHONE SPLICE BOX		
⊕	ELECTRIC SPLICE BOX		
⊕	ELECTRIC METER		
⊕	TRANSFORMER		



LOCATION MAP

Encroachment Statement

A BUILDING CROSSES FRONT SETBACK LINE BY A WIDTH OF 20.0' FOR A LENGTH OF 209.6' AS SHOWN.

Utility Notes

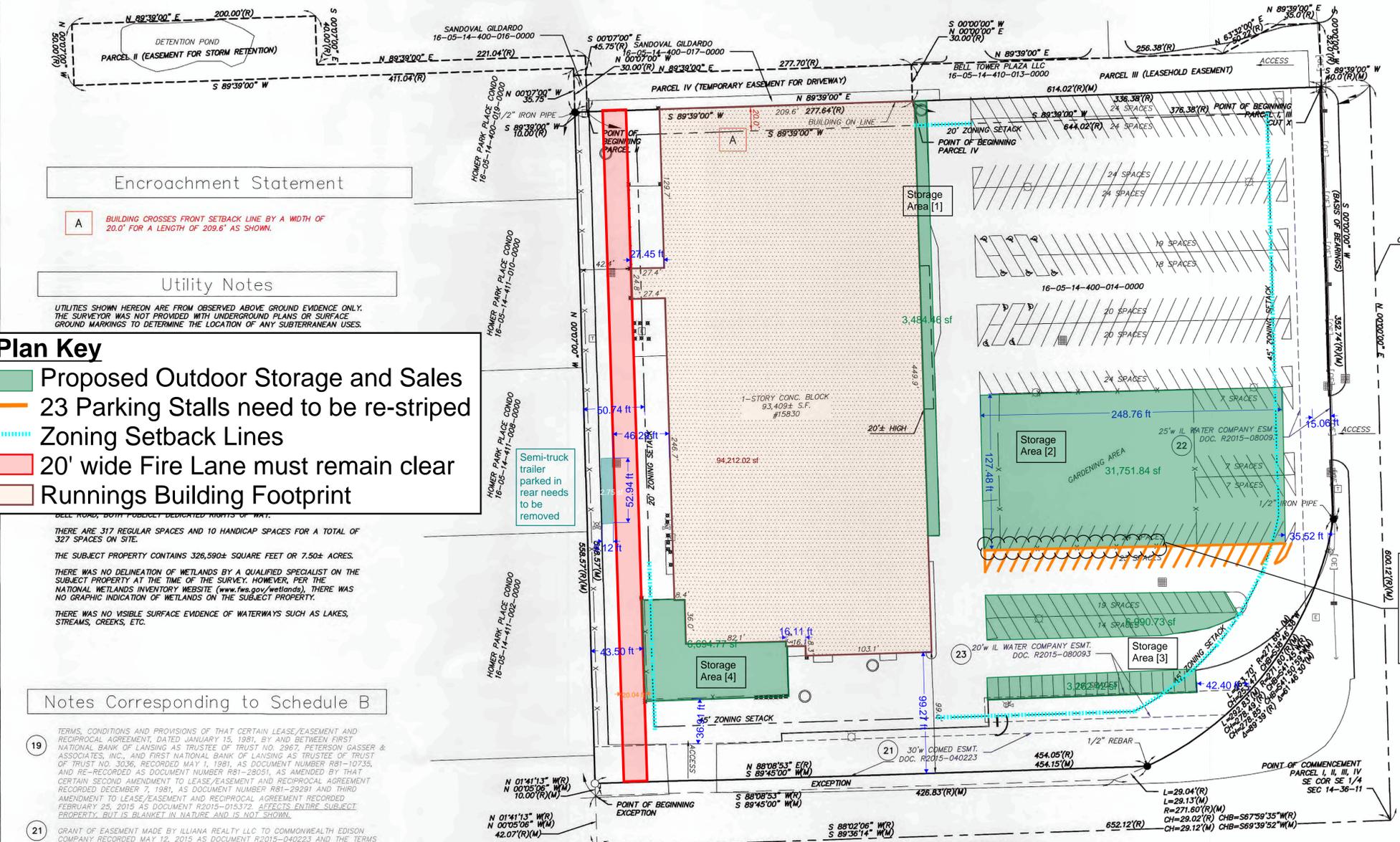
UTILITIES SHOWN HEREON ARE FROM OBSERVED ABOVE GROUND EVIDENCE ONLY. THE SURVEYOR WAS NOT PROVIDED WITH UNDERGROUND PLANS OR SURFACE GROUND MARKINGS TO DETERMINE THE LOCATION OF ANY SUBTERRANEAN USES.

Site Plan Key

- Proposed Outdoor Storage and Sales
- 23 Parking Stalls need to be re-striped
- Zoning Setback Lines
- 20' wide Fire Lane must remain clear
- Runnings Building Footprint

Notes Corresponding to Schedule B

- 19 TERMS, CONDITIONS AND PROVISIONS OF THAT CERTAIN LEASE/EASEMENT AND RECIPROCAL AGREEMENT, DATED JANUARY 15, 1981, BY AND BETWEEN FIRST NATIONAL BANK OF LANSING AS TRUSTEE OF TRUST NO. 2967, PETERSON GASSER & ASSOCIATES, INC., AND FIRST NATIONAL BANK OF LANSING AS TRUSTEE OF TRUST OF TRUST NO. 3036, RECORDED MAY 1, 1981, AS DOCUMENT NUMBER R81-10735, AND RE-RECORDED AS DOCUMENT NUMBER R81-2891, AS AMENDED BY THAT CERTAIN SECOND AMENDMENT TO LEASE/EASEMENT AND RECIPROCAL AGREEMENT RECORDED DECEMBER 7, 1981, AS DOCUMENT NUMBER R81-2892, AND THIRD AMENDMENT TO LEASE/EASEMENT AND RECIPROCAL AGREEMENT RECORDED FEBRUARY 25, 2015 AS DOCUMENT R2015-015372, AFFECTS ENTIRE SUBJECT PROPERTY, BUT IS BLANKET IN NATURE AND IS NOT SHOWN.
- 21 GRANT OF EASEMENT MADE BY ILLIANA REALTY LLC TO COMMONWEALTH EDISON COMPANY RECORDED MAY 12, 2015 AS DOCUMENT R2015-040223 AND THE TERMS AND PROVISIONS CONTAINED THEREIN APPLIES AND AFFECTS AS SHOWN.
- 22 EASEMENT AND RIGHT OF WAY AGREEMENT MADE BY AND BETWEEN ILLIANA REALTY LLC AND ILLINOIS-AMERICAN WATER COMPANY RECORDED JULY 30, 2015 AS DOCUMENT R2015-064369 AND THE TERMS AND PROVISIONS CONTAINED THEREIN APPLIES AND AFFECTS AS SHOWN.
- 23 EASEMENT AND RIGHT OF WAY AGREEMENT MADE BY AND BETWEEN ILLIANA REALTY LLC AND ILLINOIS-AMERICAN WATER COMPANY RECORDED SEPTEMBER 17, 2015 AS DOCUMENT R2015-080093 AND THE TERMS AND PROVISIONS CONTAINED THEREIN APPLIES AND AFFECTS AS SHOWN.



Note that there is merchandise outside of the fence and some materials that are blocking the parking stalls in orange.

FLOOD NOTE: By graphic plotting only, this property is in Zone(s) X of the Flood Insurance Rate Map, Community Panel No. 1719700185E and is not in a Special Flood Hazard Area. 09-06-1995 and is not in a Special Flood Hazard Area. ZONE X - AREAS DETERMINED TO BE OUTSIDE 500-YEAR FLOODPLAIN.



PLAN COMMISSION MEMORANDUM

Agenda Item Number: 8.b
Plan Commission Meeting Date: January 15, 2026
December 18, 2025 (kept public hearing open)
Subject: Case No. HG-2530-PUD
Address: 13812 S. Bell Road &
13830 S. Bell Road
(Lot 1, Lot 2, and Remnant of Lot 4)

Item Title: Consider a request for approval of [1] a Special Use Permit for a Major Change to the Menards Planned Unit Development (PUD) with requested exceptions; [2] a Special Use Permit for a car wash (*classified as an automobile repair, service and body shop use*), [3] a Special Use Permit for a drive-through establishment, [4] a Special Use Permit for an automobile service station (*gas station*), [5] a Special Use Permit for 24-hour operation; and [6] Site Improvement Plans for a unified development (depicted in plans on file with the Village) on certain real property within the C-2, Local Business District, generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois. (PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000).

Staff Contact: Director of Planning & Zoning Christopher Gruba & Assistant Planner Dana Kahn

Property Information

Location: 13812 S. Bell Road & 13830 S. Bell Road
Current PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, &
16-05-02-213-004-0000
Previous PIN (prior to R2025004595): 16-05-02-200-016-0000
Planned Unit Development: Lot 1, Lot 2, and Remnant of Lot 4 from the Resubdivision
of Lot 5 in Menard's Subdivision
Lot 1 Area: 56,156 sf / 1.29 Acres
Lot 2 Area: 78,126 sf / 1.79 Acres
Remnant of Lot 4: 44,431 sf / 1.02 Acres
Existing Zoning: C-2, Local Business, no map amendment requested
Adjacent Zoning: N: C-2 Local Business
E: R-4 Single Family Residential
E: E-1 Single-Family Estate Residential
S: C-2 Local Business
W: C-2 Local Business

General Note

The proposed plans are nearly identical to the former project that was approved in 2023 but eventually expired due to not obtaining building permits and starting construction. The substance of the plans has not changed and this request again pertains to a gas station with convenience store, restaurant with drive-through use and a car wash. The differences between the old plans and the new plans are negligible, but the latest plans do offer more detail than the original plans. Since the old plans expired on April 11, 2025, the applicant is seeking “re-approval” of this project (see Project History attachment). The applicant has expressed to staff that they would like to begin construction in March of this year. Lots 1, 2 and the remnant part of Lot 4 were previously subdivided during the previous approval and were recorded with the county. This Plat of Subdivision does not expire once recorded.

Background Information

The applicant, NSS Group, Inc, is proposing a major change to the Menards Planned Development to develop Lot 1, Lot 2 and a remnant portion of Lot 4. Lot 1 would be developed for a 4,865 square foot drive-through car wash with outdoor vacuum stations, and an 11’ tall monument sign. Lot 2 would be developed for a 7,710 square foot automobile service station (gas station) divided into a 4,929 square foot convenience store (Breezy’s) and a 2,781 square foot restaurant (Scali’s Beef). The remnant part of Lot 4 would be developed for a vehicle connection driveway, a 13’ tall monument sign and 2 light poles.

List of Proposed Businesses, description and hours of operation:

Business Name	Proposed Hours of Operation	Description	Permitted or Special Use (§220 Table 2A)
Platinum Car Wash	7 am – 10 pm	Car wash building with overhead vacuum canopy	Special Use in C-2
Gas Service Station: Mobil	24 hours	Gas station with 8 gas pumps	Special Use in C-2 and Special Use for 24-hours
Breezy’s Convenience Store	24 hours	Convenience store for gas station connected to restaurant	Permitted in C-2 but needs Special Use for 24-hour operation
Scali’s Beef	11:30 am – 11 pm, possibly midnight on weekends eventually	Restaurant with indoor seating and drive-through	Special Use for Drive-Through with permitted use of a Restaurant in C-2 and Special Use for 24-hours

Public Comment: Staff has not received any public comment on this project as of January 9, 2026.

Conformance with Zoning Regulations Site Plan Bulk Regulations (§220 Table 1B):

Setbacks

The building setbacks meet code, with the exception of the side yard setbacks, as measured from both buildings from the interior shared lot line. The applicant is requesting a reduction to the required side setback for the north side of the restaurant/convenience store from 20’ to 12.5’ and for the south side of the car wash from 20’ to 17.2’ feet from the central lot line between the proposed Lots 1 and 2 (see PUD Exceptions).

Parking areas and drive aisles are required to be located a minimum 5' setback from property lines. By the nature of a planned development, the parking lot straddles Lots 1 and 2 and therefore creates the need for an exception to allow the pavement to come right up to the property line. The applicant is requesting a parking/drive aisle setback reduction from 5' feet to 0' (see PUD Exceptions).

Nonresidential uses are required to provide a 30' landscaped setback (no parking/buildings) from the front property line. Although the exact dimension varies of the landscape setback in front of the Menards parking lot, Arby's, Firestone, etc., generally, there is a reduction of this front landscape area along the entire Menards PUD to between 18' and 20'. The applicant is requesting a reduction to the required landscape setback from 30' to 20', allowing the parking lot to follow the same layout/setback as the remainder of the Menards PUD (see PUD Exception). The applicant has provided a landscape plan that provides landscaping above and beyond code requirements within this area of the site plan to help offset the reduction.

Impervious/ Lot Coverage

The maximum lot coverage permitted for the C-2 Zoning District is 65%. The proposed impervious surface coverage for Lot 1 (Car Wash) is 63.9% and complies. The proposed impervious surface coverage for Lot 2 (Gas Station/C-Store/Restaurant) is 69.7%, exceeding the permitted 65% maximum. The applicant is requesting an exception to allow for an increase in impervious surface from the required 65% to 69.7% (see PUD Exception). Preliminary engineering review on the stormwater has indicated that the pond will not need to be enlarged as the lots were originally designed to generally handle more than 75% impervious coverage.

Accessory Structure Height

The applicant is requesting an exception for the height of the accessory structures. The applicant is requesting the height of the "pay canopy" for the car wash to be 19.2' above the permitted 15'. Also, the applicant is requesting the height of the gas canopy for the gas station be 17' above the permitted 15' (see PUD Exception).

Parking Standards (§220-1002): The proposed development meets all off-street parking minimum space requirements including ADA spaces (see more details on ADA below). The parking table below shows the parking counts for Lots 1 and 2 detailing each use area. Note that staff counted gaming seats in the restaurant and gas pump parking spaces. Note that the applicant plans to store 2-3 food catering trucks for Scali's Beef on-site in the parking lot; there are no code concerns with this per the Building Department.

Car Wash on Lot 1						
USE AREA	# OF VEHICLES IN STALL	EMPLOYEES	SQUARE FOOTAGE	RATIO	REQUIRED	PROVIDED
Car Wash	3	2	n/a	2 per vehicle in wash; plus 1 per average # of employees	8	
Reservior Stalls				10 per wash stall	10	MORE THAN 10
TOTAL REQUIRED (not less than 15)					18	
Vacuum stalls (includes 1 ADA vacuum space)						22
Standard Spaces						3
ADA Space						1
TOTAL PROVIDED						26

NOTE: SOME PARKING IS LOCATED BETWEEN LOT 1 AND 2; not included in this count see: § 220-X-1002C

RESTAURANT/CONVENIENT STORE/GAS STATION LOT 2						
FROM "033 PROPOSED CARWASH MEZZ & FLOOR PLAN A-3"						
USE AREA	SEATS	EMPLOYEES	SQUARE FOOTAGE	RATIO	REQUIRED	PROVIDED
Retail North (convenient store)	n/a	n/a	5,000	4 per 1,000sf	20	
Restaurant South	30	2	n/a	1 per 3 seats plus 1 per employees at largest shift	12	
Drive-Through for restaurant					3 spaces for queue	
Gaming*	6			determined by zoning officer		
TOTAL REQUIRED					32	
Standard Spaces					43	
ADA					3	
Drive-Through for restaurant					3 spaces for queue	
Pump*					16	
TOTAL PROVIDED					46	

NOTE: SOME PARKING IS LOCATED BETWEEN LOT 1 AND 2; has been included in this count

Parking Stall Size

Per §220-1002(F) Off-street parking, parking spaces shall be at least 9' in width and at least 19' in length with a vertical clearance of 7.5'. The applicant is requesting that the parking stall depth be reduced from 19' to 18.5', which is consistent with parking lot stalls throughout the Menards PUD (see PUD Exception).

ADA Parking Stall Requirements

According to the ADA Standards for Accessible Design, the required number of accessible parking spaces shall be calculated separately for each parking facility. As such, Lot 1 with the car wash has 25 traditional parking spaces which requires a minimum of 1 ADA compliant parking space accessible for a car and van. For Lot 2, the total number of spaces is 43 which means that they need 2 ADA Parking Spaces, one of which is van accessible.

The proposed Site Plan provides all required ADA spaces. The applicant has opted for ADA spaces and access aisles that are 8' wide each which complies with ADA standards for van and car-accessible spaces that share an access aisle. Note that accessible parking must be placed on level pavement.

Loading Zone

One loading berth/zone as required by code, has been provided as a striped area in the lane just north of the gas station. The loading zone is proposed to be at least 12' x 30', which meets code.

Electric Vehicle (EV) Charging Stations

The applicant mentions wanting to include electric vehicle (EV) charging stations on site at some point in the future. Sheet C-2 of the Geometric Plan calls out a row of 10 parking spaces for "Future EV

Authorized Spaces”, but does not illustrate the charging stations themselves. It’s unclear whether the chargers would be located in the parking space, on the sidewalk or in the landscaped area next to the row of parking. If they were located in a parking space, it would create a deficient space depth. If they were placed on the sidewalk, it could obstruct the required 3’ wide path for wheelchair travel. If they were placed in the landscape area, some of the shrubs or trees would need to be removed. Staff recommends that the Plan Commission discuss these options and perhaps add a condition of approval to the Major Change to the PUD if necessary, that adequately anticipates and addresses future chargers.

Chargers would require a permit from the Building Department as is the case for personal EV charging stations on residential properties. The current Zoning Code does not address EV stations, although staff would like to develop such specific regulations in the near future.

Landscaping Standards: Code required landscaping has been met. Additionally, significant landscaping has been provided throughout the site and at the foundations of the two principal buildings, which is not required by code.

Zoning regulations require parking lot island landscaping. The project proposes all parking lot islands to be landscaped at the ends of the rows, and the perimeter parking aisles have continuous buffer of landscaping provided. The combination of these efforts meets the regulations of the code.

Landscape buffering has been provided between the subject property and the active horse farm approximately 14’ to the north in the form of canopy trees and large evergreens. During the initial review process in 2023, staff received comments requesting additional landscape buffering along that north side to protect the existing horse farm. Staff is requesting feedback from the Plan Commission regarding the landscape buffer as proposed along the north property line.

Signage Standards (§220-1005):

The proposed signage is illustrated and contained within the Sign Plan and the building elevation drawings. The two monument sign illustrations were prepared by Purohit Architects. The wall signage for the car wash building was prepared by Signs & Graphics Inc. The wall signage for the convenience store/restaurant building is illustrated on the building elevation drawings prepared by Purohit Architects. The signage for the gas station canopy is illustrated on the elevation drawings prepared by Purohit Architects.

Per a conversation with the applicant on January 9, 2026, all of the above signage as illustrated shall be used for building permit applications, with the exception of the wall signage on the convenience store/restaurant building. For the convenience store/restaurant building, the applicant has elected to install wall signs in accordance with the Village’s current sign regulations.

In general, any signs not illustrated referenced in the above plans, such as the menu sign for the restaurant, shall be installed in accordance with the Village’s current sign regulations.

Monument Signs

Double-sided multi-tenant monument signs are proposed at the southeast corner of the remnant of Lot 4 and also at the Bell Road entrance on Lot 1. As Lot 4 is an unimproved lot, typically an accessory sign would not be allowed by code. PUD’s often will allow a master signage plan to cover overall signage for the development. The applicant is requesting to allow for a multi-tenant monument sign on remnant of Lot 4 (see PUD Exception) and also to reduce the setback for that monument sign from five (5) feet to one (1) foot which, according to the applicant, may be needed for utility configuration (see PUD Exception). Other than those exceptions, both monument signs meet all other sign code regulations.

Note that only Mobil Gas, Platinum Car Wash, and Scali's Beef are included as tenant panels on both monument signs. Breezy's Convenience Store is not on either of the monument signs, but it does have a wall sign.

Wall Signs

The applicant is requesting an exception to allow an additional wall sign on both the north and south elevations of the car wash (see PUD Exception). The aggregate copy area of the two signs complies with the maximum allowable copy area for each elevation.

The applicant is also requesting an exception to increase the wall sign copy area for east elevation of the car wash: from forty-three (43) square feet to seventy-six (76) square feet. This elevation is narrow which has a direct impact on allowable copy area (see PUD Exception). The wall signs have good proportions with the architecture proposed and do not appear excessive. Other than the exceptions listed above, all wall signs proposed meet all other sign code regulations.

Business Operations & Hours of Operation:

§220-711(A)(11) of the Zoning Ordinance states that "*Non-residential uses may be open for business between the hours of 6 am – 11 pm. Establishments seeking twenty-four-hour operation must be approved as a special use...*" As noted on page 2 of this report, the gas station pumps and the convenience store would be open a full 24-hours per day, requiring a special use permit. Scali's Beef restaurant is currently proposed to be open from 11:30 am – 11 pm, but could potentially be open until midnight. Scali's Beef could be added to the same special use permit for operating *outside* of the 6 am – 11 pm window. All of these uses are located on Lot 2.

On Lot 1, the car wash is proposed to be open from 7 am – 10 pm, within regular business hours. If the applicant has any future plans for operating the car wash earlier than 6 am or later than 11 pm, it should be discussed at this public hearing so that it can be clear for the record that operating outside of the 6 am – 11 pm window is permitted.

Drive-Through Use: A drive-through is proposed for Scali's Beef (which requires a Special Use Permit) along the west and south sides of the convenient store building. The stacking spaces for queuing of cars is sufficient as proposed. There is a by-pass lane provided along the outside of the drive-through. The drive-through lane is sited in a way to reduce the visibility from Bell Road.

Conformance with Other Village Regulations

Exterior Construction Standards (Chapter 75, Article II): The Exterior Construction Standards do apply to this request as this is a new commercial building. The applicant is proposing all masonry construction on the car wash and the restaurant/convenient store building which complies with the Village's code.

Outdoor Lighting (Chapter 75, Article II): The Outdoor Lighting regulations do apply to this request. A Photometric Plan and lighting details have been submitted and were found to be in compliance by the Village's lighting consultant.

Conservation Design (Chapter 107, Article IV): The Conservation Design regulations do not apply to this request because the applicant has not proposed the development of a residential subdivision.

Tree Preservation (Chapter 107, Article III): The Tree Preservation regulations do not apply to this request as the applicant has not requested the approval of a Preliminary Plat of Subdivision or proposed the division of the subject property, and the subject property is less than five (5) acres in size. This site is predominantly grassed and was mass-graded as part of the original PUD.

Subdivision & Stormwater (Chapter 138, Article I): The Subdivision regulations do not apply to this request as the property was formerly subdivided and the Final Plat was recorded in January 2025. The Stormwater regulations do apply to this request. Final engineering was reviewed after the original approval in 2023. If there are any minor changes to the Final Engineering Plans in the future, they should be easily addressed by the Village Engineer.

Park Donation (Chapter 138, Article II): The Park Donation regulations do not apply to this request as these regulations only apply when the final Plat for a residential subdivision or development has been approved.

Fire District: The Fire District reviewed the plans in 2023 and had no comments on the proposed plans. Applicant provided a Firetruck Circulation Plan.

Conformance with Adopted Plans

Comprehensive Land Use Plan: The Comprehensive Plan designates the subject property as *Commercial*. The gas station, car wash, convenience store, and restaurant uses are consistent with the land use designation in the Comprehensive Plan from 2005.

Traffic/Transportation Plan: The applicant's proposal and requested Special Use Permits conform to the regulations and recommendations set forth within the Village of Homer Glen Transportation Plan. The proposed site plan features two driveway connections, one from the private access road to the south of Lot 4 within the Menards PUD and one from an established ingress/egress onto Bell Road.

Village staff and consulting engineers have reviewed the auto-turn circulation documents and determined they are sufficient. The applicant has provided several auto-turn exhibits for delivery trucks, fire trucks and fueling trucks. One of the auto-turn exhibits illustrate that a semi-truck *cannot* circumnavigate the site, nor is this a requirement per code.

The traffic consultant recommended a number on onsite signage recommendations to help with internal circulation. Based on the traffic consultant review, there is a recommendation for a condition on the PUD for the operator to take precautions with regard to car wash traffic during peak season. The condition includes but is not limited to having additional staff manage traffic, controlling hours of operation, controlling car movements with signage (see PUD Condition #3).

Motions for Consideration

SUP for Major Change to PUD

1. Is there a motion to recommend _____ (approval / approval with conditions / denial) of a Special Use Permit for a Major Change to the Menards Planned Unit Development (PUD) with requested exceptions [list exceptions], in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

SUP for Car Wash

2. Is there a motion to recommend _____ (approval /approval with conditions / denial) of a Special Use Permit for a car wash (*classified as an automobile repair, service and body shop use*), in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

SUP for Drive-Through

3. Is there a motion to recommend _____ (approval / approval with conditions / denial) of a Special Use Permit for a drive-through establishment, in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

SUP for a Gas Station

4. Is there a motion to recommend _____ (approval / approval with conditions / denial) of a Special Use Permit for an automobile service station (*gas station*), in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

SUP for 24-hours operation

5. Is there a motion to recommend _____ (approval / approval with conditions / denial) of a Special Use Permit for 24-hour operation, in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

Site Improvement Plans

6. Is there a motion to recommend _____ (approval / approval with conditions / denial) of Site Improvement Plans for a unified development, in accordance with the reviewed plans, public testimony and staff report, on certain real property generally located at 13812 S. Bell Road and 13830 S. Bell Road, Homer Glen, Illinois, PINs: 16-05-02-213-002-0000, 16-05-02-213-005-0000, & 16-05-02-213-004-0000?

Findings Motion for Consideration:

Is there a motion to adopt applicant's findings as the findings of the Plan Commission?

PUD Exceptions: Approve such exceptions, variances, departures, and deviations as may be necessary and appropriate to develop the subject property as depicted in the Final Engineering Plan and Plat of Re-subdivision submitted herewith, including the following:

1. Reduce the Required Landscape Setback along S. Bell Road from the required 30' to 20'. (Zoning Ordinance, Table 2B, Site and Structure Bulk Requirements)
2. Reduce the south side yard setback for the car wash from 20' to 17.2' for Lot 1. (Zoning Ordinance, Table 2B, Site and Structure Bulk Requirements)
3. Reduce the north side yard setback for the restaurant/convenient store from 20' to 12.6' feet for Lot 2. (Zoning Ordinance, Table 2B, Site and Structure Bulk Requirements)
4. Reduce the parking lot/drive aisle setback from 5' to 0' along the property line between Lots 1 and 2. (§220-1002(C)(3))
5. Increase the accessory structures height from 15' to 19.2' for the pay canopy for the car wash and from 15' to 17' for the gas pump canopy. (Zoning Ordinance, Table 2B, Site and Structure Bulk Requirements)
6. Increase the lot coverage/impervious coverage for Lot 2 from 65% to 69.7%. (Zoning Ordinance, Table 2B, Site and Structure Bulk Requirements)
7. Reduce the parking lot stall depth from 19' to 18.5'. (§220-1002(F))
8. Allow 2 wall signs on both the north and south walls of the car wash, whereas only 1 is permitted on each wall. (§220-1005)
9. Increase wall sign copy area for east elevation of Car Wash from 43 square feet to 76 square feet. (§220-1005)
10. Allow an off-site sign, the multi-tenant monument sign, on the remnant of Lot 4. (§220-1005)

11. Reduce the setback for the monument sign on the remnant of Lot 4 from 5' to 1'. (§220-1005)
12. Allow 2 wall signs on the east side of the gas station pump canopy, whereas only 1 wall sign is permitted. (§220-1005)

PUD Conditions:

1. Meet all applicable building code requirements and apply for all applicable building permits.
2. Meet all applicable engineering codes and requirements for Final Engineering.
3. The car wash special use is approved on the condition that the operator will take precautions necessary during the peak season to avoid hazards in the access road or blocking of the access road, including one of more or the following: additional staff will be used to help direct and manage the traffic flow through the site; a staff person will be placed at the internal intersection west of Bell Road to help direct traffic; electing the most efficient time for vacuuming (i.e. before or after a car wash; and/or traffic exiting the car wash will be prohibited from making a left turn to (1) exit the car wash site via the right-turn in/right-turn out access drive or (2) enter the vacuum stations. The operator shall also report to staff upon inquiry concerning the peak season operations and any of the precautions noted above.
4. To provide masonry veneer on the gas pump canopy columns. Masonry veneer shall consist of brick or stone of the variety used on the primary building.
5. Record a Shared Parking Agreement between Lot 1 and Lot 2.
6. Signage on the building on Lot 2 shall comply with current Village sign regulations.

Attachments

1. Project Description, received 1.7.26
2. Market & Operations Analysis, received 12.11.25
3. Project History, prepared by staff
4. Zoning Map with Aerial Image, prepared by staff
5. Recorded Plat of Subdivision, illustrating the former and current lot layout, prepared by staff
6. Standards for Major Change to PUD, prepared by applicant 1.7.25
7. Standards for Special Use Permits, prepared by applicant 1.7.25
8. Plan Commission meeting minutes excerpt, September 21, 2023
9. Village Board meeting minutes excerpt, October 11, 2023
10. Environmental Impact Statement, received 12.11.25
11. Traffic Study prepared by KLOA, received 8.30.23
12. Proposed menu for Scali's Beef
13. 3D Renderings of buildings, prepared by applicant
14. Final Engineering Plans, received 11.25.25
15. Autoturn Exhibits for Fire Department, Delivery Vehicles and Fueling Trucks, received 9.13.23
16. Floorplans for Convenience Store and Car Wash buildings, received 7.26.23
17. Building Elevations for Convenience Store and Car Wash, received 7.26.23
18. Elevations for Gas Station Pump Canopy, received 1.9.26
19. Sign Plan, received 11.25.25
20. Photometric Plan, received 1.7.26

NSS GROUP REAUTHORIZATION OF SPECIAL USES, INCLUDING
MAJOR AMENDMENT TO MENARDS PLANNED UNIT DEVELOPMENT
13812/13830 S. BELL ROAD (PINS 16-05-02-213-002, -005)
LOTS 1-2, RESUBDIVISION OF LOT 5 IN MENARDS

PROJECT DESCRIPTION (JANUARY 7, 2026 REVISION)

Applicant NSS GROUP submits this further revised narrative in support of its application for special use permits, including an amended planned unit development following multiple communications with staff.¹ First, Applicant submitted its *final* engineering with its current zoning application along with plans relating to the last approval since work towards permits led to substantial civil review and all that essentially remained was building permitting. Second, there has always been approved off-site signage reflected in the still-recorded Plat of Resubdivision of Lot 5 in Menards Subdivision, but staff perceived it to be safer to apply a belt-and-suspenders approach and add a further planned unit development approval for the shared off-site signage and to an extent treat area outside of the proposed planned unit development as governed by the Applicant's plan for planned unit development. Third, with periodic discussion of whether Applicant intended 24-hour operations, Applicant learned that staff's interpretation of the "24-hour" regulation amounted to any business open before 6 AM and after 11 PM (17 hours maximum), so Applicant has now added a request for a special use permit to operate 24 hours (it has also decided to avoid any ambiguity and pursue operations 24 hours each day under the more common understanding of the term. Fourth, Applicant does not pursue variations from the Village's lighting standards, but a disagreement having no impact on the zoning hearing was eventually resolved with staff concluding that the narrow strip of land north of the Menards Subdivision has been zoning C-2 since annexation on June 25, 2002.

None of these issues were ignored in any prior application, and all were discussed and considered previously. With staff changes came different viewpoints, none of which remain the subject of further investigation. Staff still desires precise placement of the shared sign on the remainder of Lot 4 even though the location approved is depicted in the plat of subdivision that created Lots 1 and 2 in the Resubdivision of Lot 5 in Menards Resubdivision. Applicant counsels against this due to utilities, sidewalk planning, and planning for the best sight triangles at the main entrance—and doing so in a fashion that Menards approves. Applicant also notes that it is not advisable to have a planned unit development governing Lot 1, Lot 2, and part of the remainder of Lot 4 because, however Menard, Inc. decides to utilize the remainder of Lot 4, it is best not to have overlapping planned unit development plans and the sign easement can best be managed as a recorded easement—as it has been and as it remains.

As Applicant informed staff and the Village Board, the uncertainties from moratoria and the direction of licensing regulation in the areas of gaming, liquor, and tobacco led to a suspension of a project that was intended to the open coming out of the pandemic. This current

¹ The Commission and Village Board will recall the timing and events related to the two prior approved entitlements. Not only did staff vet the project with Village consultants and counsel each time, but Applicant also worked closely with Menard, Inc. which has had its real estate and legal divisions involved in the matter. The new staff reviews and satisfactory prior reviews by staff, the Village and its consultants again confirm a solid project and offer confidence in the depth of review on this occasion as well.

application seeks to re-entitle the same project approved previously under circumstances where the delay tied only to technical regulatory issues. Notably, Applicant relied on details provided by the Village in its initial development pre-application meetings when it pursued a car wash and multi-tenant retail and restaurant center. The predictions of success in marketing the site bore no fruit, even to the point of (a) attempting to design commercial use at two elevations on part of Lot 4 and (b) attempting an A&W restaurant (with a drive through) smaller than normally approved by the franchisor. Still, Applicant remains committed to the Property and to engaging in business in Homer Glen. It is hoped that comments such as those made at the opening of the hearing in December 2025 and positions that, presumably, prior staff and counsel must have missed legal components of the development, submitted flawed plans or ignored the Dark Skies regulations will end and this matter will proceed to hearing and a favorable outcome so that the Village can see development of the Property beginning in early Spring 2026.

The above background was necessary so that no one loses sight of the context of this hearing and the depth of review of plans on four occasions—the latter review occurring with at least two new staff members who dove quite deeply into the project to apply their expertise. Although the direction of some of the recent communications about the project caused concern, the ultimate outcome is that Village staff and its consulting engineer should have no concerns about the development, building design and on-site or off-site impacts. Applicant and Menard, Inc. appreciate the time spent by staff and staff's willingness to gain the same intimate knowledge of the project as their predecessors and Applicant looks forward to dealing with the Village going forward.

The project will benefit the Village, its residents, and the uses in the northern stretch of Bell Road. The further amended planned development will allow a 24-hour automobile service station and convenience store, a restaurant, and a modern car wash on Lot 5, situated slightly northeast of the primary east wall of the Menards store. Menard Inc. supports the project, and the parties have recorded an easement agreement concerning various matters that will apply going forward. The most recent two-lot subdivision (filed 2025) intended this development, including the shared freestanding signs.

The Village annexed Menards Subdivision pursuant to agreement in 2002. As noted above, Menard Subdivision is one roughly 20-foot wide strip of land (on its north end) smaller than the annexed territory that was to be zoned C-2. Development of the Menards soon followed. All lots in Menards Subdivision are zoned C-2. The narrow strip to the north is zoned C-2. The subdivision currently hosts Menards, Arby's on Lot 3 (replacing a bank with drive-through operations) and a Firestone tire and service center on part of the original Lot 2. Areas remaining for development include the remaining portion of Lot 2 south of the Firestone and Lot 4. Lot 4 had limited potential for development due to access needs and severe changes in elevation.

Applicant, as the owner of Lot 5 and what was previously the north 100 feet of Lot 4, resubdivided to create Lots 1-2 in the resubdivision of Lot 5. A copy of this plat is provided with the application. The car wash will be on northerly Lot 1 and the gas station and convenience store building with the drive-through restaurant will be on southerly Lot 2. The Village approved the resubdivision following some discussion related to the use of former Lot 4 for a small-format A&W franchise, but it was determined that the reasonably developable portion of Lot 4 was best

suited for use by occupants of neighboring parcels due to the two tiers of developable space, the cost involved, and design impediments to use of Lot 4 by a single user. A&W would not fit on Lot 4 even with a reduction in footprint. There is still a sign easement on Lot 4 (southeast corner), and Applicant and Menards will use this area for one of the freestanding signs. (Menards still approves all signs.)

Applicant relies on existing storm water planning adjusted only for its work. Ingress and egress exists at the existing Bell Road restricted access abutting the subject property and full access at the signalized Bell Road intersection that is the main entrance. There is no vehicular interconnection between Menards Subdivision and land to the west. No such interconnection is possible between Applicant's tract and land to the west due to the detention facility west of the Property. Applicant will rely on existing, long-planned cross access through the interior of Menards Subdivision. It will construct the access route from the restricted north driveway across Lot 1 and Lot 2 (as well as part of the remainder of Lot 4) that connects with the signalized intersection with Glengary Drive. The access will be constructed to Village specifications as depicted in the full engineering set provided with the application.

Applicant will also construct a ground sign near the northwest corner of the intersection of Glengary Drive and Bell Road. There will be a ground sign on Lot 1 as well. The two ground signs will offer tenant panels for Applicant's uses (panels are planned for the gas station and restaurant as well as the car wash). The convenience store will rely on wall signage, but could have access to unutilized or underutilized ground sign panels in the future (this is dependent on Lot 4 use). The ground signs will be similar in design to one another, adequately spaced (well over the 300-foot minimum spacing required) and served by electric conduit appropriate for the sign plan. The signs are substantially the same as previously approved.

Applicant continues a plan similar to, but slightly larger than, its approved development of Lot 5 that excluded the north 100 feet of Lot 4. The north parcel (Lot 1) will host a modern tunnel car wash and related amenities. The south parcel (Lot 2) will host the service station, restaurant with a drive-through operation and convenience store. The service station with convenience store (likely) or the restaurant will offer video gaming. A shared central driveway will allow for vehicular traffic between the access road and the drive-through in the rear of the building. Stacking exceeds what is required by the Zoning Ordinance.

On Lot 1, the car wash will be situated on the south portion of the Property, have its entrance on the west elevation and discharge vehicles on the east elevation. Drivers will enter Lot 1 and, on most occasions, have a choice to vacuum before or after the car wash. On busier days, the owner may make this choice for customers based on traffic flows into and out of the car wash. An employee stationed on the north side of the intersection can determine whether this choice should be made for the customer at any given time.

Applicant will consider animal grooming (dog wash area) as a convenience to Homer Glen residents. This is a local amenity that could occur inside the northeast corner of the building. Typically, self-service animal grooming is an option at car washes only during non-peak times and most pet owners do not wash their dogs outside of the house (other than at a pet groomer) during winter months. The amenity involves a simple wash basin and hose, and there is

no staff. The area is simply available to customers in non-peak times if they want to wash their dog. The floor plan for the car wash does not reflect this use area, but if Applicant offers this amenity in Homer Glen, it would need to obtain appropriate building permits to do so, and it would be inconvenient to amend the PUD on a further occasion simply to add a component to the current storage room that would allow an individual to wash their dog. Due to the importance of vehicular movement on site, Applicant will not allow use of the basin to wash pets during busy periods or between November 15 and March 31.

There are 22 planned vacuum stations (one is accessible). Vacuums are suspended from above the vehicle space under a sunshade that is slightly less than half the length of the stall. The sunshade allows more than 7.5 feet of clearance. Vacuums are part of a unified, closed system and the hand device is reinserted into a holder that eliminates the whistle from the vacuum that can be heard within a few feet of the car when vacuuming. Lighting under the sunshades is less intense than any property lighting and is essentially absorbed into the photometrics for the other site lighting.

The car wash will offer three pay stations with stacking for over 27 vehicles. Metered entry into the car wash is available via credit card, pass code or membership. Cars enter the stacking between the pay station and the car wash in a single-file line. A bypass lane from the stacking after the pay station is available for egress. The car wash itself can be controlled to slow or accelerate the conveyor. The car wash has an automated default detector that will slow or stop operations. An employee may also slow or stop operations. At the exit for the car wash there is another detection device that slows or stops the car wash if there is an obstruction at the exit. A ground heater prevents icing at the exit. Cars leaving the car wash may turn north or south.

With respect to interior traffic movements, Applicant plans for uncontrolled entry into Lot 1 (north) from Bell Road at a partially stop-controlled intersection. Parking was once approved north and south of the Bell Road restricted access point, but Applicant now plans to landscape this area, place the car wash parking at the southeast corner of Lot 1, and maintain accessible parking near the building. Parking on the east side of the access drive is planned where it was originally contemplated. The landscape setback is similar to landscaping east of parking in the southern portion of Menards Subdivision. Applicant has eliminated parking on the west line of the access road and replaced this with a landscape island and two access driveways.

The car wash has on-site management and employees while being part of a network of affiliated operations. While the typical employees on site will include a manager and one other employee, there will be times when only one employee is necessary. Similarly, there are occasional times when three employees would be engaged in car wash operations. During a brief period with thawing weather mixed with snowfall that occurs during a few weeks spanning February and March, car washes are in highest demand and will temporarily use more employees. The site will be developed in a fashion that will not require employees for traffic direction or for the collection of funds from consumers. The primary responsibility of employees on site will be consumer interaction and satisfaction, site cleanliness and site security. Typically, employees are stationed between the pay stations and the exit to the car wash to observe patrons. An employee will greet customers in the vacuum area, observe the pace of the conveyor and assist customers using pay machines.

Tasks will eventually be divided as the hours of operation progress and the number of employees will fall later in the day. The car wash may be open as early as 7:00 AM and close as late as 10:00 PM (demand may call for operation until 11:00 PM as allowed by code, but this would not be in the immediate future). Hours may vary seasonally and expand or contract in compliance with Village regulations. The ordinance approving the major change should not regulate car wash hours of operation.

On Lot 2 (the south lot, as expanded by the north 100 feet formerly part of Menards Lot 4), the building will have one or two tenants that manage their own tenant space. The 7,715 square foot building will operate in a unified fashion. The convenience store and gas station will operate 24-hours a day. Scali's Beef could be open from 11:00 AM until 11:00 PM. Scali Beef's operational hours will depend on catering, activity in the Bell Road corridor, and periodic assessments of demand. Many similar businesses are open until 11:30 PM or midnight. The special use for 24 hour operations will apply to Lot 2.

Applicant hopes to avoid an unnecessary full demising wall between Scali's Beef and the convenience store. The drive-through will stack in a standard fashion and there are plans for preview and full menu boards. Loading for the retail center is provided at the rear of the building, but the nature, size and timing of deliveries often allows loading in front of the building. In general, loading is a controlled function in this environment. Most deliveries will be scheduled for intake during non-peak hours and usually well before opening. Gas deliveries will occur east of the canopy. The fuel tanks will be situated in the east-central portion of Lot 2. Fuel deliveries can be restricted to hours that do not obstruct the drive through.

The loading zone has adequate accessibility and will not interfere with site traffic or parking. Several loading operations or deliveries for the use will occur from a parking space. The turn template provided with the application shows that gas and delivery trucks can readily enter and depart the site with no difficulty using the Glengary Drive intersection with Bell Road. However, Applicant has also provided a turn template that shows that the site is not designed for semi-trailers. This design was preferred during the prior hearing. If the Village's prerogative in this regard has changed, Applicant is willing to consider adjusting the site plan accordingly, but it does not intend to provide services to trucks with semi-trailers.

Lot 2 will benefit from an easement for parking on Lot 1. As a result of early, pre-application discussions (with economic development staff), the original proposed lot line carried through into the plat based on considerations of re-use of both properties if they were not always hosting these uses. Applicant plans for distinct waste enclosures, with one on each lot, but the original plan called for a shared enclosure at the west end of the shared lot line. Applicant proposes ample landscaping that meets the requirements of the Zoning Ordinance. Applicant seeks a collection of exceptions from the current regulations, but all exceptions relate to practical and efficient design efforts that present a better project following an approval of the major change. The new plan allows better efficiency along the access road and adds a landscape island on the west line of the access road. Although lot coverage ratios exceed Village standards, Applicant notes that Menard Subdivision was designed in this fashion and that the proposed excess lot coverage does not approach the lot coverage previously approved by the Village.

In every community where Applicant has affiliated operations (Applicant's owners have numerous sites in the Chicago area and in Florida), parking spaces shorter than 19 feet deep are allowed. Applicant meets the required 9-foot width. Applicant's attempt to meet the 19-foot depth requirement prior to application were difficult and came at the sacrifice of landscaping desired by staff and a loss of building area. Lastly, the Menards Subdivision contemplates an access road that is similar at the north and south ends of the overall development. With efforts to locate final plans with dimensioned parking stalls proving unsuccessful, Applicant was left to scale old drawings and believes spaces along the access road to the south are shorter than 19 feet. Menards produced what it believes are its initial civil drawings and these reflect parking spaces with a depth of 18 feet. The drive aisles are 24 feet wide or wider, and this width is sufficient for parking spaces with a depth of 18 feet. (Webster, McGrath, Ahlberg, revised March 12, 2004) Applicant provides for parking spaces with a depth of 18.5 feet.

It appears that the Village may have amended its maximum lot coverage after the Menards Subdivision approval (perhaps in 2009 and on one earlier occasion). It has been difficult to determine the scope of the prior lot coverage determinations in light of the limited availability of records and exhibits. Large developments often contemplate an evaluation of lot coverage throughout the overall development. In this instance, the Lot 1 lot coverage will be between 63.9% and 64.8% and the Lot 2 lot coverage is 68.9% and 69.7%. The overall lot coverage for Lot 1 and Lot 2 will be roughly 67.1 %. These figures do not include the 20-foot strip north of Lot 5 that extends the full depth of Menards Subdivision or the 6.2 acre Outlot A open space and detention area that obviously figure into the overall lot coverage in Menards Subdivision. The overall development area within Menards Subdivision is 40.97 acres. As a result, 15% of the overall open land is situated on Outlot A. The "uncovered" portion of the subdivision expands to more than 21.5% if open space in the Glengary Drive entrance features, around the Arby's and along the west and south sides of Menards are included. The southerly roughly eight acres remaining to be developed will provide open space as well. Excluding most of the eventual open space in the southern eight acres and on the remainder of the original Lot 4, Menards Subdivision has at least 23% open space. None of the developments along Bell Road north of 143rd Street offer visually apparent lot coverage of less than 80%. With this in mind, another analysis could exclude the southern eight acres from the denominator to evaluate lot coverage-meaning that the 6.2-acre Outlot A would be more than 17% of the open space and overall open space in the development would be roughly 27%. A highly visible portion of the remainder of Lot 4 will remain open due to the access road and grades. More open space surrounds the development than on other commercial tracts.

Applicant still needs additional wall signage and sign area. The sign area for the car wash was previously approved, and none of the circumstances related to the original approval have changed (Applicant provides this analysis elsewhere in the application). With respect to the service station signs, Applicant acknowledges that the request is substantial. However, awareness of the location of the pumps will benefit drivers attempting to reach them. Applicant believes it has met staff's preference to have reduced canopy signage, but the canopy signage is one that the gasoline brand determines. Staff has asked for a rendering with canopy signage in a departure from what was a prior understanding with the Village. Since the brand has not been determined, providing signage for zoning is premature. Monument signs were approved twice for this project

and there is an additional prior approval for a freestanding sign on Lot 4 where planned as part of this application. Since 2002, the Village has authorized the location of the south ground sign situated at the northwest corner of the intersection of Bell Road at Glengary Drive. Menards still has control over signs under the plat and under the covenants, and it has approved the signs and panels. A gas station also requires a larger number of signs for pumps, directions, instructions, and trade dress.

Dated: January 7, 2026

Respectfully submitted,

DANIEL LAW OFFICE, P.C.

By: _____
Mark W. Daniel
Applicant's Attorney

**NSS GROUP REAUTHORIZATION OF SPECIAL USES, INCLUDING
MAJOR AMENDMENT TO MENARDS PLANNED UNIT DEVELOPMENT
13812/13830 S. BELL ROAD (PINS 16-05-02-213-002, -005)
LOTS 1-2, RESUBDIVISION OF LOT 5 IN MENARDS**

MARKET AND OPERATIONS ANALYSIS (DECEMBER 11, 2025 REVISION)

The planned development will allow a gasoline service station, convenience store and restaurant on Lot 2 with a modern car wash on Lot 1, situated slightly northeast of the primary northeast corner of Menards. Following application for an earlier revision in December 2022, Applicant contracted to acquire the north 100 feet of Lot 4 in order to provide more proximate parking, including commercial vehicle parking. Applicant provides for some excess parking since demand for EV charging stations is increasing, but the demand is not yet to the point where the charging stations are economically feasible in this market without local participation. State and federal EV programs exclude the Homer Glen market area. Applicant has resubdivided to pursue the project approved under Ordinance No. 23-062. Market and operations have not changed for planning purposes since 2022 and 2023.

To the extent that Applicant refers to its sales or sales tax and other benefits, it has provided these figures to Village staff and it will address or re-visit them as needed. The reauthorization relies on these same figures. Menards continues to support the project. The majority of Menards' customers are destination shoppers who have choices in the area. Menards is one of two large retailers of home and construction goods on Bell Road. Bell Road is just outside Cook County and situated well to allow easy shopping opportunity to those in Cook County who avoid Cook County taxes on significant purchases and, more often, smaller purchases. Menards offers plenty of products that are convenience items as well. There are several gasoline stations, convenience stores and car washes near home, hardware and lumber centers (two of Applicant's affiliated car washes and one gas station and convenience store are adjacent to Home Depot stores) and their customers benefit from the co-location of these uses. Notably, there is countless anecdotal evidence (bags in cars, staff questions) that car wash customers are also drawn to the larger adjacent retailers.

As has occurred on the east side of Bell Road and south of the subject property along the west side of Bell Road, commercial uses and restaurants typically share a retail and service synergy with large retailers. One of the secondary reasons for drawing in large retailers is to encourage landowners to invest in the creation of space for new businesses of all types, for new employment and for new offerings in the Village. In light of the Firestone location, it is a bit unclear what will eventually develop on the southernmost parcel in the subdivision, but even this lot would benefit from a visibility standpoint with an increase in internal access road usage.

The remaining outlots in the Menards Subdivision are some of the last Bell Road corridor lots to develop. They have remained vacant relatively longer than other outlots in comparison to the time of the related or adjacent initial larger retail development. Whether this is a function of access or the direction of traffic at key times of consumer activity, the reality is that adding the car wash, gasoline sales, retail and a restaurant to the development will positively impact the number of vehicles entering the Menards Subdivision and several of the entering vehicles will

use the main entrance to exit-leaving them a more direct opportunity to shop at Menards or to eat at Arby's or another restaurant. Various factors could have affected interest in the outlots near Menards from 2002 until the present, but, since 2015, less demand for large brick and mortar retailers has had a dramatic impact on the secondary benefits available to smaller goods and service retailers. Those developing sites in this environment are contributing greatly in a changed retail environment and they typically are going to rely heavily on tenant rental income more than their own sales (note the turnover observed for secondary retail sites in Downers Grove near Lombard's Yorktown shopping center for an example in a heavily populated area). In this instance, one driving factor in the proposal is the Applicant's investment in the uses on the subject property through affiliated operators. In 2025, the undersigned also entitled a car wash as a redevelopment component of a 120,000 square foot shopping center in Downers Grove (one Applicant did not pursue due, in part, to Homer Glen commitments).

The proposed car wash, gas station with retail, and restaurant at the subject property will more appropriately convert a vacant and historically underutilized property to a modern use. The planned gas and retail operation with a modern car wash is not available in this part of Homer Glen. The only use offering similar services is a truck stop serving much larger vehicles near the intersection of 159th Street and I-355 which has a service area that extends far north, south and west of Homer Glen and would not overlap with the main service area for the proposed use. Additionally, the uses serve different corridors in town. The existing gas station and car wash will continue serving substantial numbers of commuters and areas in west and south Homer Glen as well as surrounding areas. The proposed gas station and car wash will serve commuters as well as persons shopping or seeking services in the Bell Road corridor. The proposed use will also serve north and east Homer Glen and surrounding areas extending into Cook County.

The proposed uses along a significant transportation corridor in Homer Glen benefit residents, employees, and those passing through the Village. The transportation network includes Bell Road as one of the most significant corridors for residents and visitors. The location thrives more on the average daily traffic than it does on the number of residents or rooftops within any radius of the Subject Property. There are hundreds of residences within a one-mile radius and the 1.5-mile radius relied on for retail considerations. The population within ten minutes is substantial.

The area surrounding this part of Bell Road has the highest population density in the Village. Several sources note that Homer Glen has a population of roughly 24,000 people. Located near three interstate highways and having two interchanges on I-355, Homer Glen offers its transportation network to thousands from outside the Village every day. The Homer Glen trade area extends through most of Orland Park and into Lemont, Tinley Park and New Lenox. The level of available discretionary spending in the area is substantial (one of the reasons why EV charging grants remain unavailable for this stretch of Bell Road).

The 2005 Plan repeatedly notes the need for additional areas for a diversity of business along Bell Road. Research relating to the once contemplated update to the 2005 Plan touches on revenue leakages to other communities, and there is noted leakage in the fields of automotive and restaurant uses. The proposed use will reduce leakage. Even though the Village found that it did not need to amend the 2005 Plan, the data evaluated remains valuable.

Applicant will not recite all economic data available, but it is notable that over 200,000 people live within 10 minutes of the site and the daytime population of the area is over 195,000 people. These figures support for an expectation of success at the subject property for all operations. Applicant has studied this location for longer than it wished due to the fluctuating economics and changes in interest rates. It is comfortable with the site as a location for its related businesses. More importantly, Applicant's work flow now allows for construction starting in early Spring 2026.

APPLICANT'S POSITION IN THE MARKET AND AS A PROPERTY OWNER

Applicant's affiliates operate service stations, similar convenience stores, and car washes throughout the Chicago metropolitan area from Evanston south through Skokie and Chicago, west to Oak Brook and the St. Charles area, and south to roughly 95th Street. The members of the original applicant, the operator (Pakitalia comprised of Rick Scali and Mohammad Yaqoob), and the owner of NSS Group, Inc. (Mohammad Yaqoob) have owned and operated several gas stations, car washes, restaurants, and convenience stores. Scali's Beef bears the name of Rick Scali. Notably, Mohammad and Rick are involved in the development of shopping centers (one ongoing project being a redevelopment larger than 110,000 square feet), so they understand and remain respectful of the needs of owners and businesses in unified developments. Applicant is self-sufficient from a financial perspective. It has considered that the subject property should host automotive uses for a few years and it is now the owner of Lots 1-2. Applicant has regularly secured financing to build projects when the retail portion will have at least one restaurant with alcohol service and video gaming terminals (this was the intent of state legislation on the subject). It is also familiar with truck stop establishments.

Applicant understands that the Village prefers restaurants before allowing gaming, but the project could allow for gaming at all hours under state regulations without having a truck stop like the one near I-355 noted above. Applicant pursues the revision to the planned unit development knowledgeable of comments made during the last hearing process, but it only indicates this flexibility so that the Village is aware that increased local revenues are possible after beer and wine sales stop. Applicant's projected revenues are based on restaurant use and gaming only during hours of operation allowed under applicable liquor licensing.

Applicant intends to construct the retail building at the same time as the car wash. Applicant has sufficient capital to proceed with the project in a fashion that is responsible, profitable and meaningful to the Village. Applicant remains willing to share relevant background with staff and reviewing commissions or committees and the Village Board. Applicant does not include personal or corporate financial data in market statements. Applicant is confident from the operation of multiple car washes, gas stations, restaurants and convenience stores in the area that demand for the uses in the Village remains strong. Similarly, Applicant has experience with gaming and licensed liquor sales at service station convenience stores.

The person behind Scali's Beef has been a longtime resident of the area and lives in Lemont. Rick has substantial awareness of the successes and failures along Bell Road and has determined that the development will benefit the area. The members have a history of project

management and development together, ranging from rehabilitation and re-use to new development of gas stations, car washes, retail locations and restaurants. The members are in the process of developing other approved projects at multiple locations.

MANAGEMENT AND SITE OPERATION

Applicant will continue the two-lot planned development which will be under unified ownership as currently planned. The lots will be capable of separate ownership but Applicant intends to continue in ownership of both lots. Menards approves of the development and proposed uses and it has not raised any issue of use limitations. The declarations and covenants applicable to Menards Subdivision remain in force and will be adjusted as needed to accommodate the development. Agreements concerning Lots 1, 2 and 4 will govern work and expenses relating to grading, drainage, the access road, access road lighting and signage. An easement will allow reciprocal access, parking and operation of the driveway between Lots 1-2 if these see distinct ownership.

The car wash may be open as early as 7:00 AM and close as late as 9:00 PM. Hours may vary seasonally. The gas station and convenience store will open earlier and close later. If area trends develop and local ordinance allows, some operations could expand to 24 hours. The restaurant area will offer a drive-through, catering and dine-in options as well as gaming in a format that will meet local concerns. Over time, the car wash may allow a wash basin for pets as an amenity to residents when the car wash is not busy during non-winter months.

LOCATION AND ECONOMICS

The Bell Road corridor draws in significant volumes of traffic for commercial, recreation and residential purposes. Its location and connectivity among major employment hubs from Chicago to Joliet and areas in between indicates that commuter numbers are consistently higher than average, and the area population is a strong sign of positive demand in the gasoline service station and convenience store market. The uses proposed for the subject property are meaningfully different from the uses elsewhere in the service area inasmuch as:

1. The car wash is the sole business on one site and the sole purpose of the proprietor is to offer a modern, long-tunnel car wash in the area with more stacking and vacuum opportunity;
2. The development will offer a single stop for consumers with multiple automotive needs;
3. The convenience store and Scali's Beef will offer consumers items and services that are either not available at all or which are in more demand than met by supply;
4. The benefits of the car wash allow convenience and conservation of resources; and
5. The service station offers a convenient stop for gas and convenience items on a larger lot than currently exists in the corridor, and it does so in an area of substantial traffic.

There is no other similar collection of uses nearby or within the same corridor. The Gas N Wash is over six miles away and it serves semi-tractors and trailers when no trucks with semitrailers are intended to be served at the property. The south side of Homer Glen has

reasonable access to two car washes along 159th Street at Gougar Road and at Wolf Road, the latter of which is in Orland Park.

CAR WASH CONSIDERATIONS

According to the International Car Wash Association, actual use of car washes increased nearly 20% between 1996 and 2014 (19.2%). Since then, trends towards increased use continued despite the pandemic and they continue today. Modern car washes have been valuable commercial uses near auto service stations, retail areas and areas with significant through traffic. For reasons ranging from convenience, yard maintenance, water usage and weather, the volume of consumers who wash their cars at home has dropped from nearly 50% to 28% over a similar period. 72% of Americans use professional car wash services regularly. Industry experts see demand growing 4-5% annually until at least 2028. Over 60% of consumers use automated tunnel-style car washes such as the car wash proposed in this case.

Large stretches of north Homer Glen and territory north of Lot 1 outside of Homer Glen do not have a car wash despite ample demand. Other area car washes are reflected in the following table:

<u>Address</u>	<u>Distance to Subject</u>	<u>Name</u>	<u>Notes</u>
12819 W 143rd St.	0.62 miles	Shell Gas	w/ short car wash
12911 W 143rd St.	0.79 miles	Softway Oil	det w/ car wash
12502 W 143rd St.	0.96 miles	Speedway Gas	w/ short car wash
15060 S Bell Road	1.62 miles	Speedway Gas	w/ short car wash
15861 S Bell Road	2.84 miles	Classic Express Bay	w/ med. tunnel
15551 W 143rd St.	4.05 miles	Speedway Gas	w/ short car wash
15819 Wolf (0. Pk.)	4.68 miles	Tommy's Express	modern tunnel
15930 W 159th St.	6.58 miles	Gas N Wash Gas	modern tunnel

The site’s proximity to areas of Cook County such as Orland Park to the east offer a destination for cost-conscious consumers. As it stands, Cook County has at least a 6 cents per-gallon gas tax, the highest in the state, on top of an already high sales tax. Local consumers may feel inclined to make purchases available in this portion of the Village, which has a lower gas tax and a lower sales tax. There is sufficient demand and projected growth to allow a reasonable conclusion that a gas station on Lot 2 will not cannibalize other stations.

The greatest strength of the development is its location. Homer Glen is a well-populated area, home to more than 24,000 people, and tens of thousands of other commuters and visitors who use major area roadways. Additionally, service stations at intersections have faced some pressure with road expansion as Homer Glen grew, and this development will not face the same issues. It would not be appropriate to place this style of development at one of the Bell Road intersections due to the ebb and flow traffic and the related turn restrictions. Many area stations are at intersections, but these stations-though they will continue-will not offer the diversity of goods and services. The proposed development will not have a direct competitor, and the service stations to the south are on the opposite side of Bell Road or at intersections.

The addition of a quality gas station and convenience store with an adjacent car wash will further incentivize consumers to make purchases in Homer Glen. The same motorists will also gain familiarity with Menards, Arby's and Firestone as well as other businesses in and near this portion of Bell Road. The diversity of nearby offerings will cause the same motorists to return to the area to make purchases when desired.

There is a longstanding trend in the success of gas stations located adjacent to other places of retail, and the success is a two-way street. Costco, for example, often includes gas stations just next to their retail locations connected by a shared parking lot and has seen tremendous success in doing so. Sources indicate that Costco's on-site gas stations account for \$20 billion in sales annually, accounting for roughly 10% of the company's net annual sales. Notably, a large volume of these gasoline sales are to shoppers who have shopped or will shop at Costco. The same is true for Applicant's affiliated BP station, car wash and convenience store near Home Depot in Oakbrook Terrace which, although directly adjacent to a Shell and removed the depth of the Shell lot from the corner, is very successful and a strong addition to the City's retail community. A gas station that is located next to a retail establishment with a volume similar to a Menards would likely see similar two-way benefits. Applicant has also observed a tie between its customer flow at an affiliate's Evanston carwash that aligns with adjacent Home Depot activity. It is important to value this application to insert a retail/service mixed use at a time when many communities in the area do not have this same opportunity in or near a key retail corridor.

The gas station market shows no signs of declining any time soon, meaning the proposed gas station will likely be profitable for years to come. According to Globe Newswire, "[t]he global gasoline stations market size is then expected to reach \$3,150.1 billion in 2027 at a [Compound Annual Growth Rate] of 5%." Even with EV charging stations expanding, there are limits to EV services. The station could host EV chargers, but the demand and return do not yet justify such significant private investment. Homer Glen is not on any federal or state list of subsidized communities. Applicant will continue to keep an eye on opportunity here, but it is possible that large retailers will have the wherewithal to install EV charging before Homer Glen's service stations do.

The sole weakness at issue is the location of the development at an elevation significantly below the main entrance elevation. Over time, consumers will become familiar with the operations and the site. The grade change may prolong the initial customer growth curve. Opportunities arise in relation to traffic patterns and new homes being developed in the area. Additionally, a new station will benefit the community and allow a breadth of goods and services that consumers demand on a quick purchase timetable, without walking into, for example, the nearby Jewel. The gas station and car wash will offer services that encourage each use (price and membership opportunities). Additionally, the ability to place EV stations in the excess parking area allows an election of internalizing this use when the time is right or placing it in parking spaces on the main access drive in conjunction with the Village if it so desires. With respect to EV stations, Applicant would not want to compete with a large retailer at an inopportune time if its planning involves EV stations since the stations themselves can be a driver for retail sales.

The only threats to gas station operations lie in governmental regulation. Gas N Wash cannot be viewed as a threat since it is over six miles away and has a different market (one that includes trucks with semi-trailers). Regulatory or fiscal legislation could affect gasoline sales, but the public will continue to demand gasoline until such time as EV technology and vehicle production replaces the existing vast number of gas-powered vehicles.

GAS AND RETAIL CENTER

Applicant worked without success to fill the small retail or restaurant units and the large retail or restaurant unit. Interest in the site left building speculative with high interest rates as an unattractive option. Scali's Beef and the related gaming operation will be divided by distance but remain accessible from the convenience store. Several city and suburban locations no longer plan for a demising wall and distinct entrances. In the city and suburbs, it is increasingly more common to avoid divisions that ultimately serve as an impediment to consumers.

While licensing is not the issue under consideration during the zoning process, the Village should understand that Applicant will comply with applicable regulations governing restaurant licensees with video gaming terminals. Applicant's members have a good history not only with local regulatory agencies but also with the Illinois Gaming Board. In addition to the catering and drive-through operations, Scali's Beef will offer dining for a few dozen patrons. The minimum floor plan is intended to reflect that there is nothing in the site planning and zoning review that would indicate inability to comply with local regulations.

Applicant attempted to design a small A&W franchise into the development, but a modem A&W franchisee found the location challenging and the site too small (Lot 4 standing on its own). The plans provided with the application include a more detailed grading and access road plan.

LICENSING

The convenience store will require either a Class D Package Store license or a Class G Beer and Wine Package Sales license. Scali's beef will require either a Class A Restaurant with Service Bar license or a Class F Beer and Wine Retail Sales license, and it would appreciate issuance of a Class I Catering license if it establishes a demand for this type of license in connection with catering services. All employees of each business will be BASSETT trained and respect and comply with the regulations applicable under the Liquor Control Act of 1934 as well as the regulations applicable to premises with relevant licenses identified in Section 83-66 of the Village Code.

The sale of alcohol on premises will primarily relate to package sales. Typically, liquor consumption in the intended environment is relatively low. Not only does Scali's Beef not offer a bar or a place isolated from the remainder of the convenience store, but those engaged in video gaming do not consume a lot of beer or wine. Please note that the itemization of licenses noted above will most likely have Scali's Beef selling only beer and wine.

Operations within the planned development qualify under two defined categories of use where video gaming terminals are allowed. First, Lot 1 and Lot 2 qualify as a licensed truck stop establishment because the facility is three acres, has a convenience store, offers a separate diesel island for commercial vehicle fueling, allows for commercial motor vehicle parking and plans the sale of more than 10,000 gallons of diesel or biodiesel fuel each month. Second, Scali's Beef qualifies as a restaurant deriving 60% of its revenue from food and beverage sales over the term of any video gaming license issued. The Village requires a license to operate video gaming terminals for each type of establishment. Each establishment will comply with the regulations under Section 83-50, but the Village should be aware that the breadth of hours of operation for a licensed truck stop establishment extends to 24 hours. (Applicant does not plan 24-hour operations at this time.) Alcoholic beverages are not required to be sold when video gaming terminals are in use.

The convenience store will require a Class A tobacco license issued pursuant to section 83-16(B)(1) of the code of ordinances. Tobacco sales will occur from behind the main counter in the convenience store. All operations will respect the Villages rules and regulations under Chapter 83, Article III of the Village Code.

Dated: December 11, 2025

Respectfully submitted,

DANIEL LAW OFFICE, P.C.

By: 

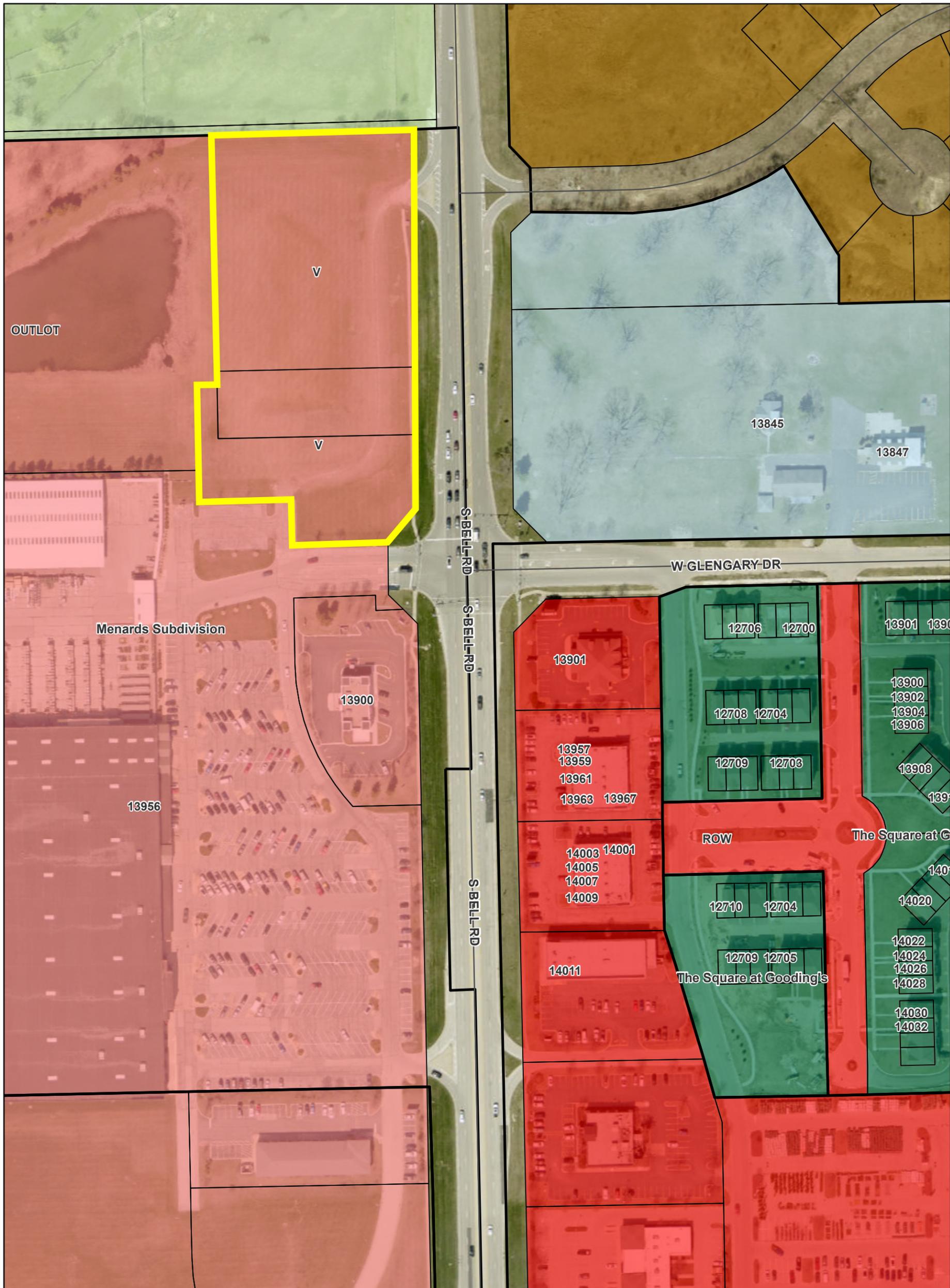
Mark W. Daniel
Applicant's Attorney

Menard's gas station/c-store/car wash – History

- 8.2.22: Plan Commission public hearing [HG-2211-PUD]:
 - Preliminary Plat of Subdivision
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash (an auto repair, service & body shop use)
 - Drive-thru establishment
 - Site Plan
- 9.14.22: Board [HG-2211-PUD]
 - **Approved Ord 22-038**
 - Preliminary Plat of Subdivision
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash (an auto repair, service & body shop use)
 - Drive-thru establishment
 - Site Plan
 - A variance from the exterior construction standards
- 9.13.23: Board [HG-2211-PUD]
 - **Approved Ord 23-052** (being an extension of Ord 22-038, one day before approvals expired under Ord 22-038):
 - Preliminary Plat of Subdivision
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash (an auto repair, service & body shop use)
 - Drive-thru establishment
 - Site Plan
 - A variance from the exterior construction standards
- 9.21.23: Plan Commission public hearing [HG-2301-PUD]
 - Preliminary Plat of Subdivision
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash (an auto repair, service & body shop use)
 - Drive-thru establishment
 - Gas station (new)
 - Site Plan
- 10.11.23: Board [HG-2301-PUD]
 - **Approved Ord 23-062:**

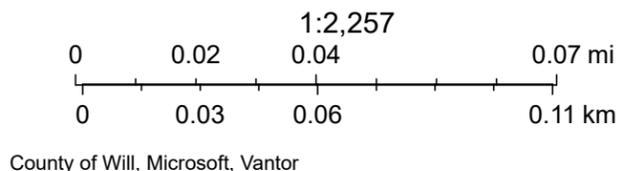
- Preliminary Plat of Subdivision
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash,
 - Drive-thru establishment
 - Gas station
 - Site Plan
- 5.8.24: Board [HG-2301-PUD]
 - **Approved Ord 24-025**
 - Final Plat of Re-Subdivision
- 12.11.24: Board [HG-2301-PUD]
 - **Approved Ord 24-063:**
 - Final Plat of Re-subdivision [HG-2301-PUD]
 - Previous final plat approval expired within 90-days. This re-approved the Final Plat because it was not recorded with the County within 90 days, and had expired.
- The approvals under Ord 23-062 [HG-2301-PUD] expired on October 11, 2024, and the 6-month grace period expired on April 11, 2025.
- 12.18.25: Plan Commission public hearing (because the former approvals expired)
 - SUP for Major Change to PUD
 - SUP's for:
 - Car wash (an auto repair, service & body shop use)
 - Drive-thru establishment
 - Gas station
 - 24 hours of operation
 - Site Plan
- 1.15.25: Plan Commission public hearing (kept open from 12.18.25 to re-notice and provide accurate documents)

Land Use Viewer of Lots 1 & 2 with a Remnant of Lot 4 of Menards PUD

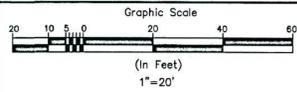


12/8/2025, 11:22:24 AM

-  Homer Glen Boundary
-  Street Centerlines
-  Subdivisions
-  Parcels- Will County
- Zoning Districts**
-  A-1 Agricultural
-  C-2 Local Business
-  C-3 General Business
-  E-1 Single-family Estate Residential
-  R-4 Single-family Residential
-  R-6A Attached Single-Family Residential District
-  Subject Property



Village of Homer Glen



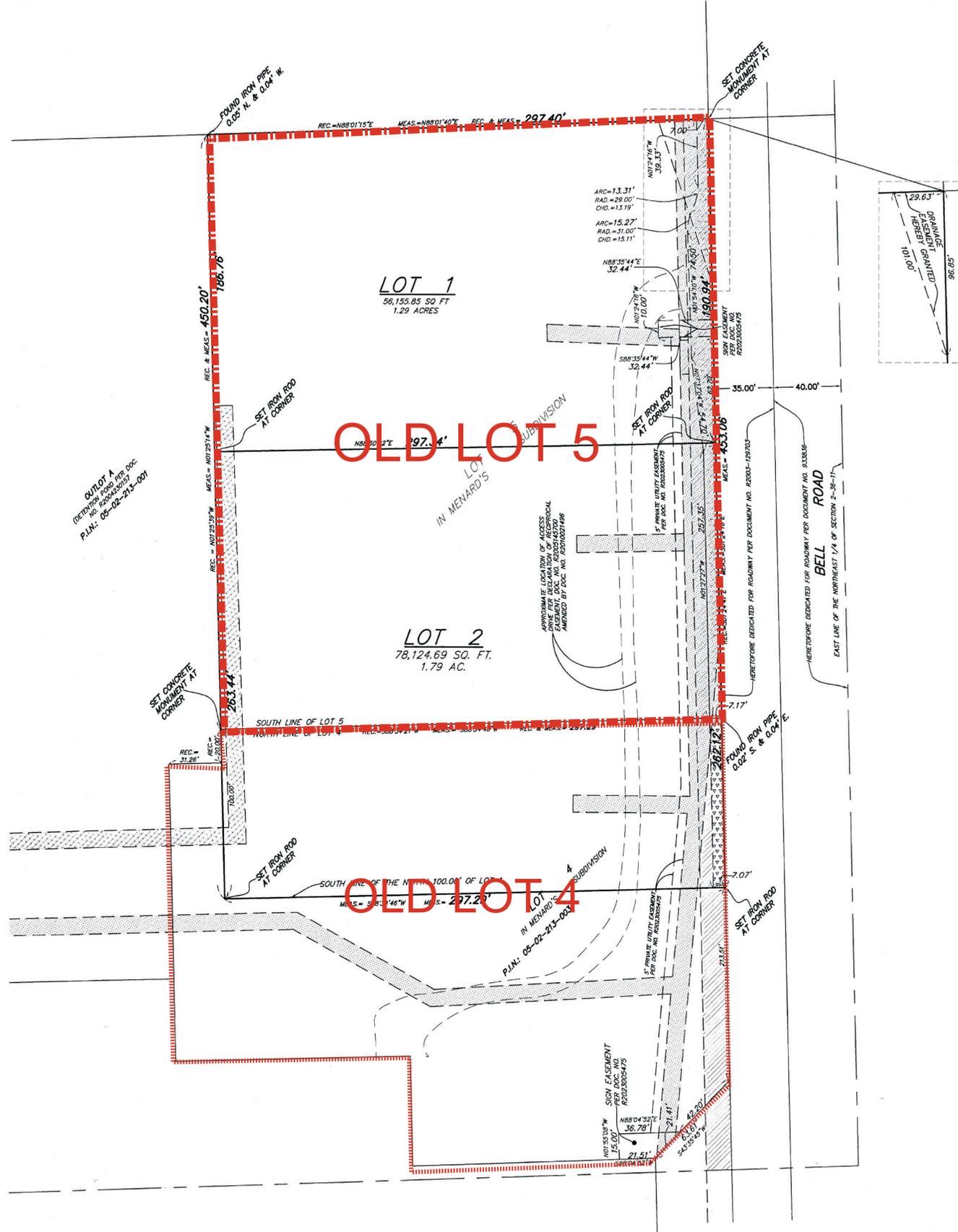
FINAL PLAT OF
RESUBDIVISION OF LOT 5 IN MENARD'S SUBDIVISION

BEING PART OF THE NORTHEAST QUARTER AND THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 36 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

P.I.N.: 05-02-213-002
-003

R2025004595
GARY A. STUNEL
WILL COUNTY RECORDER
RECORDED ON
01/26/2025 11:40:48 AM
R/C # 05-02-213-002
RENTAL # 05-02-213-002
PAGE 1

FORMER LOT LAYOUT



LEGEND

	COMMONWEALTH EDISON EASEMENT PER DOC. NO. R200306537
	EASEMENT FOR WATER MAIN PER DOCUMENT NO. R201000936
	EASEMENT FOR SANITARY SEWER MAIN PER DOCUMENT NO. R201000935
	SIDEWALK EASEMENT PER DOCUMENT NO. R202300475
	SIDEWALK EASEMENT HEREBY GRANTED

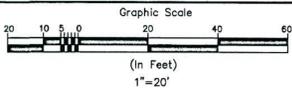
NO.	DATE	DESCRIPTION	BY
7	11/28/2024	REVISED PER EMAIL DATED 11/22/2024	MNG
6	05/24/2024	LEGAL DESCRIPTION & OWNER'S NAME REVISED.	MNG
5	03/15/2023	THE N. 100' OF LOT 4 IS INCLUDED.	MNG
4	08/07/2022	EASEMENT PROVISIONS ADDED	MNG
3	09/06/2022	DRAINAGE CERTIFICATE ADDED & PRELIMINARY CHANGED TO FINAL.	MNG
2	08/25/2022	5' PRIVATE UTILITY EASEMENT EXTENDED PER EMAIL DATED 08/24/2022	MNG
1	08/18/2022	SIDEWALK & DRAINAGE EASEMENTS ADDED	MNG

G GENTILE & ASSOCIATES, INC.
PROFESSIONAL LAND SURVEYORS
850 E. ST. CHARLES PLACE
LOMBARD, ILLINOIS 60148
PHONE (630) 916-6282

PLAT RETURN ADDRESS
SUBMITTED BY MAIL TO:
VILLAGE OF HOMER GLEN
14240 W. 151ST ST.
HOMER GLEN, IL 60491

PREPARED FOR: DANIEL LAW OFFICE, P.C.
DRAWN BY: MNG
ORDER NO.: 22-22484-RESUB-REV 7
SHEET 1 OF 2

ILLINOIS PROFESSIONAL DESIGN
FIRM LICENSE NO. 184.002870



FINAL PLAT OF RESUBDIVISION OF LOT 5 IN MENARD'S SUBDIVISION

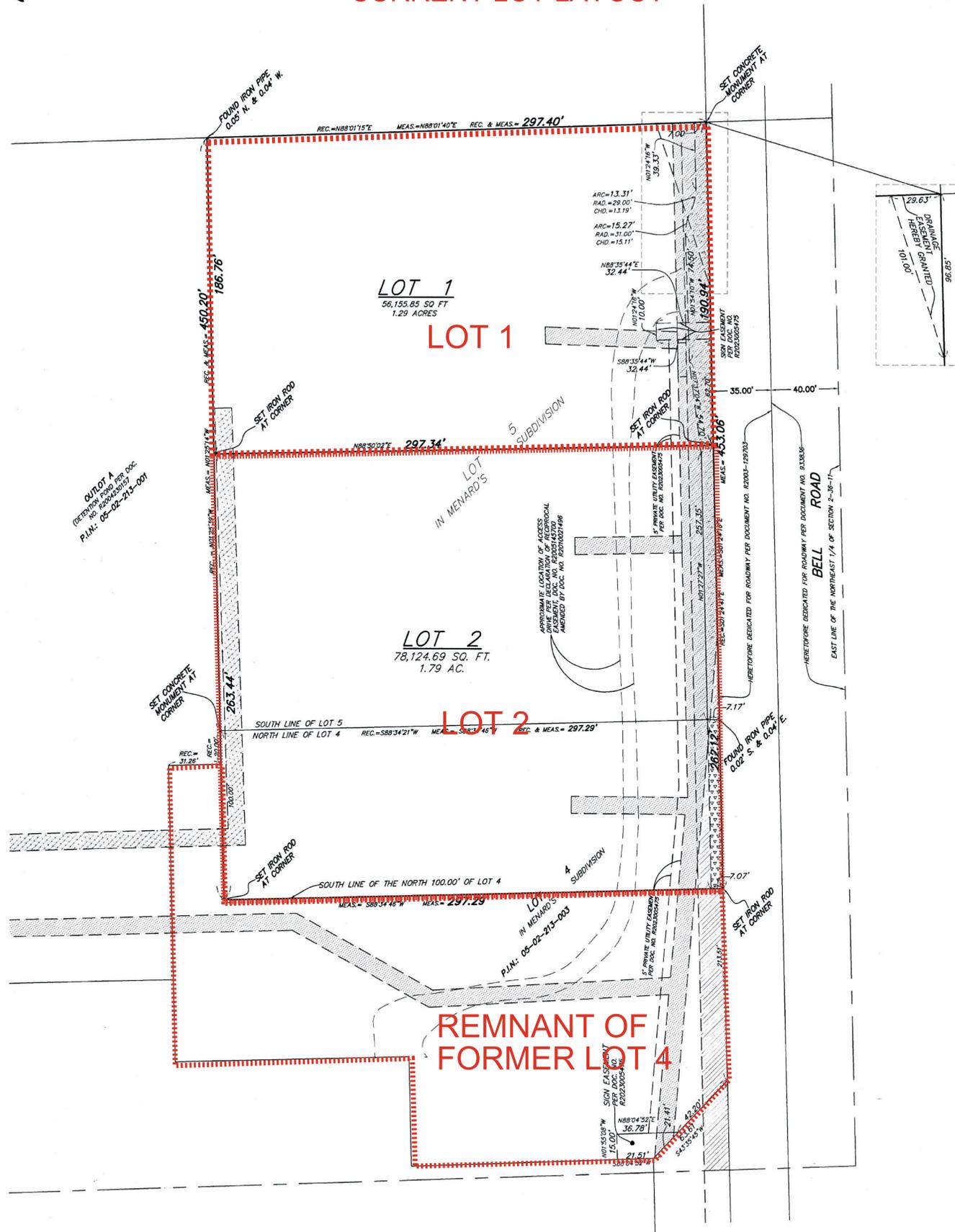
BEING PART OF THE NORTHEAST QUARTER AND THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 36 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

P.I.N.: 05-02-213-002
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GARY A. STUNEL
WILL COUNTY RECORDER
RECORDED ON
01/26/2025 11:40:48 AM
R/C # 2025-0000
RENTAL # 2025-0000
PAGE 1 OF 1



CURRENT LOT LAYOUT



LEGEND

- COMMONWEALTH EDITION EASEMENT PER DOC. NO. R200306537
- EASEMENT FOR WATER MAIN PER DOCUMENT NO. R201000938
- EASEMENT FOR SANITARY SEWER MAIN PER DOCUMENT NO. R201000935
- SIDEWALK EASEMENT PER DOCUMENT NO. R2023005475
- SIDEWALK EASEMENT HEREBY GRANTED

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PLAT RETURN ADDRESS

SUBMITTED BY/MAIL TO:
VILLAGE OF HOMER GLEN
14240 W. 151ST ST.
HOMER GLEN, IL 60491

PREPARED FOR: DANIEL LAW OFFICE, P.C.

DRAWN BY: MNG ILLINOIS PROFESSIONAL DESIGN
ORDER NO.: 22-22484-RESUB-REV 7 FIRM LICENSE NO. 184.002870
SHEET 1 OF 2

NSS GROUP REAUTHORIZATION OF SPECIAL USES, INCLUDING
MAJOR AMENDMENT TO MENARDS PLANNED UNIT DEVELOPMENT
13812/13830 S. BELL ROAD (PINS 16-05-02-213-002, -005)
LOTS 1-2, RESUBDIVISION OF LOT 5 IN MENARDS

**SPECIAL USE STANDARDS (PLANNED DEVELOPMENT-MAJOR CHANGE)
(DECEMBER 11, 2025 REVISION)**

Applicant restates the findings and support provided before the Village's adoption of Ordinance Nos. 22-038 and 23-062. It also notes that the car wash findings should remain unaffected by the proposal of a service station on Lot 2. Section 220-1209D of the Code of the Village of Homer Glen states the required standards for making findings of fact for a special use (and any amendments thereto). Applicant incorporates the special use standards narrative, project description, environmental statement, and market and operations statement. Applicant meets the standards with respect to the major change proposed.

On September 14, 2022, the Village Board approved Ordinance No. 22-038 which granted, in part, a special use permit for a major amendment to the Menards planned unit development with exceptions, special use permits for a car wash and a drive-through establishment, site plan approval and a variance from exterior construction standards. Ordinance No. 22-038 also approved a preliminary plat of subdivision. The Village Board also approved the requested major change via Ordinance No. 23-062, but it requires reauthorization. Applicant's final plat of subdivision was recorded in January 2025. It continues plans for the modern car wash and for the drive-through. Applicant continues plans for work on Lot 4 related to the access road and grading and slightly alters plans for signage on Lot 4.

Applicant proposes a two-lot planned unit development that includes a car wash on Lot 1 and a gasoline service station, convenience store and a restaurant with a drive-through on Lot 2 of land formerly known as Lot 5 in Menards Subdivision and the north 100 feet of Lot 4 in the same subdivision. Lot 2 will have 24-hour operations. The application contemplates special use approvals for the service station and amended drive-through operation with exceptions that are slightly different than those approved under Ordinance No. 22-038 (and identical to those within Ordinance No. 23-062). The proposed site plan and landscape plan substantially conform to the general design principles reflected in the plans approved by in the prior entitlement which is now supported by a final engineering set which is part of the application.

The project involves a further amendment to the final governing documents of an approved planned development. Applicant meets the requirements of Section 220-1209D of the Code of the Village of Homer Glen. Applicant meets the standards and findings necessary for a special use planned development (and any amendments thereto). Applicant meets the standards with respect to both the service station, drive-through, car wash and 24-hour operations on Lot 2. (See Special Use Standards narrative which is incorporated herein by this reference.) The overall planned development meets the standards of Section 220-1209D as follows:

1. ***The establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.*** Menards

is a 20-year old development. At the time of its approval the Village found that the project met local PUD standards. Over the years, the Village saw vast development of lots opposing Menards and land to the south along Bell Road. As designed, planned and constructed, the Menards planned development poses no risk to the public health, safety, morals and welfare. Over the years, outlots with major retailers anchoring sites were also developed, and the development of these outlots (simultaneously or later) has not impacted the public. Within the Menards planned development, the Village authorized the division of the southerly Lot 2 in a fashion that nearly mirrors the 2025 division of Lot 5 at the north end. The common detention and open space is west of Lot 5 (now Lots 1-2) and more development area is available at the south end of the Menards PUD. The planned development will comply with the Zoning Ordinance except as the regulations are modified. The 88 parking spaces (including 22 vacuum spaces and 16 pump stations within the development) will afford ample parking areas. The development preserves and enhances the access road and provides for uniform signage. Public right of way design and internal road planning is supportive of the proposed planned development amendment and collaborative land uses on each lot. Applicant provides a large landscape island along the access road. The landscape plan and the photometric plan are contained in the civil drawings set.

2. ***The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.*** The uses in the amended planned development territory will draw from area traffic much of the time. Trip generation for the service station is to be expected as noted in the KLOA report which supports the car wash, retail, drive-through and restaurant operations. Like the car wash, the service station will bring vehicles into the site that add to existing site traffic but will not add to Bell Road traffic since most of the drivers will be in the C-2 district for other purposes or making a trip through the district that they would likely have made anyway.

3. ***The special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.*** Service stations, drive-throughs and car washes in the area have long co-existed with nearby commercial uses (as well as residential uses). The retail building for the service station contains a tenant space for a restaurant as well as a large convenience store. The uses will compliment the corridor by offering goods and services necessary to residents, visitors and employees in the Village. Competition may be a consideration for some owners, but it is not a factor that can really control the decision. Regardless, the restaurant space should not affect the new Arby's and the impacts of the planned development amendment on Lots 1-2 will not extend to Lot 3 other than the beneficial impact of having more traffic on the interior of the Menards Subdivision for purposes that include quick stops for lunch or dinner at Arby's. The relationship of the activity on site does not pose a risk to nearby planning and land use. The planned development is further from residential use than other similar uses in the Village.

4. ***The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.*** While many may not notice, the addition of a service station with a modern car wash at I-355 and 159th Street has led to positive reviews and gratitude. Additionally, residents appreciate the availability of additional services which the data collected for the most recent paused effort to update the 2005 Plan supports. Finally, modern trends relating to service stations reflect positively on efforts to include a good restaurant and large convenience store in gas station planning.
5. ***The proposed special use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.*** The amended planned development will authorize uses deemed legislatively appropriate within the zoning district. All uses exist in the Bell Road corridor, though a service station with a modern car wash is not available to residents in this part of town or, for that matter, readily available to folks east of the site and extending into Orland Park. Numerous commuters from Will County and Cook County use Bell Road, and the pause for gas or a car wash lead to a longer stay in the Village's key commercial corridor. While there will be some discussion of signage, the overall development substantially meets the zoning ordinance. The development will include construction of the access road and uniform ground signs across Lot 1, Lot 2 and Lot 4.
6. ***The establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*** The planned development special use already exists. This amendment is intended to continue the development intent of a multi-lot, mixed-use development with the addition of a use across two lots that will offer synergy while also providing improved access to the remainder of Menards Subdivision and operating in an envelope desired by the Zoning Ordinance (landscaping, lighting, etc.). The amendment is necessary to develop a use that draws traffic into the Menard's Subdivision that will prove beneficial to owners, occupants and visitors alike. The development contemplates compliance with all standards in the Village Code with a relatively small number of exceptions. The development complies with the Village's Dark Skies regulations. The amendment should have no impact on the stables or on nearby religious and assembly use, open space and commercial uses. Residential land use and zoning is well-removed from Lot 1 and Lot 2. The businesses on Lot 1 and Lot 2 will provide convenience to residents in the Village and in the immediate area.
7. ***The exterior architectural appeal and functional plan of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.*** All buildings share visual and design similarities. Additionally, The style of construction will be similar to others in the Bell Road corridor. The Redeveloped Arby's building also carries similarities with the

buildings planned for Lot 1 and Lot 2. The signage, including freestanding or ground signs is also consistent with expectations along Bell Road. Lastly, the service station canopy will be designed in a fashion that respects the Village's preferences for similar canopy signs while will also allowing proper use of the gasoline brand's trade dress. The color of the canopy face is placed thereon using a decal application and it is not joined with the sign or illuminated. Only the signs will be illuminated. The use of two signs on the east canopy face intends to serve each of two entrances from Bell Road, alerting drivers to the location of the pump islands. Signs on the canopy will be confined to sides of the northeast and southeast corners of the canopy.

8. ***Adequate utilities, access roads, drainage and slash or necessary facilities have been or are being provided.*** Minimal work to connect to utilities is required because the site has been engineered since 2004. All utilities, public streets, and interior access roads are sufficient for the uses within the planned unit development. Lot 1 and Lot 2 will serve the same intended purpose with a similar intended intensity of use of these utilities and facilities as reflected in planning for the original Lots 1-2. The inclusion of a portion of Lot 4 in Menards Subdivision was intended to allow better landscaping while removing parking from one side of the access road and adjusting parking for the convenience of customers. The detention facility is adequate to serve Lot 1 and Lot 2, and it will still operate as a physical separation for homes well West of the development.
9. ***Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.*** The traffic study and related consultation and design afforded applicant the opportunity to evaluate and plan for proper site access without overburdening the access drive. The access drive will continue the free flow of traffic from the north Bell Road entrance, as has been intended since the original development of the Planned Unit Development. The amendment maintains the intent and design capacity for proper management of traffic using public streets and the access road to access Lot 1 and Lot 2 whether utilizing the stoplight or the restricted northerly Bell Road access.
10. ***The proposed use, at its location, meets the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the Plan.*** The modification to the planned unit development meets the objectives of the 2005 Comprehensive Plan and data collected not only in relation to the 2005 Plan but also when considering a more recent update to the plan. The village determined it was not necessary to amend the comprehensive plan. The amendment still offers the synergy intended by the 2005 Plan for developments along Bell Road. The synergy exists within the particular shopping center. The development will also respect surrounding developments by maintaining adequate lighting, building spacing and orientation, and landscaping. The plan relies on long-planned access from public streets and is consistent with County and Village planning. Although signage for gas stations always requires relief, the signage in this instance is oriented reasonably towards streets, private access drives, and interior use areas. The signage is reasonable in light of the size of the building, distance from Bell Road, and nature of the signage (including pump signage and trade dress appearing above and around gasoline pumps). Applicant's canopy signage

will be similar to that previously approved by the Village even though some sign relief is required to meet the practical intent of allowing canopy signs to serve Bell Road drivers as well as drivers on the access road.

PLANNED UNIT DEVELOPMENT EXCEPTIONS

Applicant has proceeded before the Village Board on two occasions in order to move this modification forward. Each time, the Village was comfortable with the various planned unit development exceptions. There has been no change to the plans since the last Village Board review. Physical constraints did not lead to delays in the completion of work on the development. The exceptions previously granted within the modification were sufficient to allow a reasonable development while adhering to the purpose and intent of the Zoning Ordinance.

The village is authorized to engage in adjustments to the Zoning Ordinance through exceptions to the applicable bulk regulations within the boundaries of the planned unit development. These bulk regulations include, but are not limited to, lot area, width, depth, height, planning for yards, building separation and the size of accessory structures including signs and sign panels. The findings necessary when approving planned development exceptions appear in section 220-904(B).

The exceptions are solely for the purpose of promoting an efficient and coordinated site plan no less beneficial to the residents or occupants of the development, as well as the neighboring property, then would be obtained under the bulk regulations of this chapter for buildings developed on separate zoning lots. The exceptions include the following:

1. From Section 220-807, to continue the approved access drive and allow for the new shared interior drive serving two lots, which extends across required interior yards of Lots 1 and 2, and serves the purpose of avoiding excessive paved area, wasted land area and pursuit of a plan that is incongruous with planning for the Menards Subdivision;
2. From Table 2B, 2:7, to allow the car wash and new retail building to be located closer to the shared lot line between Lot 1 and Lot 2 in furtherance of a development that features a large open space area to the north on Lot 1 and open space to the south on Lot 2, all of which allows better building separation and more perimeter open space;
3. From Table 2B, 2:7, to allow lot coverage in excess of 65% within each resulting subdivided lot as reflected in the plan submitted with the application, including the final engineering plans, which is consistent with lot coverage planning for the overall planned unit development within the Menards Subdivision;
4. From Table 2B, 2:7, to allow a 19-foot tall pump island canopy;
5. From Table 2B, 2:8, to continue the long-standing 20-foot landscaped front yard along Bell Road adjacent to the planned access road with efficient use of parking along the drive that is consistent with the remainder of the Menard Subdivision;

6. From Section 220-1002(F), to permit parking spaces that are six inches shorter (19 feet deep) in order to continue the approved access drive and allowing consistency in parking design across the development site and landscaping and an area for loading even though most uses of this type do not require a loading space;
7. From Section 220-1002(H)(8), to allow the continued use of parking in the front yard along the approved access drive which has been planned since development across the front yard in the existing improved areas in the Menard Subdivision;
8. From Table B(4), to allow adequate signage for the development and individual tenants as follows:
 - a. To allow additional wall signs on the north and south walls of the car wash;
 - b. To allow additional wall sign area on the entire car wash, which has exceptionally limited building frontage and requires more signage;
 - c. To allow additional overall wall signage on the Lot 2 building in light of the effort to maintain visibility;
 - d. To allow signage as depicted on the pump islands' canopy (including two canopy signs on the east elevation of the canopy);
 - e. To allow trade dress branding directions instructions, video panels and viewing screens at and near each pump station;
 - f. To allow two ground signs, one of which will be situated on the remainder of Lot 4 according to plans that have long existed for ground signs at the southeast corner of Lot 4 that will serve businesses other than Menards which has its primary sign on the opposite corner of the main intersection of Glengary Drive and Bell Road; and
 - g. To otherwise approve the sign package, including use of signs reserve for Lot 4 by one or more users of Lot 1 and Lot 2.
9. From Table B(4) In order to allow placement of the Southerly monument sign closer than 10 feet to the interior Southern lot line at the entrance (Menards and staff will still review this matter during permitting).

Each of the above exceptions has a legal and practical foundation tied to development of an efficient and coordinated site plan with resulting land improvements that respect the needs of the businesses on Lot 1 and Lot 2, the businesses already existing in Menards Subdivision, and the prerogatives of neighbors and visitors to the subject property. None of the relief imposes any bulk or other development burdens on neighbors. Indeed, the relief sought serves to maintain and improve traffic flow, wayfinding, and the appearance of a successful business district in the Bell Road corridor.

The planned development with exceptions provides amenities above and beyond the minimum requirements, such as outstanding design and architecture, the quantity and quality of open space and landscaping, or other features. As noted above, several of the adjustments are intended to internalize planning for drive aisles and parking and to afford applicant the opportunity to maintain a consistent landscape yard along Bell Road, increase open space north

of the car wash on Lot 1, karma and increase open space south of the development area on Lot 2. The result is a project with better open space and substantial landscaping beyond that which is required by ordinance. The placement of the buildings also preserves views across the Menard Subdivision to the recently redeveloped Arby's drive-through restaurant and the Menards operation. Notably, all perimeter yard planning remains as contemplated by the Village since 2004 and through each redevelopment phase within the Menard Subdivision. Modification allows for unified onsite and offsite access and signage. Lastly, the various deviations to parking design allow for a landscape island between the access road and the pump islands that serves the Village's interest in avoiding traffic for gas customers that include trucks with semi-trailers and this island will soften the site and provide an improved westerly view from Bell Road.

Dated: January 7, 2026

Respectfully submitted,

DANIEL LAW OFFICE, P.C.

By: _____
Mark W. Daniel
Applicant's Attorney

NSS GROUP REAUTHORIZATION OF SPECIAL USES, INCLUDING
MAJOR AMENDMENT TO MENARDS PLANNED UNIT DEVELOPMENT
13812/13830 S. BELL ROAD (PINS 16-05-02-213-002, -005)
LOTS 1-2, RESUBDIVISION OF LOT 5 IN MENARDS

**SPECIAL USE PERMIT STANDARDS (JANUARY 7, 2026 REVISION)
(CAR WASH, SERVICE STATION, DRIVE-THROUGH, 24 HOUR OPERATIONS)**

Applicant restates the findings and support provided before the Village's adoption of Ordinance Nos. 22-038 and 23-062. It also notes that the findings concerning the car wash (Lot 1, north) should remain unaffected by the proposal of a service station on southerly Lot 2. The service station proposal improves the car wash's functionality by allowing the elimination of parking spaces at the stacking entrance and by eliminating parking on the west line of the access road. Section 220-1209D of the Village of Homer Glen Zoning Ordinance states the required standards for making findings of fact for a special use (and any amendments thereto). Applicant meets the standards with respect to the service station, hours of operation (new since the last approval) and the restaurant offering a drive through.

This return visit to the hearing and approval process should add confidence in plans for construction planned to begin in early Spring 2026. Final platting of the approved subdivision occurred in 2025. Among other things, the approved plat includes the form of the two-lot planned unit development modification and the sign easement for the shared sign at the northwest corner of the main entrance. Menard, Inc. and Applicant recorded an easement relating to the shared signs among other matters. Final engineering plans were provided to the Village before and after platting. No material change to plans approved at zoning were necessary.

As noted in the project description, there were some new staff requests in December 2025, which is confirmation that what amounts to a fourth review of the project occurred prior to hearing. One significant comment about lighting on the north line of Lot 1 was resolved once staff confirmed a narrow buffer parcel (owned by Menard, Inc.) was situated in the C-2 zoning district and not zoned A-1 Agricultural. Additionally, while a building permit was not fully applied for or obtained in 2025, the Applicant proceeded with Village civil engineering review for permitting (full engineering set provided with application, photometrics near access road updated per staff request and provided this same date). For zoning purposes, and in light of the final engineering, the plans with the application should be acceptable to the Village.

CAR WASH SPECIAL USE (LOT 1)

1. ***The establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.*** The overall site plan underwent several revisions through staff meetings, hearings and Village Board meetings. The plan features sufficient traffic flow and has been supported by KLOA throughout. The current plan is optimal and involves what can be characterized as ordinary car wash operator precautions that are focused on employee observation and various controls that occur during peak usage times. The car wash is remote from residential use (800' -plus to the north beyond a farm and a Nicor utility line, 1,000'-plus

to the southeast and 1,000'-plus to the west). The nearest land zoned but not used for residential use is across Bell Road (a religious land use zoned E-1 Estate Residential that is 800 feet away on Glengary Drive and vacant land platted for open space (Goodings Grove Unit 5) zoned R-4 or R-5 roughly 350 feet away). The +/-17 acre tract one strip of land removed from the car wash (Rivera's Andalusian Farm, 13700-13728 Bell Road) is zoned A-1 Agricultural and hosts a boarding and stable operation with multiple buildings (over 600 feet away). The location of the car wash tunnel, the tunnel's west-to-east alignment, and the counterclockwise flow from pay stations, with no outdoor public address, consider noise to lot lines near zoning transitions. The outdoor vacuums (closed system) will not generate noise at the lot lines. The car wash will draw traffic into three pay stations near the northwest corner of Lot 1 that offer more than sufficient pre-pay stacking capacity and sufficient stacking between the pay stations and the car wash entrance in the event that there is a pause near the exit of the car wash. The planning for the car wash respects the shared driveway serving lots within Menards Subdivision. Noise from the car wash will meet applicable state laws and regulations for daytime and evening noise. Lastly, following confirmation of the C-2 zoning on the buffer parcel (as noted above), Applicant adjusted its lighting plan to reduce commercial to commercial lighting spillover so that it met standards desired by staff which are well within the commercial lighting standards.

2. ***The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.*** The car wash is situated so as to present the least impact to the access road, to Rivera's, and to any nearby current or future residential uses. In this particular C-2 district, the car wash benefits from the depth of the overall commercial planning along each side of Bell Road, including the distance between the stacking and car wash entry from residential uses west of the ComEd utility lines on the west side of the Menards Subdivision. The utility right-of-way offers a recreational and open space buffer planned along with and subsequent to annexation of the Menards Subdivision years ago. Bell Road features 20 acres of parking lots, two gasoline service stations and numerous trip generating uses. The car wash is a use that draws from the traffic already in the area. It provides residents and visitors a reason to stay in the C-2 district longer and will offer a benefit that aligns with the vehicular-intensive nature of the corridor. The car wash will not interrupt the planning in the C-2 corridor because it is situated at a location that is presently the northernmost usable commercial lot.
3. ***The special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish, and impair property values within the neighborhood.*** Rivera's is a large stable operation, with an indoor training arena and other amenities. Rivera's entrance driveway is situated nearly 500 feet north of the shared driveway that has been planned since 2004. Interference with the operation of this driveway should not arise due to the signalized intersection at Glengary Drive. Existing and planned residential areas were designed in contemplation of Bell Road planning and the business uses along Bell Road. There is no

interconnection other than a pedestrian way or recreation path between Menards Subdivision and the adjacent residential area to the west. The potential for vehicular interconnection is slim. The determination since 2004 not to create a vehicular route from these residential areas to the intersection at Glengary Road indicates that such a connection is not desirable. There is no reason for car wash traffic to utilize roads east of Bell Road unless the trip originates or ends in these areas.

4. ***The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.*** An automated car wash will provide a necessary and desirable use and it is in the interest of public convenience to have this use along Bell Road. In fact, most heavily travelled corridors have car wash uses where they can be a benefit. There is no similar use in the corridor and the Village notes related leakage in relation to gasoline service stations. The car wash near these stations is an amenity that will secondarily benefit the stations. In general, modern car washes use far less water than do older car washes, and today's car washes save even more water at a time when trends toward automated washing have reduced the number of people washing their car at home to well below half of private vehicle owners.
5. ***The proposed special use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.*** As noted elsewhere, the surrounding zoning, actual land use and continuity of the corridor will not be affected. Menards itself is a heavy generator of traffic. The Firestone use to the south should benefit. Additionally, the new Arby's should benefit from having multiple potential customers use the car wash and make additional purchases in the area. Trip generation for the car wash is actually fairly nominal, but the car wash is an additional draw into Menards Subdivision for people making these trips. The car wash circulation will neither obstruct the views to Lot 2 or the remainder of Lot 4 nor impede the use of the access road to reach Lot 4 or other lots in Menards Subdivision.
6. ***The establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*** The car wash will be situated on land at the north end of the C-2 corridor and rely on shared access that has been planned since 2002 and pursued in 2004.
7. ***The exterior architectural appeal and functional plan of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.*** The car wash offers architectural and landscape relief. Although the car wash comes along well after Menards, it will rely on the same design planned for functional access to Menards. Even though other lots in the development do not have similarity of style or appearance, the car wash will have visual connectivity with the retail building.

8. ***The adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.*** There are sufficient utility, street, drainage and other facilities, some of which were partially constructed or fully constructed for the development of former Lot 5 which is now most of Lot 1-2 in the resubdivision. There is no longer a need for the once-planned interim lighting along the shared access road as well as a surface and cross-section deemed adequate by staff for the purpose of relying on the access road for access at a time when Lot 4 remains to be developed. Lot 1 and Lot 2 will be developed at the same time, with Menards and the Village participating in permitting for the work on the remainder of Lot 4.

9. ***Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.*** The KLOA traffic memorandum indicates that the car wash will primarily draw from existing traffic, but that new traffic may also arise. (This is so for every development on vacant land and even for redevelopment such as the Arby's re-utilization of a former bank building.) Adequate Bell Road ingress and egress is available directly using the right-in/right-out and from northbound lanes using the signalized main entrance. KLOA mentions precautions that occur during the peak annual usage days of a car wash, and Applicant will take them as it does at other locations. KLOA also mentions stop bars and signage that will be added to plans during staff review.

10. ***The proposed use, at its location, meets the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the plan.*** The Village's 2005 Comprehensive Plan supports commercial use of Lot 1. A car wash is one of the special uses deemed legislatively proper within the C-2 district. The 2020 draft future land use plan continued the designation but this plan was not advanced to approval. Notably, the property one lot removed to the north from Lot 1 is designated as agribusiness and the land across Bell Road remains designated for public/institutional use. Bell Road is one of the Village's three retail corridors and also its most heavily developed. The plan recognizes mixed use is the next opportunity for diversification of the Village's economic climate along Bell Road. The car wash will contribute an amenity supportive of gasoline service stations in the area inasmuch as Meijer has no car wash and the Citgo car wash is not as modern. The draft 2020 Plan identifies retail leakage in the service station category. Having a car wash of this type along Bell Road will encourage use of the corridor for other automotive uses. The new use will also add another reason to return to and shop in the Bell Road corridor.

LOT 2 SERVICE STATION

1. ***The establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.*** Applicant converts the retail building to a service station with a 7,715 square foot restaurant and retail building divided from the access road by a landscape island for better traffic flow. The service station is remote from residential use (800' -plus to the north beyond a farm and a Nicor utility line, 1,000'-plus to the southeast and 1,000'-plus to the west). The nearest land zoned but not used for residential use is across Bell Road (a religious land

use zoned E-1 Estate Residential that is 800 feet away on Glengary Drive and vacant land platted for open space (Goodings Grove Unit 5) zoned R-4 or R-5 roughly 350 feet away). The tract one strip of land removed from the car wash (Rivera's Andalusian Farm, 13700-13728 Bell Road) is zoned A-1 Agricultural and hosts a farm (agribusiness) with multiple buildings. The location of the service station considers proper buffering, parking, traffic movement and landscaping.

Another issue raised by staff, but since addresses, was that it prefers to have the shared freestanding sign approved for the northwest corner of the main entrance subject not only to a plan for planned unit development that applies to the Menards planned unit development but also subject to a plan for development approved by ordinance for Lots 1 and 2 in this instance. Staff saw inconsistency in handling the easement for the shared shopping center sign through a recorded easement and plat, but Menard, Inc. should control development in this area subject to compliance with the easement benefiting Lot 1 and Lot 2 that are the subject of this application. Staff has also asked that Applicant pinpoint the location of the freestanding sign with more detail than lying within the approved easement area when the Village was previously comfortable with the location even if Menards and Applicant, subject to permitting, moved the sign slightly within the easement area due to utilities or lines of sight. Applicant hopes that the Village will approve the same approach as it has previously, which would allow for this movement within the easement area so that neither a minor change under Section 220-903(C)(2) nor a major change under Section 220-903(C)(1)(A)(9, 11) requires more meeting review. In all events, the Village will still have permit review for the freestanding sign.

Another staff request involves the canopy sign, and this request is similar to the last hearing process comments. Applicant has sufficiently described the nature of signage on the canopy in this application and did so in the prior approved application. Then, and now, Applicant does not provide a final rendering of signage on the canopy because the national brand is yet to be determined. Then, and now, the likely brand is Mobil. The Village can expect a canopy wrapped with blue (cornices will be white, and the wrap will not be illuminated). At the eastern ends of the north and south faces, there will be a Mobil sign. Applicant is still reviewing lines of sight with branding, and it is possible the sign on the north face may be eliminated after zoning. The east face of the canopy will feature a Mobil sign at its north and south ends. There will be a disconnection between the illuminated Mobil signs and the blue decal on the canopy so that it will not appear to be part of the sign. Additionally, the blue on the canopy is not repeated as trade dress on the building, freestanding signs or the car wash on Lot 1. Applicant cannot confirm the identification of the brand prior to entitlements and permit application for the canopy because this business decision may not occur until after construction starts. Selection of brand and the jobber involved arises in a constantly changing gas market, so the Village should be aware, as it was before, that Applicant intends to allow a Mobil or a BP. Speedway, Shell and other brands already exist in the area. Again, the matter boils down to whether something is a major or minor change and Section 220-903(C)(1)(A)(9) identifies a change that “[s]ignificantly alters the appearance of structures or signs” while Section 220-903(C)(1)(A)(11) contains a catch-all referring to changes in underlying documents. Respectfully, Applicant asks that the Village approve the project with two

signs on the east face of the canopy provided that the Mobil signs on any face will not be illuminated or exceed 20% of the area of the face of the canopy. This will avoid a delay in site development and an unnecessary series of meetings for a matter that is avoidable.

2. ***The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.*** The service station is situated so as to present the least impact to use of the access road, to Rivera's Andalusian Farm, and to any nearby current or future residential uses. In this particular C-2 district, the overall Lot 2 use and design benefits from the depth of the overall commercial planning along each side of Bell Road, including the distance between the rear of the building from residential uses west of the ComEd utility lines on the west side of the Menards Subdivision. The utility right-of-way offers a recreational and open space buffer planned along with and subsequent to annexation of the Menards Subdivision years ago. Bell Road features 20 acres of parking lots, but it has only a few gasoline service stations despite the number of trip generating uses. The service station will draw from the traffic already feeding into the area. With the car wash as an additional draw, the Lot 2 service station will provide residents and visitors a reason to stay in the C-2 district longer and will offer a benefit that aligns with the vehicular-intensive nature of the corridor. The service station will not interrupt the planning in the C-2 corridor because it is situated at a location that is adjacent to the northernmost usable commercial lot.
3. ***The special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.*** Rivera's Andalusian Farm is a large stable operation, with an indoor training arena and other amenities. Rivera's entrance driveway is situated nearly 500 feet north of the shared driveway that has been planned since 2004. Interference with the operation of this driveway should not arise due to the signalized intersection at Glengary Drive. Existing and planned residential areas were designed in contemplation of Bell Road planning and the business uses along Bell Road. There is no interconnection other than a pedestrian way or recreation path between Menards Subdivision and the adjacent residential area to the west. The potential for vehicular interconnection is slim. The determination since 2004 not to create a vehicular route from these residential areas to the intersection at Glengary Road indicates that such a connection is not desirable. There is no reason for service station traffic to utilize roads east of Bell Road unless part of a trip originates or ends in these areas.
4. ***The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.*** The service station will provide a necessary and desirable use and it is in the interest of public convenience to have this use along Bell Road. In fact, most heavily travelled corridors have many of these uses where they can be a benefit. There is no similarly aligned service station and car wash in the corridor and the Village has noted related revenue leakage in relation to

gasoline service stations and restaurants. The service station is an amenity to the public and to the Village.

5. ***The proposed special use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.*** As noted elsewhere, the surrounding zoning, actual land use and continuity of the corridor will not be affected. Menards itself is a heavy generator of traffic. The Firestone use to the south should benefit. The new Arby's should also benefit from having multiple potential customers use the service station and make additional purchases in the area. Trip generation for the use is actually fairly nominal, but the service station is an additional draw into Menards Subdivision for people already making these trips. The service station design improves functionality of the access road intended to serve Menards Subdivision. The Bell Road corridor and Menards Subdivision were planned for uses with this type of traffic.
6. ***The establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*** The service station will be situated on land at the north end of the C-2 corridor and rely on shared access that has been planned since 2002 and pursued in 2004. The inclusion of the north 100 feet of Lot 4 renders the remainder of Lot 4 available to Menards or another user working in cooperation with Menards.
7. ***The exterior architectural appeal and functional plan of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.*** The service station offers architectural and landscape relief. Although the use comes along well after Menards, it will rely on the same design planned for functional access to Menards. Even though other lots in the development do not have similarity of style or appearance, the service station will have visual connectivity with the car wash building. It also incorporates horizontal and vertical elements similar to those approved before.
8. ***Adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.*** There are sufficient utility, street, drainage and other facilities, some of which are partially constructed or fully constructed for the development of former Lot 5, which is the largest part of current Lots 1-2 in the resubdivision. Applicant has the option of planning for interim lighting along the shared access road as well as a surface and cross-section deemed adequate by staff for the purposes of relying on the access road for access at a time when Lot 4 remains to be developed. To be clear, however, Applicant will pursue development of the access road as reflected in the civil engineering drawings.
9. ***Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.*** KLOA provided a traffic memorandum and design consulting. It indicated that the car wash and service station will draw largely from existing traffic, but that new traffic may also arise. (This is so for

every development on vacant land and even for redevelopment such as the Arby's re-utilization of a former bank building.) Adequate Bell Road ingress and egress is available directly from southbound lanes using the Lot 1 right-in/right-out and from northbound lanes using the signalized main entrance. KLOA previously recommended precautions during the peak annual usage days of the car wash, and Applicant will take them as it does at other locations. KLOA has mentioned precautions to be taken in the operation of the service station. Applicant and KLOA believe the service station works well with the car wash even though it will generate more internal traffic. Applicant will plan according to the traffic memorandum by KLOA to the extent it is relevant to the new plan.

10. ***The proposed use, at its location, meets the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the plan.***

The Village's 2005 Comprehensive Plan supports commercial use of Lot 5. A service station is one of the special uses deemed legislatively proper within the C-2 district. The draft 2020 future land use plan continued the designation of Lot 5 as commercial, but this is only an indication that the area has not changed and not an official plan. Notably, the property one lot removed to the north from Lot 1 is designated as agribusiness and the land across Bell Road remains designated for public/institutional use. Bell Road is one of the Village's three retail corridors and also its most heavily developed. The service station will help resolve retail sales tax leakage in the service station category (considered in the 2005 plan and identified in the draft 2020 plan). The service station at this location along Bell Road will improve the corridor. Notably, the service station and retail and restaurant use will operate with a level of synergy that is not currently in place along Bell Road.

LOT 2 DRIVE-THROUGH SPECIAL USE

Several of the remarks above apply to the drive-through special use. Applicant will not repeat them, but will rely on them relating to factual conclusions below. Applicant also notes that, via Ordinance No. 22-038, the Village has concurred in the findings necessary for a drive through on Lot 2. The proposed drive-through has now been shifted 100-105 feet west of the access road improving on the prior design. Applicant has eliminated parking west of the drive through which caused some concern under the earlier design. A bypass lane is provided adjacent to the stacking area. On a small number of occasions, loading may occur from the bypass lane, but loading will be planned so as to avoid peak hours of drive-through use. Applicant improved access into and from the drive-through so it is wider at relevant points.

1. ***The establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.*** The restaurant is a permitted use. The drive-through for the restaurant will stack from the northwest corner of the building to preview and menu boards on the southwest corner with a service window in the eastern half of the south façade. The drive-through will no longer exit directly onto the access road across Lots 1, 2, and 4. It will exit to Lot 2's interior drive aisles then onto the access road. The main driveway into Menards Subdivision and a distance over 500 feet separate the proposed drive through from the Arby's drive through. Proposed stacking exceeds that which is required by code. Within the Menards Subdivision, there will be no

detriment or endangerment. Drive-through operations at this location offer ingress and egress that are both efficient and practical. The relocation of the drive-through also minimizes possible conflict west of the building.

2. ***The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.*** The drive-through will have peak hours that lead to busier drive aisles within the development, just as does the Arby's drive-through on Lot 3. Plans depict the drive-through on Lot 2 with sufficient spacing and curb radii to allow parking on the area that was formerly the north 100 feet of Lot 4 and now part of Lot 2. Vision triangles are respected.
3. ***The special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.*** Compliance with the Zoning Ordinance is a measure of whether the drive-through will cause injury or a diminution or impairment of property values. The drive-through substantially conforms to the Zoning Ordinance. Reasonable relief is necessary in some respects to allow for proper circulation with landscaping requested by staff. The Menards Subdivision is essentially its own neighborhood as a result of the absence of any interconnectivity to the north or west and the sole connections otherwise involving Bell Road. Beyond the Menards Subdivision, there will be no noise, visual or other impact whatsoever that could impact land values or the use of land.
4. ***The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.*** Applicant could provide a restaurant without a drive-through at this location, but the burdens for carryout coupled with the loss of a broad range of possible customers ultimately would fall on consumers and the Village. Additionally, drive-through or drive-up operations have been encouraged in the Menards Subdivision on Lot 1 (Menards), on Lot 3 (bank and Arby's), and on Lot 2 (Firestone) as well as in the C-2 district in general. Some of these drive-through operations have been approved on lots with no contiguity to Bell Road. Lastly, though not directly relevant, the convenience of a drive-through allows a reasonable expectation of greater sales and resulting local sales or food and beverage revenues.
5. ***The proposed special use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.*** There has been and will be no interconnection with the stables to the north. The drive-through will have no impact on development along the east side of Bell Road. The stormwater management area west of the drive-through prevents pedestrian and vehicular access between the drive-through and the residential area to the west. Menards' operations screen the drive-through from view

and any noise impact such as a honking horn. (The menu board speaker will not be audible far beyond the lot line which is adjacent to a large storm facility).

6. ***The establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*** Drive-through uses have historically developed along the corridor and along streets connecting to Bell Road. In light of the design of the drive-through and its excess stacking (and reserve stacking area), there is sufficient protection of the access route across Lots 1, 2 and 4.
7. ***The exterior architectural appeal and function of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.*** The drive-through design is standard with stacking along the rear of the building in the service area and the forward-of-center window. The window is extended from the primary south façade and this operates to articulate the wall. The drive-through itself is not out of keeping with the character of the other similar uses in the area.
8. ***Adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.*** The access road planning in the development has always contemplated ingress and egress along a shared access road with parking or lot development amenities relying on the access road. The drive-through properly stacks and disperses inasmuch as volume and safety are concerned. The current plan improves on the earliest approval of two-car depth formerly proposed between the drive-through and the access road.
9. ***Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.*** During prior engineering review on the earliest application, Applicant and the Village discussed traffic conflict west of the building as well as signage and pavement markings to avoid wrong-way movements. Applicant has now eliminated the parking west of the building that caused these concerns. Applicant provides plenty of stacking and seeks approval of a preview board and a menu board (customary for quick service restaurants and more popular for fast food uses). The southerly bypass south of the drive-through lane avoids congestion at the drive-through when needed. Sufficient parking is available for the use on Lot 2 in the event that holding spaces are necessary.
10. ***The proposed use, at its location, meets the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the plan.*** Under the 2005 Plan, Bell Road is designated as a vehicle centric corridor with large and small commercial uses, most of which require a vehicle and several of which require convenience to drivers such as drive-through facilities in banks and

restaurants. While not approved, the 2020 Plan contained data that indicates some level of sales tax leakage to other communities. Both the 2005 Plan and the data behind the draft 2020 Plan indicate that diversification of the tax base and avoidance of sales tax leakage should be pursued. Setting aside that this project comes along well after the annexation agreement, and nearly two decades after planning for the area, the project-including a drive-through for the restaurant-will diversify the restaurant component by adding a restaurant type that does not exist in the area. The design of the drive-through avoids congestion in the public and private streets by providing excess stacking and allowing a reserve area for additional stacking if needed. The 2005 Plan has always supported highway commercial uses such as those along Bell Road and proposed with this project. The 2005 Plan continues to support development in Menards Subdivision as a unified commercial area. The project does not qualify as a strip commercial development discouraged under the 2005 Plan.

SPECIAL USE FOR 24-HOUR OPERATIONS

Applicant seeks a special use permit for 24-hour operations on Lot 2 and those easement areas benefitting Lot 2 (the sign easement area on the remainder of Menards Lot 4 and the parking easement on Lot 1). These easements are property rights that run with ownership of Lot 2. Section 220-711(A)(11) provides: “Hours of operation. Nonresidential uses may be open for business between the hours of 6:00 a.m. and 11:00 p.m. Establishments seeking twenty-four-hour operation must be approved as a special use in accordance with § 220-1209.” For the past four years, Applicant has informed staff and the Village that it was not planning to operate 24 hours a day from the outset. Such operations were possible later. Due to what staff sees as a prior misreading of the ordinance quoted above (even though a vague land use restriction would ordinarily be read in favor of the property or business owner), staff notified that its inquiries about 24-hour operations related more directly to whether Applicant would be open longer than 17 hours a day. Following word of this interpretation in December 2025, Applicant had a series of internal deliberations through which it determined that it was best to avoid the entanglement of the Village in its determination of whether to stay open past 11:00 PM and to open prior to 6:00 AM or to convert to fully 24 hours each day.

Operations on Lot 2 will be 24 hours per day according to the standard definition. This will include the automobile service component and the convenience store component. It may include Scali’s Beef (which is inside the convenience store building). To be clear, operations with respect to gasoline and diesel sales and on the interior of the convenience store will extend a true 24-hours a day, not merely opening sometime before 5:00 AM and closing sometime after 11:00 PM. If not open the full 24 hours, Scali’s Beef will be open past 11:00 PM at least a few nights each week. Operations may occur before 6:00 AM with deliveries and food prep as well as catering, but the start of operations will depend on catering orders and its opening of operations is less regular than Arby’s or Firestone, for example. The full menu will be available from 10 AM until 10 PM (11 PM Friday and Saturday). Scali’s is intended to run 24 hours.

1. ***The establishment, maintenance or operation of the special use will not be detrimental to, or endanger, the public health, safety, morals, comfort or general welfare.*** The 24-hour operations will be situated primarily on the south side of the car

wash on Lot 1 (not a 24-hour operation). Upon learning of staff’s interpretation, Applicant evaluated hours of operation for a number of businesses in the Village, several of which are much closer to and, sometimes, adjacent to residential land uses. In discussing these with staff, one response was that several 24-hour operations were grandfathered. The grandfathering of these businesses is a legislative determination that 24-hour operations are not inherently problematic. Further, the classification of the uses as special uses is another legislative determination that unless there is a particular issue arising with 24-hour operations at a property that does not prevail at other non-residential lots in the Village, 24-hours operations are appropriate.

Using the January 2026 Google Earth aerial images of the vicinity and existing land use, Lot 1 and Lot 2 comprise just over three (3) acres of a contiguous 180-acre plus “nonresidential use” territory extending north, east and west. To the south lies another contiguous territory with nonresidential use that far exceeds 500 acres. Relatively recently, the Village permitted townhome development to extend into the area, but not to the extent of interrupting contiguity of 680-700 acres of nonresidential use that includes Lot 1 and Lot 2.

The following nonresidential uses are true 24-hour nonresidential operations:

Anytime Fitness	12513 W 159th Street (Adj. to res., rear to rear)
Circle K/Convenience	14310 S Will Cook Road (Adj. to res. R-R, side-R)
Citgo Gas	12108 W 159th Street (Adj. to res. Common area)
Gas N Wash	15930 W 159th Street (Adj. to ag use, fut. Dunkin)
Shell	12819 W 143rd Street (Adj. to commercial)
Shell	15861 S. Bell Road (opposes residential, south)
Speedway	12007 W. 159th Street (adjacent to commercial)
Speedway	15551 W. 143rd Street (opposes residential)
Speedway	15060 S. Bell Road (adj. to ag opposes commercial)
	(Dunkin Donuts opens at 4:30 AM at this address)
Speedway	12502 W 143rd Street (Adj. to res., rear to rear)
Silver Cross Emergency Care	12701 W 143rd Street (Adj. to res., rear to rear)

The following businesses are technical 24-hour nonresidential operations:

Dotty’s Casino	14041 S Bell Road	1:00/2:00 AM
Dunkin Donuts (listed above)	15060 S Speedway (above)	4:30 AM
Dunkin Donuts	14135 S. Bell Road	4:00 AM
Davidson’s Bar & Grill	14136 S Bell Road	11:30 PM/1AM/2AM
Front Row Restaurant	14903 Founders Crossing	Fri/Sat until 12:00AM
Homer Glen Smoke & Vape	14201 S. Bell Street	Midnight (not Sun)
Jewel/Osco	14200 S. Bell Street	6 AM-Midnight
McDonald’s	14298 S. Bell Road	5:30 AM 5 days
MegaPlex Sports Center	15301 S. Bell Road	7 days 2:00 AM
Meijer Supermarket	14169 S. Bell Road	6 AM-Midnight
Mugshots Sports Bar	13031 W 143rd Street	Fri/Sat until 2:00 AM

Pelican Harry's Sports Grill	14807 Founders Crossing	12 AM/1 AM/2 AM
Rivera's Andalusian Farm	20 feet north	Often pre-6:00 AM
Starbucks	11359 W. 159th	5:00 AM
Taco Bell	14358 S. Bell Road	2:00 or 3:00 AM
Tavern in the Glen	15761 S. Bell Road	M-F 12AM/S-S 2AM
Wendy's	14115 S Bell Road	1:00 AM Fri – Wed
Wing Stop	14346 S Bell Road	10:00 AM – 1:00 AM

2. ***The proposed use at the proposed location will not have an undue or substantial adverse effect, above and beyond that inherently associated with such use, irrespective of the location in the particular zoning district, upon adjacent property, the character of the neighborhood, or other matters affecting the public health, safety and welfare of the community.*** There is no abutting residential use. Rivera's is an agribusiness with its nearest buildings on the opposite side of the car wash on Lot 1 and more than 700 feet to the north. Homes to the west are over 600 feet away across a much larger stormwater management area than those abutting any other 24 hour use. Applicant will comply with applicable licensing regulations at the state and local level.

3. ***The special use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.*** There is no private street connectivity except within the Menards Subdivision. No public streets offer access to an immediately adjacent residential use. Other 24 hour operations exist along and nearby Bell Road, and the uses near these operations have not suffered.

4. ***The proposed use at the particular location is desirable to provide a service or facility in the interest of public convenience and the gain to the public and all or a part of the community exceeds the hardship imposed upon the property owner.*** Please see above discussion of the service station special use. 24 hour operations are common in Homer Glen. At this location, near other 24 hour operations that involve a commute, a service station with an attractive convenience store is a valuable option for those working shifts between 11 PM and 6 AM.

5. ***The proposed special use is generally suitable for the particular zoning district and will not adversely affect development of adjacent properties in accord with the applicable district regulations.*** Please see above discussion. 24 hour operations (actual and as defined by code) are common in the C-2 district and other commercial or non-residential districts.

6. ***The establishment of the special use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*** The 24 hour operation is on the interior of the Menards Subdivision and will not affect use or development of Rivera's.

7. ***The exterior architectural appeal and functional plan of any proposed structure will not be so at variance with either the exterior architectural appeal and functional plan of the structures already constructed, or in the course of construction in the immediate neighborhood or the character of the applicable district, as to cause a substantial depreciation in the property values within the neighborhood.*** This factor does not apply to the request.
8. ***Adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.*** Please see above discussion.
9. ***Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.*** Please see above discussion. Traffic is greatly diminished for those hours of operation under the proposed special use.
10. ***The proposed use, at its location, meets the location, goals and objectives of the Village's Comprehensive Plan and is in general accord with the guidelines of the plan.*** Please see above discussion.

Dated: January 7, 2026

Respectfully submitted,

DANIEL LAW OFFICE, P.C.

By: _____
Mark W. Daniel
Applicant's Attorney

something kind of out of thin air. There are no sketches, no nothing. Bill said, I guess I don't understand. The site plan is here. Chairman Hand asked, with the landscaping on there? Bill said, we don't have landscaping. Chairman Hand said, that's what we want to see. Bill asked, you want to see landscaping? So, we're not going to get a vote because we don't have landscaping? We just spoke about Arborvitaes. Arborvitaes are great. They grow fast. They grow tall. They grow thick. Fence and Arborvitae. Chairman Hand asked, exactly where? I don't have anything to look at. Bill said, along the back of the curb on the south side and on the east side. Chairman Hand said, I know verbally. I feel we should table it. Commissioner Stanly said, I agree. Table it. Chairman Hand said, I think we're on the right track though. Let's take a look at what you come back with across the south lot line. Melissa said, for timeliness we can always say October 5th. Commissioner Bugos-Komperda said, they are probably trying to start construction before the freeze. I feel like it will get muddy if something happens with that meeting. As long as everybody who's in the room that needs to be notified has signed in so that I have contact information. That would be helpful. If there are not phone numbers on that list and you'd like to be notified of when this meeting is going to happen and it is not October 5th. If for some reason that meeting gets canceled, then I need to know that you're on the list. If you've not signed in, please sign in so that I can contact you directly, which is not normal procedure. I would like to do that in this case. Then we can do it to a date certain. The problem with doing it further out or not doing that is that we would have to notice. We would have to go back to noticing. I would have to go look at the dates for that. We can try for October 5th. Let me make sure I have that date right. Yes, October 5th. Commissioner Stanly asked, if that is canceled then it would be October 19th, right? Melissa said, correct. Chairman Hand asked, we can commit to October 5th? Melissa said, I think what we'll do is, you would need to table it tonight to the October 5th meeting. That is making notice here in the room with the people who are here and on the record. Then if something happens where that gets canceled between now and then, I will notify the people who were signed up on the thing. We will also notice the way we normally notice. Commissioner Foley said, I'm leaving for Arizona that morning, so I will not be here on October 5th. Melissa asked, is everybody else going to be here on the fifth? Chairman Hand said, we are all here. Melissa said, we will only have five (5) Plan Commissioners that night. Chairman Hand said, six (6) with Jay. Melissa said, he cannot be in this room.

Chairman Hand asked for a motion to table. Commissioner Stanly made a motion to table Case No. HG-2306-SMPV. The motion was seconded by Vice-Chair McGary. The motion passed unanimously voting in favor six (6) to zero (0). and will come back to the Plan Commission on October 5, 2023 unless that meeting is canceled.

b) Case No. HG-2301-PUD, Special Use Permit for Major Amendment to Menards PUD Lots 4 & 5 (Public Hearing): Consider a request for: [1] a Preliminary Plat of Subdivision; [2] a Special Use Permit for a Major Amendment to the Menards Planned Unit Development (PUD) with requested exceptions; [3] Special Use Permits for (a) a car wash (an automobile repair, service and body shop use), (b) a drive-through establishment and (c) an automobile service station (gas station); and [4] a Site Plan, for certain real property generally located in the C-2 Local Business District on Lots 4 & 5 of the Menards PUD Subdivision, Homer Glen, Illinois.

Director King presented the facts of this case. This is a request for a Preliminary Plat of Subdivision, a Special Use Permit for a Major Amendment to the Menard's Planned Unit Development (PUD) with requested exceptions, a Special Use Permit for a car wash, a drive thru establishment, an automobile service station (gas station) and a Site Plan for certain real property located in the C-2 Local Business District on Lots 4 & 5 of the Menards PUD. The applicant, NSS Group, Inc is seeking to re-subdivide Lot 5 and the north one hundred (100) feet of Lot 4 of the Menard's PUD into Lot 1, which is approximately one point two nine (1.29) acres and Lot 2 is approximately one point seven nine (1.79) acres. The applicant's proposing a major amendment to the Menard's PUD to develop the subject property for an automobile service station, also known as gas station. Also, a seven thousand seven hundred ten (7,710) square foot commercial building with two (2) tenants, a restaurant and a convenience store with a drive thru establishment. The site will also have a four thousand eight hundred sixty-five (4,865) square foot standalone automated car wash with a drive thru.

Just a little bit of background and I think Attorney Daniel is going to do some comparison background between the two (2) plans. We have two (2) new Plan Commissioners since this project came, Commissioner Fiskow and Commissioner Bugos-Komperda. Was everybody here when this project came through before at the Plan Commission? You were at the meeting? Commissioner Stanly asked, November of last year, right? Commissioner Foley said, I remember because they were wondering why they didn't buy it. Director King said, the Lot 4. That original plan, which I'll pull up right here, was a car wash on Lot 4. Now just to orient yourself, I turned all these sideways, not sideways, but the north is to the right. To the bottom of the page is Bell Road, just to make it clear and Menards would be kind of off to the top left if you were going to pretend like it was there. To the north side of the property, the right is the car wash. That long building with vacuum stalls to the right of it and then obviously the circulation around to go into it. Then on the southern part of that plan, there was a multi-tenant building with a restaurant. You would have to remind me. Attorney Daniel Daniel said, two (2) restaurants, retail and a salon. Melisa said, then there was a drive-thru with that as well. This project was recommended for approval at the Plan Commission level unanimously and then approved by the Board of Trustees. That was in September of 2022 and in January of 2023, they formally resubmitted with a new plan. The car wash and its operation circulation has not changed on this site. The vacuums are in the same place and the building is the same. The circulation is the same and pay station. All the accessory things that are in here and I'll show them to you, but those components haven't changed. The site on the southern portion, on the southern half of Lot 5 was in January, proposed to be a gas station. The applicant came in, applied, and it went through a first round of review with staff. It was met with quite a bit of site circulation issues as well as parking lot issues. As far as location of where the parking was on the property, not how many stalls, but just where it was. I don't know if there was anything else that was the impetus for this except the comments that came back from staff, both from myself and also engineering. The applicant went back to Menards at that time and negotiated additional land acquisition as part of Lot 4, which is that smaller lot that's really challenged with topography and all kinds of stuff. To add to the area enabled them to kind of spread things out a little bit and readjust where they were parking was going to go. There were quite a few things that we had some issues on the site circulation and they were able to get past all that. So that was good. This is the plan. Laura and Steve, if you have any questions about this plan in particular, I wasn't going to spend a lot of time looking at it because I think all of the stuff with the car wash is in here because it dovetails with all of it. We are requesting a special use

anyways because I think that we have to look at the site as a comprehensive look. The traffic memo gets adjusted based on the gas station and its use and all that. I think we have to look at all of it over again. We are asking for all these things to be looked at and re-motioned on. The special use, for instance, for the car wash is still live. It was approved. It was going to expire, but the board granted an extension for that so that approval is still in place. It's tied in with that site plan. This is an entire new request.

The traffic impact and the circulation in the traffic memo has been reviewed by our staff and has been determined to be sufficient. We currently don't have any major comments from the design team. As part of our review, just to bring this back up so everybody understands where the condition came from, particularly the two commissioners who were not here before. There were some concerns in the initial development about some of the circulation right at the car wash entrance. How it interacts with that four way or three way stop when it comes in from Bell Road. There were some conditions recommended in the traffic memo and then the Plan Commission and Board placed conditions on the PUD. There is kind of like a staggered approach to how they're going to monitor the car wash during a busy season and how they may have to add staffing and potentially even up to signage. The same recommendations are coming to staff and have also been placed as a condition for this PUD. The fire district did look at these plans and did not have any comments for us and feels it's sufficient as it has been designed. The drive-thru for the car wash, just to go through stacking for these uses that require stacking. The drive-thru for the car wash requires three (3) per drive-thru lane and there are twelve (12) to fourteen (14) stacking provided per lane. For the restaurant component three (3) are required for the drive-thru lane. Four (4) is provided for the point of sale in this plan to the pickup window and five (5) additional stacking are provided behind the point of sale to the menu board. I think that one thing we look at when we're looking at a drive-thru, is that they're not right up on the streets. We want to make sure that they're kind of hidden away from public view. I think that the one on the east side with the circulation on the car wash is difficult to completely get out of that right of way visibility. We do have landscaping around the property, so that should help. Then the one around the restaurant is obviously on back side of the property.

The applicant is requesting a stall depth reduction on the parking stalls. This was granted the first time around and is being requested again this time. This request is to match the same stall depth that's at the Menards PUD currently. Staff does not have a concern with the small amount of parking variance. We have granted this before in the past. One loading berth is being provided behind the convenience store. It does meet the sizing requirements. I've talked to a number of people, including our fire chief and I do think that delivery, even though we have sometimes put it on the page somewhere in a spot, does happen in lots of different spaces on a site like this. The parking breakdown is pretty simple. It has been met for all of the land uses. The car wash use is being met. The major issue with this, with staff, originally was that the majority of the parking that was being proposed was actually east of the internal drive aisle. You see a lot of parking here now that's around the outside of the convenient store. A lot of that didn't exist in the first iteration. There was concern from staff that there was a restaurant inside this convenience store. We want to make sure that there's enough adequate parking for people to park right near the building and not have to cross a major internal circulation drive. They were able to adequately provide quite a bit of parking right up near the building, which is great. All of the required parking is being met.

This is just another look at their data table that shows that parking breakdown. Vice-Chair McGary asked, what would the required parking be for this building? Director King said, on page three from the staff report, I think would be the most helpful. In the car wash scenario, the requirement for the car wash itself is actually eighteen (18) stalls and there are twenty-six (26). There is no restriction in the code that says you can't have vacuum stalls being part of those stalls. Vice-Chair McGary asked, for the building with the restaurant? Director King said, the building with the restaurant, thirty-nine (39) stalls (is required) and they have forty-six (46) stalls on the property. There is still a lot of parking on the east side of that drive aisle, but they were able to adequately add quite a bit of parking up near the building. In the previous version (2022), we had that breakdown before that showed the three (3) different scenarios of the most intense conversion of the multi-tenant building (i.e. all restaurants). Any questions on parking?

The minimum front setback is forty-five (45) feet in this zoning district and the front setbacks on the car wash are eighty-six and a half (86.5) feet. The convenience store is one hundred ninety-eight (198) feet and ninety-nine (99) feet to the canopy of the gas station. The side minimum in the district is twenty (20) feet. The side proposed for the car wash is one hundred thirty-two (132) feet and thirty-one and a half (31.5) feet to the canopy and then seventeen (17) feet to the southern property line. The side for the retail convenience store/restaurant building is twelve (12) feet on the north side of that building and then ninety-eight (98) feet on the south. (The side setback requests) have to do with that kind of center line lot division that is happening between that the two lots, which we see quite a bit. The rear minimum is twenty (20) feet. The rear proposed from the car wash is seventy (70) feet and forty-three (43) feet to the pay station building that's in the top right corner of that image. The rear proposed retail is thirty-seven (37) feet and ten (10) feet to the waste receptacle, which that ten (10) feet also meets code for our accessory structures. The maximum height on the building in this district is thirty-five (35) feet. Attorney Daniel indicated: the proposed height is twenty-nine (29) feet.

Director King said, the impervious maximum percent on this site is sixty-five (65) percent. I included this here because I thought was important to show that they have gone down quite a bit from the last plan. This is an aggregate number for the two (2) lots. When we did this analysis in 2022, it was pretty intensely looked at for the impervious. To make sure that the design basin in the original Menards PUD could handle overage on all lots that were designed as they were designed with; Jeff Miller (the civil engineer), said run off coefficients. Even if everything was built out at seventy-five (75) percent and assuming one hundred (100) percent for Menards, the overall impervious for the entire PUD was at sixty-eight (68) percent. The design coefficient runoff amounts were over what is being proposed for these lots. Our engineers looked at all of this back then and in fact in this case now, this plan is well below (impervious calcs) what they were even proposing back in 2021/2022. Staff doesn't have any concern with this request for this case.

For landscaping on the property, the code requires parking lot landscaping in the islands. Buffering is required between residential and commercial and they have provided buffering along the east property line. I believe that landscape portion has not changed from the original plan, Attorney Daniel? Attorney Daniel said, it's a bit denser with more trees. Director King asked, then it was in 2021/2022? Attorney Daniel said, the northeast corner we eliminated parking so you have four (4) new trees and additional shrubs. Director King asked, near Bell Road, that bottom corner? Attorney Daniel said, yes. Director King said, the Village ordinance does not require foundation landscaping. It also does not require the corridor planting that you

see kind of along the roadway. I think the level of landscaping in the landscaping islands is sufficient. I think short of the Village getting around to adopting a landscape code, where we're looking at things above and beyond basic foundation planting. What is in here is well above and beyond what is code required. A lot of this design was in the original plan as well. Obviously, that just changed. We have asked them to continue the heavy landscaping around the convenience store in this plan, just like we did in the plans from the previous iteration. The zoning regulations require a thirty (30) foot front yard landscape area. They are requesting a reduction along that frontage, which they requested in the previous iteration of the plan. This depth matches the same depth along the front of the rest of the Menard's PUD.

I think this rendering gives you an idea, but you can obviously tell that there's no landscaping in it. Attorney Daniel brought some materials for us to look at for the building because the materials and the color palette has changed from 2021/2022. I don't know if you remember, but it was more like a brown and there was some teal, I think, in the awnings. The buildings now are more of a stone and then black accents. The canopy that you're seeing, I'll let Attorney Daniel speak to the aesthetic of this canopy. The one thing I did guide him on with this canopy, when we had Gas N Wash come in, we did not allow them to have this bright color (band). They came in with a red band on the canopy and we had them come back with something that would match the building. I alerted the applicant to that just because it's a precedent. Staff also verified, that these bands are not being proposed to be lit, as that is not allowed per code. I did under some Google searching see that Mobil canopies have some installations where there are color bands are lit up. Th applicant is aware of that. So, will the blue be on there? Is that what's being proposed?

Attorney Daniel said, good evening, everyone. On this particular canopy we're proposing that there be the trade name and an extent of blue. I think we've mentioned in the narrative sixty (60) percent coverage or tapering it off from two (2) corners. It wouldn't be entirely wrapped. Director King said, we should talk about that.

Vice-Chair McGary asked, so the white, gray, and the blue?

Attorney Daniel said, right. When I say blue, the one thing that Director King mentioned, we cannot tell you for certain it's going to be a Mobil. We think it will be. We think the odds are that it will be, but franchises or brands don't allow a site to go forward until after they see the plan and can get their full permit package in. We think it's going to be a Mobil. We think this fits everything, but we are showing their trade dress because of that likelihood. It's possible that it may not be a Mobil, but we would comply with that same tapering condition (if added). We would have the sign on two (2) faces like we show and then taper whatever that trade dress color is as you move away from the trade name. It would fade to something that's more solid, which is what Director King is talking about.

Director King said, the other thing, the pumps that are in this rendering, I asked Attorney Daniel to have brick wrapped at the pumps. We see this application in the higher end gas stations. This is obviously in a very prominent area of the village. And so we want to put our best foot forward if it's going to be a gas station. The rendering doesn't show (the stone wrap).

These are the architectural elevations for the convenience store and the restaurant. I will note that the restaurant has its own separate entrance, but inside I don't actually have the floor plans they're in the packet. Currently shown inside the space like a half wall separation between the two (businesses), but still connected. It's not a mixed use building with a full wall separation, for

now. But the restaurant does have its own entrance store and they also have a drive thru on the south side.

This is pretty in keeping, I would say, with what we had asked before in the past with regard to masonry upgrades. And the car wash is not a whole lot different, although I think what they're proposing now and I've asked is to make sure that the two buildings speak to each other, so that the materials on them will be the same.

Attorney Daniel stated, we had committed to the stone wrap before. Staff stated, we need to add that to the conditions of the of the painting if that is to be considered.

I just want to make sure, there was originally a request for a setback reduction (for the monument sign on lot 4), but the site plan that I have pulled up appears to meet code, is that correct?

Attorney Daniel indicated, we would possibly move it depending on lines of sight into that intersection. You may pull it back and move it north. So there was a bit of a setback issue. We think it will comply, but there's a possibility during permitting you might find a better view.

Director King indicated if we're going to do that then I think I think you need to ask for the absolute minimum necessary.

Attorney Daniel stated: We were just trying to give staff and Menards and the applicant the best flexibility we were comfortable with this position. But if Menards and the village are looking at options, if something were to change with the Glengary intersection, we can we can keep it where it is now, but it would really be at the request of staff that we would go down to the one foot. We're not proposing that we move it. The flexibility during permitting might prove to be important and that might be the minimum relief is to make sure that staff's request to get down to that foot or even go below where it's located.

Otherwise, the monument signs still meet all of our regulations with regard to copy height masonry base. We have a one-foot masonry base required on masonry on monument signs.

Staff doesn't have any issue with this digital messaging in case anybody has a question, digital messaging is not allowed in the village except on gas station signs.

Commissioner McGary asked, do we have a maximum height of the pricing because, it's like six inches, the size of the numbers with the price, it just seems kind of a little hard to see. Is that through the sign ordinance or just the design of the scale? Is that standard?

Attorney Daniel stated it varies depending on the street that you're on. If you're in a commercial district where speed is slowing, you can go with smaller. But if you're on a 45 mile an hour road, you could be at eight inches or larger.

Commissioner McGary was just curious why they're small and within requirements sufficiently.

Attorney Daniel stated I think we have had a larger base on this one than required.

Director King continued, a foot of base is required. But there are a few adjustments could be made without exceeding any sign regulations and having to come back if we if we did need to go larger.

There is a request to have that signage on Lot 4 because you can't have an accessory structure on a property that doesn't have a principal use or principal structure. We can have Attorney Daniel talk to their discussions with Menards about the remainder of Lot 4 and the future of it. But I think that if they owned all this property along the front, the sign code would allow two monument signs as long as there is 300 feet in between.

The wall signage has changed quite a bit and has actually gone down in exception requests. The elevations on the multitenant building (from 2021/2022) had four copy overages that were being requested at the time because of the narrowness of each tenant space, we deal with this with lots of signage on narrow tenant frontage. Everybody I think at the planning Commission at the time felt like these signs looked fine proportion wise with the building.

Commissioner Stanley asked if the Plan Commission had previously asked to put a condition on how many signs could say "free". Is that still in here?

Director King was not sure if we put that in the conditions, it rings a bell, but I forgot what for. There is a free parking sign on one of the towers in the north building. But this is essentially the "frees" than the old plans.

Attorney Daniel indicated I think we may have had a canopy sign proposed that we removed during the process. We sometimes put them above on the sunshades. Now you just have the one. There's one on each elevation. There is one on the north elevation, one south elevation as was shown here.

Director King continued, the all the copy here is well below what's allowed by code and then the two signs on the main entrance are fine. Our new code allows for a sign for each separate business can have a sign on the outside of the building and then they're subject to maximum copy. The signs meet code and this one the copy is fine, but the additional wall sign on each elevation is being requested, which was being requested previously as well. And this is the same request as before with regard to the narrow frontage of that car wash and the overage and size. Staff did not have a problem with this request. I believe when I requested this (exception) before it was at 90sf, and I think that's because I included the exit sign for some reason, we do not include exit signs as part of sign copy.

The waste enclosures were relocated from an original plan. That concluded staff's presentation.

A motion was made to open the public hearing by Commissioner Stanly, seconded by Commissioner Bradarich all in favor, zero (0) opposed. Motion carried. Well, we will hear from the petitioner.

Attorney Daniel introduced Jeff Miller, Civil Engineer, he's been in charge of the site design and overseeing landscaping, which was handled by a licensed landscape architect. Scott Birkland is the broker that's been working on this from day one, starting in 2021, and then behind the two of them is the architect on the project. And I'm Attorney Mark Daniel. I've handled projects various projects. I've done a lot of work around these Menards sites and Home Depot sites. In fact, one of the projects that we relied on for compatibility purposes and some the floor interior design and the building is an open terrace project where we put a BP next to a shell on an old Burger King site that sat vacant for a long time. I think it's important to note a couple of things.

A little background, we acquired Lot 5 using cash. It wasn't intended, but construction expense quite a bit affected this project. The year that it was pending. It took us a while to get to hearing

and we were pushing and staff got us before you and we did get the approval for the prior project. And then we worked on different users, particularly the food and beverage type users.

The retail and the salon were spoken for, but we really couldn't find a food and beverage retailer, so we shifted gears. One of the big pluses here is that everything is going to be owner operated. You'll have related corporations running the property as the ownership side and for tenant operational purposes, the shareholders of the property owner are going to own the buildings and operations. That's pretty important for financing purposes. We ended up in a position where we applied in January and there was this discussion of parking which was rational and we relocated parking and created a few future electronic vehicle spaces in the south east part of our lot to the lower left in this drawing, Lot 4 was always an issue, and we discussed that last time. I think all of you know that there's about a 21 foot drop in grade from where you enter on Glengary and hit that access road that's been planned since 2005 to where the gas station parking is in. This case, it's about a 21 foot drop off and then we get to the north end of the property. And once you get by, Rivera's horse farm, there's another 20 foot drop off.

I'm sorry, ten foot drop off. The site was kind of isolated when we worked with Menards previously. They (Menards) were insisting that we purchase the full lot four and I think that's public knowledge. Staff worked with us. We worked with Menards. Menards may have called staff, but we just couldn't close the gap on that. And eventually after the approval and sometime in January when we closed five, we also signed the contract on the north, generally the north hundred feet of lot 4. And you can see that most of that is still going to stay open space. That did allow us to expand, but we didn't stop trying to market the property for food and beverage. We specifically focused on A and W at that location because we knew they had a smaller footprint or site plan that A and W could pursue. And a client of mine operates three and had a fourth underway in Franklin Park and we had them look at the site and he just said it was too hidden.

Once they created the envelope for the drive thru on the south side, the grade changes required the half million dollar retaining wall that had to be built to hold back that dirt that all created problems with visibility. You know, ultimately it wasn't an attractive site. The corridor itself was attractive. And you know, certainly the response was if you see something else on the road, maybe we can talk. But, you know, this location is a little bit tough. And that's when we really knew that we weren't going to be able to use our portion of Lot four for anything other than as part of this project.

On this landscape plan, there are a handful of important changes. One of the concerns was the four-way intersection that Director King mentioned. We did eliminate parking on the east side parking north of the entrance towards the right side of the entrance in this diagram and replace that with landscaping, that whole North lot line next to the horse farm is landscaped. The gaps that we have there are generally intended to provide stabilization and maintenance opportunities so we can get in and replace things. If we have to. The piece that you see in the outline are existing protected trees and a lot of those trees are on land that Menards owns. There's another 20-foot strip. So when you think about this pond surrounding on the north side and on the west are two strips of land owned by Menards. The west side's a little bit thicker or wider, but we do have that 20-foot strip that you can consider not only for the stormwater and runoff coefficient, but for landscape and buffering. Those trees are Menard's property.

We align the carwash in the same direction that we intended to last time. Out of respect for the folks at the farm, you don't want to have that noise spilling out the exit towards the north and it's going east west in a way that doesn't create any obtrusive annoyance. When you look at the car wash radius leaving, the car wash, that was another concern. We were pinched there. The last project was approved with an area of curb that was a bit narrow. And one of the precautions that KLOA told us to take was to keep an eye on that during busy times and possibly control that traffic movement in what was then a two way drive while essentially a car leaving the car wash that wanted to turn into the convenience or the shopping center area would cross into the other lane of traffic.

The drive thru in this case has been widened. I think there were some concerns. I can't remember if it was at this level or at the village board level, but a truck making that turn around building to the drive thru and pass there was pinched. That's no longer the case in this plan and obviously the drive thru was a lot further removed from the circulation now than the last plan.

On the last plan, the drive thru emptied out directly into the circulation of the access road, so there was little separation. It might have been a car length and a half at that point in time, one parking space plus a sidewalk. So those are pretty solid changes for circulation purposes. On this plan, the overall site is three acres in the packet towards the back.

You'll see a lot of truck templates, you know, the turn radii that we run to see if trucks can get in and out of the property. And there's one that's a failure (for semi-trucks). I think there are two now that are failures and that's intended. When we brought forward this project, we didn't want people to think that it was designed for trucks.

We will sell diesel, but if you try to bring a semi in there, you're not going to fit. The tanker pulls in on a different template or a different turn. The fire trucks come in on a different plan. But no matter how you look at it, a semi-tractor trailer cannot get into that property without running over a sign, without hitting a curb, without destroying landscaping. And that's intentional. Operationally, the underground storage tanks are east of the canopy and the tanker can fit in there with both types of cabs.

Director King said there's no section in the zoning ordinance, what drove all the landscaping?

She says there's no section, but she's really good about demanding the additional landscaping. You have quite a bit more foundation landscaping here as far as the view to Menards, it did take some time to work with them on the landscaping that Director King wanted to see along the street. During the last process and during this process, I think we've added another couple of trees, but Menards really likes to have that view up into their store and they've agreed to go with that. There are height limits on buildings they have to approve, but that landscaping, obstructing the view is something they pay close attention to. But I think I think we got there.

The future EV charging stations are located on the interior of the main lot, but it's possible to put them on the outside of the lot. One might ask why we're continuing with the spaces that are along Bell Road. It's to make sure there's a static continuity. This road is planned with those spaces. The depth of the access road is shown in the original Menards subdivision and the restrictive covenants that apply to the subdivision. They all contemplated the access road where it is with eighteen and a half (18.5) foot spaces. The entire parking field has shorter than nineteen (19) foot parking spaces. But this does match the south end. It's supposed to be a mirrored access road. So when you get down towards Firestone, you'll see this these same

spaces existing today, but with less with less landscaping In general, Scalia's beef would occupy the South unit in the commercial building. The unit is designed with a partial wall.

We preferred the open space, I think from design perspective, it's capable of being extended as a full wall if it's required. In most of the communities where we've had this situation, a lot of folks recognize that some of the gas stations do have really good restaurants or opportunities in some communities have been asked to take those out too, to create a more open space.

And that may come down to a decision on licensing and everything else. But we are able to install that full wall if we need to.

Director King made a few comments that I'll verify the tones that you see, and that's the main reason why we kept the renderings and the plans. So you can see what we intend for building color and tone. The bricks that I was holding up earlier, they're a little bit darker than what we intend to have. That was the closest match that we could find, but we intend to be in the gray palette with white stone, and that's what's shown in those plans. We thought about redoing them, but at this point in the hearing process, it's difficult to get the renderings redone and we still had changes in progress and those aren't cheap. So we gave you the nuts and bolts of it.

Gas station signage typically has a number of variances. I think you're pump instructions are excluded and pump numbers are excluded. But on the canopy we are willing to work with you to go with that fade that I discussed. We don't want to have just the name, the trade name and the gray backing or the white backing. We'd rather have the trade name in a transition to that white backing. I think it's fairly important for us at this point in time because we do have to comply with the same package. And when we when we go to mobile, we see what they expect they don't need the backlit sign or the interior lit sign that will not be a lighted sign.

Otherwise, the canopy has internal canisters and everything else. It's got the director lighting. We are working on supply and costing out those canisters, but we will comply with all the photometric requirements of the village of Homer Glen, our Photometric plan shows that, and however we supply this thing when we're building it, we will still comply. I believe, from an engineering perspective.

Jeff Miller, Engineer: I'll keep it real brief because I know it can get really involved. We are below the runoff coefficient that was the pond was designed for. So that's actually a significant difference when it comes to stormwater management. So we're not going to overburden the detention basin. It's designed for this this type of use, this intensity of use. We don't have any retaining walls on the project. So in lieu of the last petition, there are no retaining walls on here. There's a lot of grade to deal with, but there's no retaining walls.

And so that was quite a feat that allowed us to put some landscaping around the in a more natural condition as opposed to fighting stone behind retaining walls and those kinds of things. We have thirty-four (34) feet plus on north side, in addition to the twenty (20) feet that that Menards owns before we hit the agricultural zoning. We have a total of fifty-four (54) minimum feet of green. There are some existing trees, they're all noted with a P that's protect they are anywhere from eight inches to 16-inch trunk diameter. There's also an existing fence on and our property. And our landscaping is a mixture of deciduous trees as well as the evergreens. You can see it will be a good year-round buffer. And then on the west side is the detention base in which we transition well, grading wise and we have the landscaping in there along that, a five-foot setback. And on the south side, it's fifty (50) feet from the edge of the walk and are eighty-

eight (88) from the walk to the south property of the new parcel. So, lots of green and then 20 feet along Bell Road plus 20 feet where there's parking stalls. The site is well framed and landscaping wise and also that allows us to do the transitions grading wise but sanitary no problem water main it's all been set up the existing sidewalk along Bell Road we're going to extend that to northern property line.

We're not asking for any additional driveways. We're going to keep the right in-right out. It works for trucks that that need to use here and as well as the fire trucks, of course, and the access road to the south is available. We've talked about that great detail in the last petition and we have since enhanced it somewhat. We have curves on both sides of the driveway. Now we have lighting and we have storm sewer to serve the driveway. So, it's a full blown curb roadway section through there and lighting like this. We meet the requirements the Village has a very strict requirements for dark sky lighting. We meet that even though we have a gas station canopy. And that was the hard part is finding the under-canopy lights that would work with all of that. So, we meet that requirement as well. And I think that covers the highlights from an engineering perspective.

Attorney Daniel spoke: We did a lot to try to get more retail into the space if we did acquire a lot four, we got pretty inventive. We just couldn't find the folks to occupy that would allow us to build.

Commissioners asked: So, it is "Breezy?"

Attorney Daniel: Yes. But he does catering and sit down and drive through.

Commissioners stated: Just so you're aware of off in the southwest corner, that's my kids old sledding hill.

Commissioners asked about the free signs.

Attorney Daniel stated I think the approval left it on the buildings and we had discussed putting it above the canopies in some projects. I think our Evanston project, I know often we have it above the canopies.

Director King stated, I don't know if that's even a thing we can do, the legality of that.

Attorney Daniel stated, you know, I think ultimately viewed as that of a concern on our side of things, I think it's important to have at least one of the free vacuum signs, and that would be the one on the exterior, because you want to catch that line of sight. And the vacuums are an important part there are people like me that will vacuum without washing. There are times that I do that and it's a service, it's a membership situation. It is an important aspect of the business. I mean, people could see it but not know they're free. It's an interesting issue to have, but other locations from Skokie to Evanston out West, Montgomery, they've all had no problem with the free vacuum signs. As long as we are within the parameters expected for sign area.

Director King mentioned, we don't have these canopy signs in the sign package and they're not on the elevations anywhere and we've not measured them, I just want you to know that it's twenty (20) percent of the side of a canopy by code, the whole side of the canopy, each side.

So, then these short sides would be measured the same way. So we haven't done analysis on that.

Attorney Daniel stated, we should be well clear. We should be well within, that I think you're literally looking at two corners of the canopy and we should be well within twenty (20) percent.

Commissioner Hand asked are we on to the public now? We're all functioning. Yes? at fifty (50) percent? Yes. Maybe twenty (20) percent, maybe seventy-five (75). Well, we will hear from the public at this time. They're all in bed. Except Sue is still here.

A motion was made to close the public hearing by Commissioner Bugos-Komperda, seconded by Commissioner Stanly all in favor, zero (0) opposed. Motion carried.

Any discussion by the Plan Commission, questions?

Commissioner Bradarich asked, I have a couple of questions, and my questions are directed to you, Jeff, on this rendering, the orientation of the pumps are in conflict the site plan.

So which way are we going? We're only showing you the rendering to show you the tone of the building. Okay? That's literally the only reason. Because the landscape, nothing's there. But it was important to let you see what the building looked like. I will say that project looks dynamite. It is a great job? Site plan looks great. I have one question that I don't understand in the vacuums aisle where the trash enclosure is, there's a bailout lane from the car wash, and that's a one way I imagine it's a one-way lane, is there going to be do not enter signs. I mean, what would keep someone from going in there and just turn it in the car wash and get in there? in the Car wash, I mean? I'm looking at that and thinking there's an opportunity.

Attorney Daniel answered, that's a good question. We normally show they usually put two bollards on either side with a chain across it, we normally show that on the but it's not on there.

Commissioner Bradarich, okay, so it's going to be chained up.

Jeff Miller said, yes. It's just for a bailout, correct. Yeah, it's emergency exit

Commissioner Bradarich said those are my only two questions.

Commissioner Hand asked, there's no plans to have people out, towel drying the cars, right?

Attorney Daniel stated, no, there's no drying service. All right.

Commissioner Hand asked about the tanker circulation, the tanker circulation it obviously takes both lanes of all the ingress and egress. So, is that plan just to be done in off hours?

Applicant responded, yes, typically it's done in off hours. I mean, just get the tanker truck plan on me. It's not quite as bad as semi because it gets in there obviously. So, when it's sitting there, you could still get around it, you know, on the outside. But it's meant to be done during non-peak time. Because if there were traffic in there, yeah. It would just hold everything up right. With hoses some times that laying on the ground.

Commissioner Hand said, well I mean because I've seen tankers at gas stations all times of the day, but as long as they have a place to park, well, you have a place to park. You just don't have a way to get in and out with when they're there.

Applicant: and sometimes those stalls that are next to the pumps, if they're there, they'll put cones up so you don't park in them while they're filling. And they're usually there for an hour and a half, 90 minutes. Sometimes it's sometimes it's an hour in open terrace. What they do is they

know ahead of time the tankers coming in. They place cones at the pumps that have their outermost exposure to the tanker in that project. The tanks are on the south end there on the side that also runs perpendicular to the pumps. So in this case, the tanker would run perpendicular to the pumps. You'd knock out two of the center ones possibly, and a condition, the proper condition would be no fuel loading from within the access aisle.

Attorney Daniel, if you if you want to insert that in the ordinance but it's designed for loading on site.

Commissioner Hand, I'm just looking at that. You've got cars trying to get in and out of the two businesses there and the tanker needs to get in and it just can't happen.

Applicant: typically, it's non-peak and typically they do use the cones on those pumps that would be at the East end in this case.

Commissioner Hand: And then as far as the no semi-truck signs, they wouldn't be able to see the signs until they're in there. Maybe put something up closer to the road, so they make it on the access road. And once they're on the access road, the signs will be oriented so that they would see it before they turn that.

Applicant: I know they always look you'd hope.

Commissioner Hand: Well, what about sidewalk from the Menards parking lot if you're parked over there like I do every day to walk down and have a sandwich, I don't see anything.

Jeff Miller: We don't have it. The grade, it was pretty challenging. If it's designed for cars, it's one thing, but for pedestrians, if you think about winter conditions and the maintenance issues that come up, it's very tricky. And there's a route around the north side of the portion of Lot four that we're not acquiring. So that's something that Menards really didn't to want to have touched. All right. It goes to the right in right out, and then it can come in through our A.D.A. walk. That does go to the building so they could take it, you know, if they want to get a few a few extra steps in.

Commissioner McGary, other than the size of the pricing on the signs. I have no other questions. So. Okay, we closed up.

Staff, Applicant and Plan Commission discussed the need for the one-foot setback for the monument sign and determined it was not necessary. Staff and Plan Commission discussed adding the columns veneer wrap requirement to the motion, as it was not shown on the screen.

Chairman Hand asked for a motion. Commissioner Bradarich made a motion to recommend approval of [1] a Preliminary Plat of Subdivision; [2] a Special Use Permit for a Major Amendment to the Menards Planned Unit Development (PUD) with requested exceptions, as listed in the attached Exhibit A; [3] Special Use Permits for (a) a car wash (an automobile repair, service and body shop use), (b) a drive-through establishment and (c) an automobile service station (gas station); and [4] a Site Plan, for certain real property generally located in the C-2 Local Business District on Lots 4 & 5 of the Menards PUD Subdivision, Homer Glen, Illinois, subject to the following conditions:

- 1. Meet all applicable building codes and apply for all applicable building permits.*
- 2. Meet all applicable engineering codes and requirements for Final Engineering.*

3. *Submit of a Final Plat of Subdivision to the Village Engineer for final review and recording.*
4. *The car wash special use is approved on the condition that the operator will take precautions necessary during the peak season to avoid hazards in the access road or blocking of the access road, including one of more or the following: additional staff will be used to help direct and manage the traffic flow through the site; a staff person will be placed at the internal intersection west of Bell Road to help direct traffic; electing the most efficient time for vacuuming (i.e. before or after a car wash; and/or traffic exiting the car wash will be prohibited from making a left turn to (1) exit the car wash site via the right-turn in/right-turn out access drive or (2) enter the vacuum stations. The operator shall also report to staff upon inquiry concerning the peak season operations and any of the precautions noted above.*

PUD Exceptions:

1. *Reduce the Required Landscape Setback from the required thirty (30) feet to twenty (20) feet.*
2. *Reduce the south side setback for the car wash from twenty (20) to seventeen (17) feet.*
3. *Reduce the north side setback for the restaurant/convenient store from twenty (20) to twelve (12) feet.*
4. *Reduce the parking lot/drive aisle setback from five (5) feet to zero along the property line between Lots 1 and 2.*
5. *Increase the accessory structure height from fifteen (15) feet to twenty (20) feet for the pay canopy.*
6. *Increase the aggregate lot coverage/impervious of Lots 1 and 2 from sixty-five (65) percent to sixty-seven (67) percent and to consider aggregate impervious for the entirety of the PUD.*
7. *To allow for parking on Lot 1 for the benefit of Lot 2.*
8. *Reduce the parking lot stall depth from nineteen (19) feet to eighteen and a half (18.5) feet.*
9. *Allow an additional wall sign on the north and south walls of the car wash.*
10. *Increase wall sign copy area for east elevation of Car Wash: from forty-three (43) square feet to seventy-six (76) square feet.*
11. *To provide masonry veneer on the canopy columns to the height of 12'.*

The motion was seconded by Commissioner McGary. A roll call vote was taken with Commissioners Bugos-Komperda, Foley, Fiskow, Stanly, Bradarich, Vice-Chair McGary, Chairman Hand voting in favor seven (7) to zero (0). The motion passed unanimously and will go before the Village Board on October 11, 2023.

Chairman Hand asked for a motion. Commissioner Foley asked for a motion to adopt staff's findings as the findings of the Plan Commission, [Case No. HG-2301-PUD]. The motion was seconded by Commissioner Stanley. A roll call vote was taken with Commissioners Foley, Stanly, Bugos-Komperda, Fiskow, Bradarich, Vice-Chair McGary, and Chairman Hand voting in favor seven (7) to zero (0). The motion passed unanimously and will go before the Village Board on October 11, 2023.

7) Old Business

None.

Trustee Fialko (2:15:50) this issue was sent back to have it re-worded. Planning Commission voted on this four to zero to adopt. There has been a lot of people come to the podium and claim this was open space already, it was never open space. It was zoned as E1. It is always good to do your homework.

Trustee Consolino (2:17:14) agrees with Trustee Fialko. Finds it odd that none of the extremely loud and vocal campaign supporters for some of the people on this board are not here. A lot of false information was put out there.

Mayor Neitzke-Troiike (2:17:56) said our supporters are not here because they trust us to do the right thing.

Trustee Steilen (2:18:14) added that this property was purchased by voter referendum designated to be open space per state statute. Trustee Fialko wanted to add that the space where Pelican Harry's is at, that portion was sold and it was forty acres of land and you have to be more than that. States if this was open space to begin with they would never have been able to sell the property.

Trustee Mason (2:19:24) mentioned that if this had been designated open space twenty years ago then it should have never been sold off.

The Mayor asked the Clerk to call the Roll on original motion:

Ayes: (6) Trustees Steilen, Mason, Fialko, McNaughton, Consolino, Reynders

Nays: (0)

Abstained: (0)

Absent: (0)

The Mayor did not vote.

The motion was tabled.

5. Motion to approve Ordinance No. 23-062 a request for [1] a Preliminary Plat of Subdivision; [2] a Special Use Permit for a Major Amendment to the Menards Planned Unit Development (PUD) with requested exceptions, as listed in the attached Exhibit A; [3] Special Use Permits for (a) a car wash (an automobile repair, service and body shop use), (b) a drive-through establishment and (c) an automobile service station (gas station); and [4] a Site Plan, for certain real property generally located in the C-2 Local Business District on Lots 4 & 5 of the Menards PUD Subdivision, Homer Glen, Illinois.

Trustee Reynders motioned to approve, Trustee Mason second.

Discussion – Trustee Reynders (2:20:46) this item came before the planning commission and she attended the meeting. This was passed seven to zero with conditions. One condition that she has concern is number 4. The attorney for the project was present and answered questions and concerns. Trustee Reynders is also concerned about the cohesiveness about the brick veneer with other buildings and businesses in the area. The attorney explained it is stone and not brick.

Trustee Mason (2:26:52) mentioned he is pro-business. Appreciates that they have taken into consideration the concerns of traffic congestion. Attorney Daniels mentioned that Menards is supportive of this project.

Trustee McNaughton (2:29:28) said the building looks beautiful and the colors seem neutral. Thanked the builders for choosing Homer Glen to invest in.

Trustee Consolino (2:30:29) asked if the Mayor could ask both Joe Baber and Melissa King what their opinions are about the traffic. Melissa King asked for clarification if she meant the traffic in general, Trustee Consolino said where Trustee Reynders is concerned. Melissa King gave her thoughts and opinion about the traffic coming in off Bell Road. There was a traffic study completed and reviewed.

Trustee Fialko (2:32:14) mentions being one hundred percent for business. Thanked them for investing millions of dollars in Homer Glen. This area is an introduction to Homer Glen. Planning Commission voted seven to zero.

Trustee Steilen (2:33:53) agrees with Trustee Reynders that she was taken by the color of the brick. Requested that the brick go all the way up to the canopy. Voting for this plan, not a liquor license approval or gaming approval.

(2:35:33) Trustee Steilen made a motion to amend the motion to also include masonry veneer on all pillars to the max height possible under the ceiling of the canopy, Trustee Fialko second.

The Mayor asked the Clerk to call the Roll on the amendment:

Ayes: (6) Trustees Steilen, Mason, Fialko, McNaughton, Reynders, Consolino

Nays: (0)

Abstained: (0)

Absent: (0)

The Mayor did not vote.

The motion carried.

The Mayor asked the Clerk to call the Roll on the motion as amended:

Ayes: (6) Trustees Steilen, Mason, Fialko, McNaughton, Reynders, Consolino

Nays: (0)

Abstained: (0)

Absent: (0)

The Mayor did not vote.

The motion carried.

8. Motion to approve the authorization of approval for the Mayor to Sign a Letter Amending the Approved Agreement Between the Village of Homer Glen and The Lakota Group for Professional Land Planner Services for the Visioning Process and Concept Development of a new Town Center. There is no change in the approved budget for the Agreement.

Trustee Reynders motioned to approve, Trustee Mason second.

Discussion – Trustee Reynders (2:39:14) this is basically amending the contract to change looking at eight sites to only one parcel of land.

Trustee Mason (2:40:01) no additional costs, agrees with Trustee Reynders

Attachment 10 - Environmental Impacts (provided by applicant)

NSS GROUP REAUTHORIZATION OF SPECIAL USES, INCLUDING MAJOR AMENDMENT TO MENARDS PLANNED UNIT DEVELOPMENT 13812/13830 S. BELL ROAD (PINS 16-05-02-213-002, -005) LOTS 1-2, RESUBDIVISION OF LOT 5 IN MENARDS

ENVIRONMENTAL IMPACTS (DECEMBER 11, 2025 REVISION)

The proposed development of Lots 1-2 in the resubdivision of Lot 5 and the north 100 feet of Lot 4 in Menards Subdivision relies on longstanding planning and public improvements for traffic, drainage and stormwater, and building arrangement. The project entails stormwater collection within curb lines whereby the storm water will be conveyed utilizing post construction best management practices through the planned connections to the Menards Subdivision detention pond. Water quality from runoff should improve since the water that may not be controlled will fall only on planted areas and the water that falls within the improved area will be better filtered. There is no wetland on site, so there is no concern that federal jurisdiction of the United States Army Corps of Engineers will be an issue.

Operationally, each vehicle wash requires an average of approximately 58-60 gallons of water-estimated to be a minimum of \$17,000.00 per year (based on 51,337 CF per month November-March 2018 and 30,940 CF per month April-October 2019, data found reliable by the Village Board on prior occasions which has not changed significantly since 2022 and 2023). Older car washes, hand washes and the self-serve spray car washes use more water. A vehicle owner that washes his or her own car at home will use roughly six to ten times the amount of water than is used in an automated car wash depending on how he or she controls water.

The car wash will not lead to an accumulation of ice outside the car wash exit. The car wash exit is commonly under the supervision of an employee. During winter months when there is the potential for icing, the combination of blowers and a heater at the exit avoid icing. Additionally, the pace of the conveyor through the car wash can be adjusted as necessary to avoid icing by keeping vehicles in the blower area longer. (This is in addition to the automated stop mechanism when the sensors identify an obstruction at the exit.) The car wash operator also pays attention to the effect of additional heat on nearby snow or accumulating snow and spreads salt or de-icing material when needed. The salt or de-icing material would be consistent with Village requirements and those of the center and these would not cause damage to the exit drive or driveway surfaces.

Following review of the title insurance commitment (provided with the application), the documents referred to in the title insurance commitment (subdivision plat, easements, declarations and amendments as well as the annexation agreement), the surveys, boundary and topographical surveys and site conditions on Lots 1-2 in the resubdivision of Lot 5 as well as the adjacent detention lot and land to the north, there are no known wetlands, floodways, special management areas or indications of historical artifacts. No nearby Indian boundary lines are shown and there are no known cemeteries or burial sites in the history of the subject property or adjoining areas. The environmental condition of the Subject Property is appropriate for the development.

Historical aerial imagery dating back to the 1980s indicates no development of the subject property other than farm buildings and an access road. The presence of an underground heating oil tank is possible, but not likely because the structures were actually primarily located west and southwest of the subject property (in the vicinity of the northeast corner of Menards' outdoor sales area and the south line of the detention basin). Tanks in this area would have been discovered and removed. Regardless, there is no known environmental condition that involves contaminants of concern within the subject property.

Most of the subject property sits 10-12 feet higher than the Ranchos Reveras property to the north. Applicants' obligations are to avoid an increase in the volume or rate of storm water flow to the north and to avoid a change in the direction of the flow of storm water onto Ranchos Riveras' property. A denial of flow could also be a problem if Ranchos Riveras relied on an existing tributary from the subject property (none is known). There is a large volume of storm water that sheet flows onto Ranchos Riveras' property as a result of historic elevations across the subject property and extending north. This water will now be mostly controlled and retained so that sheet flow will be eliminated.

Watermark Engineering Resources has engaged in EcoCAT investigations concerning endangered species. The site has no visible signs of hosting endangered species. The State Historic Preservation Office has not raised any issues since 2022. Due to the existing site work and permitting to date, no historic resources of any kind are expected. Correspondence initiating the EcoCAT and SHPO process is on file with the Village (file re: Ord. No. 23-062).

Soil conditions on site are deemed adequate. Neither building has a basement, but one could be considered if needed. The normal depth of footings is planned and no special design is needed at this location. Applicant will provide access road improvements without drainage. The impervious area presently exists and there has been no abnormal erosion of adjacent land and no notable damage to the access route.

Lighting on the subject property and across the access road extended across Lot 4 meet Village lighting standards (please see final engineering and recall the buffer strip north of Lot 1 that Menards owns). LED lighting will reduce energy consumption and excess bulb replacement. Applicant provided a landscape plan that exceeds Village requirements. Presently, there is no landscaping on the subject property, but Applicant will be adding dozens of shade, ornamental and coniferous trees that will contribute to create a cooling effect and provide screening. While manmade, the sun-shades over the vacuums and the pay station canopy also provide a cooling effect.

The nearly 33-foot wide north planting area will contain significant plantings that do not exist today. Applicant continues the same Bell Road landscape yard as was intended at the location, and it utilizes another planting island on Lot 2 between the access road and the pump islands. Significant shade tree planting will occur along the Bell Road lot line. All unimproved and unplanted areas will feature grass or other natural ground cover.

Planning for the two-lot subarea within the planned development features a shared driveway for equal depths from the shared lot line. This planning reduces the amount of heat

generating hard surface. Additionally, the single joined waste enclosure at the north end of the shared access between the retail building and the car wash allows for a reduction of visual obstructions and avoidance of remnant waste in the area. The exceptions for yards between Lot 1 and Lot 2 also reduces hardscape that can generate heat. Lastly, Applicant has reduced paving for sidewalks along drives and vacuum spaces to further avoid the heat generation.

The alignment of the car wash places the blowers at the exit (east end) from the car wash. Environmental background noise is often louder than the blowers and is always louder than the noise at the west entrance into the car wash. No PA system will be in use at the car wash. Vacuums are not audible at the lot line when in use and they are stored in a continuous air system when not in use. The system and the pumps do not generate noise that will be audible at the lot lines. Lastly, the drive-through menu board and ordering system will not create noise that is audible at the west lot line. Drive-through operations may have impacts that are audible near the west line of Lot 2, but these are ordinary district sounds that are contemplated throughout lots on Bell Road and there is a detention area, parking lot and materials yard for Menards to the west and none of the noise should be audible to others who are not on Lot 2.

The gas station is designed with the purpose of excluding trucks with semi-trailers. This was a Village prerogative shared by Applicant and Applicant remains willing to continue with this design. Although there is no parking for trucks with semi-trailers, these vehicles cannot even access the site. Signs will be posted to properly notify the drivers of these vehicles.

The service station will offer eight (8) pump islands with sixteen (16) pump stalls. Drivers are encouraged to remain at their vehicles while filling their tanks, but the pumps have both automatic shut-off and emergency shut-off. Each pump station will be wired to the inside register for employee communication. Pumps are installed in a slightly raised island area to avoid water infiltration. All installations proceed in accord with manufacturer specifications and with applicable federal, state and local laws and regulations. Upon permanent closure, Applicant is required to take various steps to close, empty and remove tanks and other apparatus. Volumes of gasoline delivered and sold are monitored closely. This monitoring, coupled with other efforts to detect leaks, will protect the public safety and general welfare.

Pump stations will be designed with modern shut-offs and measures to avoid spills during fueling that we are all familiar with. Protocols govern the management and recording of any spill. Additionally, both Lot 1 and Lot 2 will feature sufficient waste receptacles so that any waste is controlled and litter best avoided. Lot 1 and Lot 2 will have distinct waste enclosures.

Building colors reflect earth tones with a primary façade color palette that is lighter and generates less heat. There are no air quality concerns with this development.

Finally, Applicant provided a full set of its final engineering plans with its application for reauthorization of the special uses and major change with exceptions. This engineering plan respects the resubdivision of Lot 5 and the north 100 feet of Lot 4. The engineering plan has also undergone significant review at the Village since Applicant is continuing to plan for utilities and water connections. The circumstance that such detailed engineering exists and would be

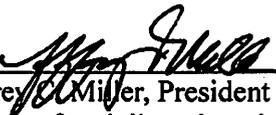
sufficient for permit issuance if building plans were available is an indication of proper respect for natural and environmental concerns as Lot 1 and Lot 2 develop.

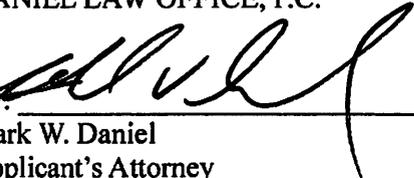
Dated: December 11, 2025

Respectfully submitted,

WATERMARK ENGINEERING RES. INC.

DANIEL LAW OFFICE, P.C.

By: 
Jeffrey Miller, President
(signing for civil engineering only)

By: 
Mark W. Daniel
Applicant's Attorney

MEMORANDUM TO: Mark W. Daniel
Daniel Law Office, P.C.

FROM: Michael A. Werthmann, P.E., PTOE
Principal

DATE: August 30, 2023

SUBJECT: Traffic Statement
Proposed Fuel Center
Homer Glen, Illinois

This memorandum summarizes the results and findings of a traffic statement prepared by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed fuel center to be located in Homer Glen, Illinois. The site of the fuel center is located on the west side of Bell Road directly north of the Menards store. As proposed, the fuel center is to contain the following uses:

- Sixteen passenger fueling positions
- An approximate 5,000 square-foot convenience store
- An approximate 2,710 square-foot restaurant with drive-through facility that will be part of the convenience store
- A car wash

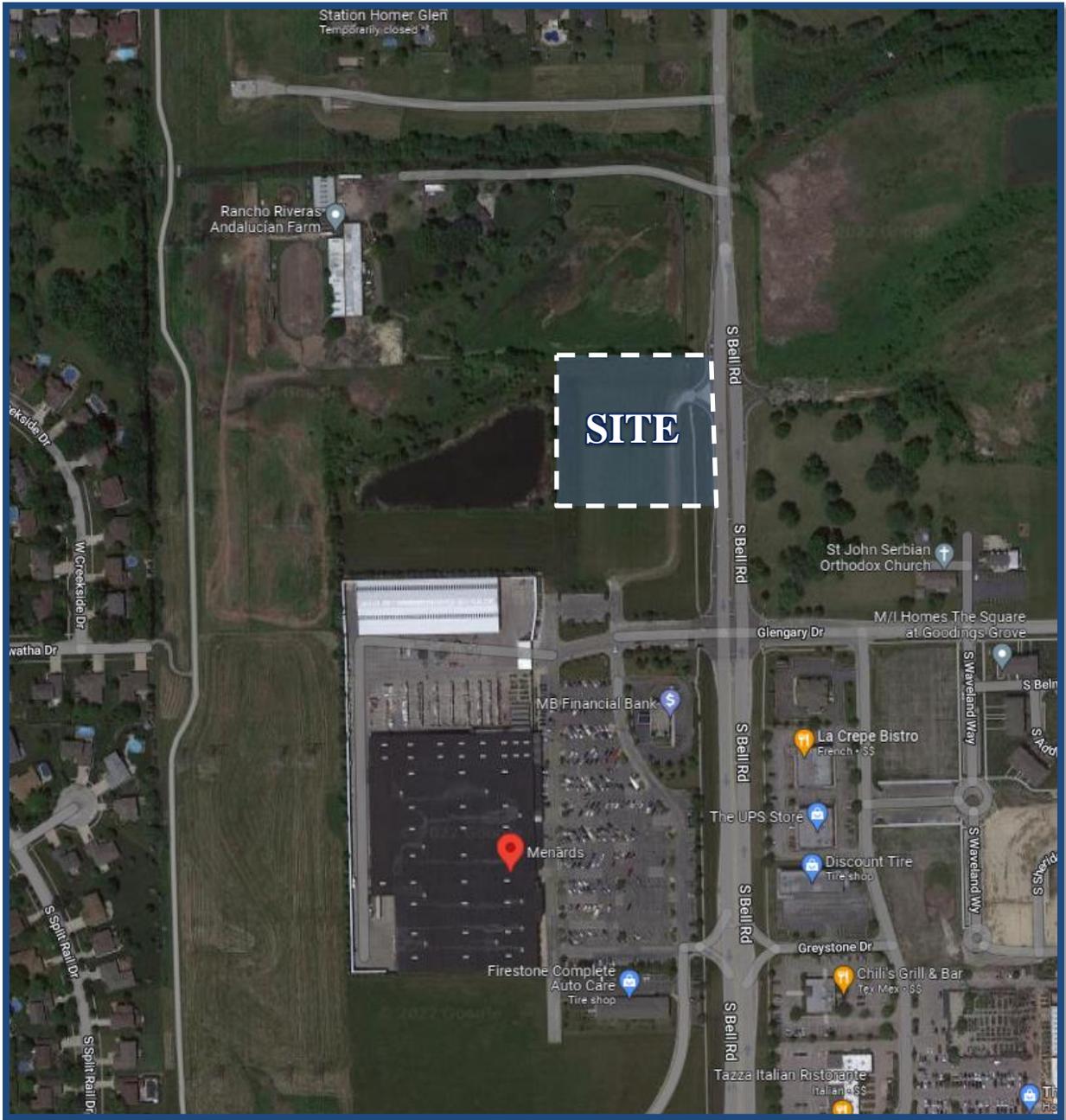
Access to the fuel center is proposed to be provided via a right-turn in/right-turn out access drive on Bell Road and cross-access with the Menards store.

The purpose of this traffic statement was to estimate the traffic to be generated by the fuel center and review the proposed access and circulation system. **Figure 1** shows an aerial view of the fuel center site. A copy of the proposed site plan is located in the Appendix.

Trip Generation Estimates

The number of peak hour vehicle trips estimated to be generated by the proposed fuel center were based on the following rates trip rates published in the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 11th Edition:

- "Convenience Store/Gas Station" (Land-Use Code 945)
- "Fast-Food Restaurant with Drive Through Window" (Land-Use Code 934)
- "Automated Car Wash" (Land-Use Code 948)



Aerial View of Site

Figure 1

It is important to note that surveys conducted by ITE have shown that a considerable number of trips made to fuel centers, car washes, and restaurants are diverted from the existing traffic on the roadway system. This is particularly true during the weekday morning and evening peak hours when traffic is diverted from the home-to-work and work-to-home trips. Such diverted trips are referred to as pass-by traffic. Further, it is important to note that the volume of traffic to be generated by the fuel center will be reduced due to patrons that may visit more than one of the uses proposed as part of the fuel center or patrons of the Menards store that may visit one or more of the uses within the fuel center (multi-purpose trips). **Table 1** shows the estimated total vehicle trips to be generated by the proposed fuel center.

Table 1
ESTIMATED PEAK HOUR TRIP GENERATION

ITE Land-Use Code	Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
		In	Out	Total	In	Out	Total
945	Convenience Store/Gas Station (16 Positions, 5,000 s.f.)	216	217	433	182	182	364
	<i>Pass-By Reduction (60%)</i>	<i>-130</i>	<i>-130</i>	<i>-260</i>	<i>-109</i>	<i>-109</i>	<i>-218</i>
	New Fuel Center Trips	86	87	173	73	73	146
934	Fast-Food Restaurant with Drive-Through (2,700 s.f.)	61	59	120	46	43	89
	<i>Interaction Reduction (10%)</i>	<i>-6</i>	<i>-6</i>	<i>-12</i>	<i>-5</i>	<i>-4</i>	<i>-9</i>
	<i>Pass-By Reduction (50%)</i>	<i>-27</i>	<i>-27</i>	<i>-54</i>	<i>-20</i>	<i>-20</i>	<i>-40</i>
	New Restaurant Trips	28	26	54	21	19	40
948	Automated Car Wash (One Tunnel)	12	12	24	35	36	71
	<i>Interaction Reduction (10%)</i>	<i>-1</i>	<i>-1</i>	<i>-2</i>	<i>-3</i>	<i>-4</i>	<i>-7</i>
	New Car Wash Trips	11	11	22	32	32	64
	Total New Trips	125	124	249	126	124	250
	Total Pass-By Trips	157	157	314	129	129	258
	Total Fuel Center Trips	282	281	563	255	253	508

External Site Access

Access between the external roadway system and the fuel center is proposed to be provided as follows:

- An existing right-turn in/right-turn out access drive located on the west side of Bell Road at the north end of the site. The access drive provides one inbound lane and one outbound lane that are channelized and signed to prohibit inbound and outbound left-turn movements. The outbound lane should be under stop sign control. In addition, an approximate 80-foot separate right-turn lane with an approximate 135-foot taper is provided on Bell Road serving the access drive.
- Cross-access with the existing Menards store which is located directly south of the site. The Menards store has a full access drive with Bell Road that is under traffic signal control at the north end of the Menards site and just south of the subject site. In addition, the Menards store has a restricted right-turn in/right-turn out access drive at the south end of the Menards site.

Internal Circulation Review

As proposed, the car wash is to be located on the north side of the overall site and the fueling positions, convenience store, and the restaurant are to be located on the south side of the overall site. A north-south, two-way circulation road is proposed to extend along the east side of the overall site from the car wash approach/stacking lanes and the right-in/right-out access drive to the Menards site. The north-south circulation road will provide access to all of the fuel center uses and cross-access between the fuel center and the Menards site. In addition, 18 perpendicular parking spaces are proposed to be located along the east side of the north-south circulation road. The following summarizes the circulation for both the car wash (north) site and the rest of the fuel center uses (south) site:

Car Wash (North) Site Circulation

The car wash tunnel will extend in an east-west direction and will be located on the south end of the car wash site with the entrance to the car wash on the west side of the tunnel and the exit from the car wash on the east side of the tunnel. A parking lot with 21 vacuum stations and two ADA parking spaces will be located on the north side of the car wash tunnel with access provided via the north-south circulation road opposite the restricted right-turn in/right-turn out access drive. Three one-way, westbound approach/stacking lanes and pay stations are proposed to be located on the north end of the car wash site directly north of the vacuum stations. Access to the approach/stacking lanes will be provided via the north end of the north-south circulation road.

Except for the vacuum station parking lot, the car wash is proposed to provide a one-way, counterclockwise circulation system. Access to the car wash tunnel entrance is proposed to be provided via the three one-way, westbound approach lanes that will be located along the north side of the car wash site with access provided via the end of the north-south circulation road. Immediately after the pay stations, the three approach lanes will narrow to one lane that leads to the entrance of the car wash tunnel. Vehicles are proposed to exit the car wash tunnel at the east side of the tunnel via the north-south circulation road and will either exit the car wash site or use the vacuum stations.

As proposed, the single-lane automatic car wash will be an exterior-only car wash system. Manual and automated controls will increase, decrease, or stop vehicle flow through the car wash. Each of the three approach lanes are proposed to be individually gated with a separate automatic pay station. The gates will meter the traffic flow proceeding to the car wash tunnel entrance and will open in sequence based on the order of vehicle arrival. Once the gate is lifted for the respective lane, the individual vehicle will proceed south and then east to the entrance to the car wash tunnel. At the entrance to the car wash tunnel, the driver will remain in the vehicle and the car wash will automatically pull the vehicle through the car wash.

According to the site plan, a total of approximately 30 to 31 vehicles can be accommodated in the three approach lanes while advancing to the three pay stations. In addition, approximately eight to nine vehicles can be accommodated between the pay stations and the entrance to the car wash tunnel. As such, the car wash will provide stacking for a total of approximately 38 to 40 vehicles.

Fueling Positions, Convenience Store, and Restaurant (South) Site Circulation

As proposed, parking for the south site is to be provided on both the east and west sides of the south side of the site and along the front of the convenience store/restaurant. In addition, 18 parallel parking spaces will be provided along the east side of the circulation road. Further, a drive-through facility is proposed to extend along the north, west, and south sides of the convenience store/restaurant. Circulation through the south site will be provided via two-way, circulation roads and parking aisles except for the drive-through facility, which is proposed to operate in a one-way, counterclockwise direction along the north, west, and south sides of the convenience store/restaurant.

“Do Not Enter” signs facing south should be provided at the south end of the drive-through facility. According to the site plan, the drive-through facility will accommodate a total of approximately 11 vehicles and will have a by-pass lane.

Internal Four-Legged Intersection

As proposed, an internal four-way intersection will be located just west of the right-turn in/right-turn out access drive's intersection with Bell Road. The right-turn in/right-turn out access drive (east leg), the access drive to the car wash approach/stacking lanes (north leg), the access drive to the vacuum stations (west leg), and the two-way circulation road (south leg) will make up the internal intersection. The overall operation of this internal intersection will be enhanced as the north leg of the intersection, which serves the car wash approach/stacking lanes, will provide access for northbound traffic. As a result, the number of turning movements at the internal intersection will be reduced. Further, a separate right-turn lane is provided on Bell Road serving the access drive, which allows the development traffic entering the access drive to decelerate outside of the through lane. In addition, to further enhance the operation of the intersection, it is recommended that the following measures be implemented at the internal intersection:

- The north, west, and south legs of the intersection should be under stop sign control, which will permit the inbound movements from Bell Road to the development to operate under free flow conditions.
- “Do Not Block Intersection” signs should be located on the north, west, and south legs of the intersection.

Mitigation Measures

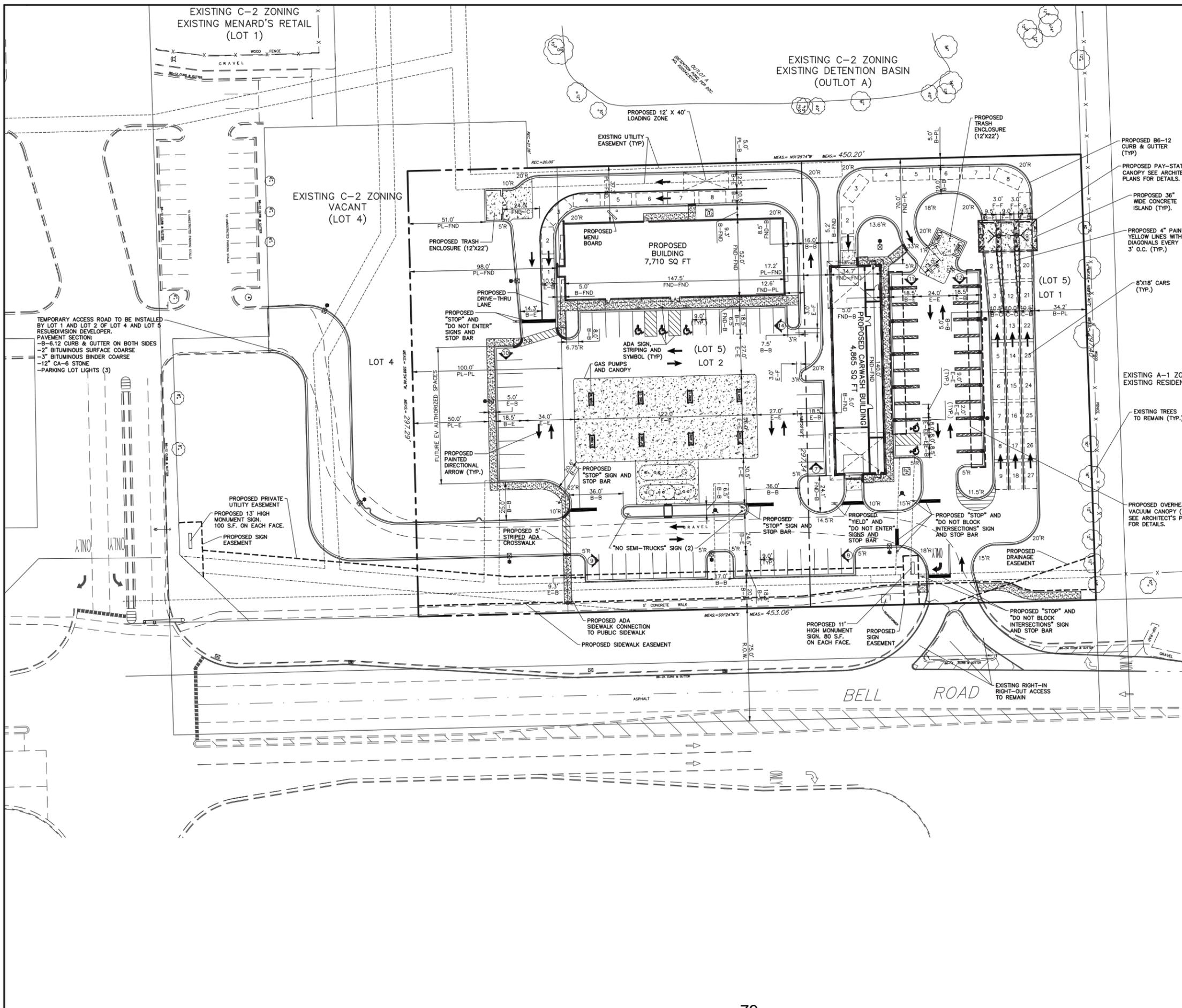
During peak days at the car wash, which typically occur during the winter months, the following measures will be implemented:

- Additional staff will be used to help direct and manage the traffic flow through the site.
- If necessary, a staff person will be placed at the internal intersection west of Bell Road to help direct traffic through the intersection and manage the intersection's operations.
- If necessary, traffic exiting the car wash will be prohibited from making a left turn to (1) exit the car wash site via the right-turn in/right-turn out access drive or (2) enter the vacuum stations, which will minimize the turning conflicts at the internal intersection west of Bell Road. All traffic exiting the car wash building will be required to make a right turn onto the north-south circulation road and exit the development via the cross-access with the Menards store. The traffic exiting the car wash building will be directed to turn right via a combination of temporary signage and/or additional staff.

The implementation of these measures will improve access to and from and circulation through the car wash site as well as enhance the operation of the internal intersection west of Bell Road.

Further, it should be noted that the intersection of the north-south circulation road with the exit from the car wash building will be located just north of the intersection of the north-south circulation road with the east-west, two-way circulation road that will extend along the north side of the south site. The proximity of the two intersections should not pose any operational issues due to (1) the car wash access drive will be restricted to one-way, eastbound traffic flow which will minimize the turning movements at its intersection with the north-south circulation road, (2) the lower volume of traffic projected to be traveling eastbound along the south site's east-west circulation road, and (3) that good sight lines will be provided between the two intersections. Nevertheless, the operation of the development's internal circulation, particularly the proximity of the two intersections, should be monitored in the future to determine that it is providing efficient and orderly circulation. If necessary, consideration should be given to converting the two-way circulation road along the north side of the south site to one-way, westbound traffic flow. The conversion to one-way circulation will further minimize any impacts due to the proximity of the two intersections by decreasing the turning movements at the intersection of the north-south circulation road and the south site's circulation road.

Appendix



LOCATION MAP

SECTION: 17 TOWNSHIP: 36N RANGE: 11E

GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE (PLAT SURVEY) (SURVEY PROJECT #22-22484 DATED 04/08/22) PREPARED BY: GENTILE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6262
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

SITE DATA

TOTAL PROPERTY AREA = 134,282 S.F. (3.08 AC.)
 TOTAL IMPERVIOUS AREA = 90,386 S.F. (2.07 AC.)(67.1%)
 PARKING REQUIRED = 39 (SEE P.U.D. DOCUMENTS)
 PARKING PROPOSED = 88 (INCLUDES 16 PUMP AND 22 VACUUM SPACES)

LOT 1 DATA

LOT 1 AREA = 56,156 S.F. (1.29 AC.)
 IMPERVIOUS AREA = 35,863 S.F. (0.82 AC.) (63.9%)
 VACUUM SPACES (INCLUDES 1 ADA SPACE) 22
 ADA ACCESSIBLE SPACES 1
 STANDARD SPACES 3
 TOTAL SPACES 26

BUILDING AREA = 4,865 S.F.
 1 STORY, 30'-11" TALL BUILDING
 F.A.R. = 0.09

LOT 2 DATA

LOT 2 AREA = 78,126 S.F. (1.79 AC.)(INC. ADDITIONAL 100 FT.)
 IMPERVIOUS AREA = 54,426 S.F. (1.25 AC.) (69.7%)
 ADA ACCESSIBLE SPACES 3
 PUMP STATIONS 16
 STANDARD SPACES 43
 TOTAL SPACES 62

BUILDING AREA = 7,710 S.F.
 1 STORY, 22'-1" TALL BUILDING
 F.A.R. = 0.10

NO.	REVISIONS	DATE
1	PER VILLAGE AND CLIENT REVIEW	5/17/2022
2	PER VILLAGE COMMENTS	6/3/2022
3	PER VILLAGE COMMENTS	7/28/2022
4	PER PLAN COMMISSION	8/15/2022
5	PER CLIENT AND MENARDS	12/21/2022
6	ADDED TWO POLES FOR ACCESS DRIVE	5/6/2023
7	ADDED PROPERTY AND EXPANDED GAS STATION	6/9/2023
8	PER VILLAGE COMMENTS	8/2/2023

Prepared For:

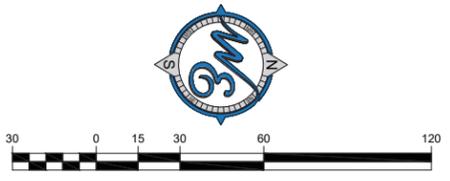
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077

HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

Prepared By:

Watmark Engineering Resources
 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-8800
 www.watmark-engineering.com

CHECKED BY: J. MILLER
 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074



SITE PLAN



Homer Glen, IL

Visit our website @ www.scalisbeef.com

Follow us on [Instagram](#) or Like us on: [Facebook/scalisbeef](#)



FULL MENU AVAILABLE:
 Sunday – Thursday: 10am – 10pm
 Friday – Saturday: 10am – 11pm



Menu

Italian Beef – 6"	\$5.49	Chicago Style Dog	\$2.79
Senior Beef – 9"	\$7.69	Double Dog	\$3.59
Add Peppers (Sweet/Hot)	.50	Served on a Poppy Seed bun, includes: Mustard, Relish, Diced Onions, Tomatoes, Kosher Pickle, Celery Salt, & Sport Peppers	
Add Cheese (Cheddar/Mozzarella)	.50		
Char-Grilled Italian Sausage	\$4.59		
Double Sausage	\$6.59		
Add Peppers (Sweet/Hot)	.50		
Combo		Maxwell Street Polish Sausage	\$4.29
Italian Beef & Char-Grilled Sausage – 6"	\$6.49	Served with Grilled Onions & Mustard	
Senior Combo – 9"	\$8.79		
Add Peppers (Sweet/Hot)	.50		

Combo Menu

COMBO MEALS INCLUDE FRIES & FOUNTAIN DRINK

#1 – Italian Beef Combo	Small	\$7.99
	Large	\$8.69
#2 – Sausage Combo	Small	\$6.99
	Large	\$7.69
#3 – Beef/Sausage Combo	Small	\$8.99
	Large	\$9.69
#4 – Hot Dog Combo	Small	\$5.29
	Large	\$5.99

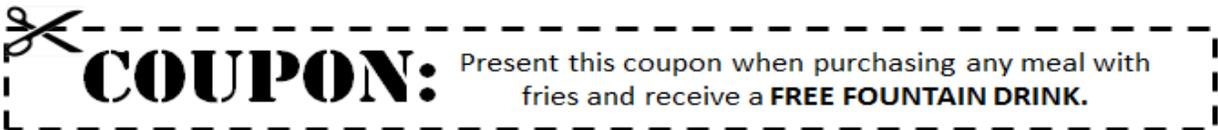
Side Orders

French Fries	Small	\$1.69
	Large	\$2.19
Cheese		.50
Tamale		\$1.99
Pizza Puff		\$2.79
Onion Rings	Small	\$1.69
	Large	\$2.19

Fountain Drinks

24oz Small	\$1.59
32oz Large	\$1.99

Coke | Diet Coke | Cherry Coke | Sprite
 Root Beer | Orange Fanta
 Hi-C Fruit Punch | Minute Maid Lemonade







Mobil

Synergy

Mobil

Synergy

Mobil

Synergy

Mobil

Synergy

Synergy

Synergy

Synergy

Seal's
BEER

BREEZY'S





RECEIVED
By Christopher Gruba at 8:28 am, Nov 26, 2025

FINAL ENGINEERING PLANS

FOR

CAR WASH / GAS STATION - HOMER GLEN, IL

AT

NWC OF BELL ROAD AND GLENGARY DRIVE- LOT 5
VILLAGE OF HOMER GLEN
WILL COUNTY, ILLINOIS

INDEX		REVISIONS						
CIVIL ENGINEERING PLANS		1	2	3	4	5	6	7
C-1	COVER SHEET							
C-2	GEOMETRIC PLAN							
C-3	GRADING PLAN							
C-4	ACCESSIBLE ROUTE GRADES & DETAILS #1							
C-4.1	ACCESSIBLE ROUTE GRADES & DETAILS #2							
C-5	UTILITY PLAN			X				
C-6	PHASE 1 SOIL EROSION CONTROL PLAN			X				
C-7	PHASE 2 SOIL EROSION CONTROL PLAN			X				
C-8	SOIL EROSION CONTROL DETAILS AND SPECS							
C-9	PROJECT DETAILS #1							
C-10	PROJECT DETAILS #2							
C-11	IL AMERICAN WATER - DETAILS & SPECS #1							
C-12	IL AMERICAN WATER - DETAILS & SPECS #2			X				
C-13	PROJECT SPECIFICATIONS							
SUPPORTING DOCUMENTS		REVISIONS						
		1	2	3	4	5	6	7
1 of 2	ALTA/NSPS LAND TITLE SURVEY	X						
2 of 2	ALTA/NSPS LAND TITLE SURVEY		X					
L-1	LANDSCAPE PLAN							
L-2	LANDSCAPE PLAN DETAILS							
1 of 2	PHOTOMETRIC PLAN							
2 of 2	PHOTOMETRIC PLAN-PROPERTY LINE EXHIBIT							
1 of 1	STORMWATER EXHIBIT							
1 of 1	BMP OPERATION & MAINTENANCE PLAN			X				

NOTE: THE DESIGNS CONTAINED IN THE ABOVE PLANS AND SUPPORTING DOCUMENTS WERE PREPARED WITH THE UNDERSTANDING THAT THEY WOULD BE USED AS A WHOLE PLAN SET. EACH CONSTRUCTION DISCIPLINE IS TO USE ALL THE PLANS AND SUPPORTING DOCUMENTS TOGETHER AS A WHOLE AND NOT AS SEPARATE DOCUMENTS. EACH CONTRACTOR IS TO BECOME COMPLETELY FAMILIAR WITH THE WHOLE PLAN SET AND THE EXISTING SITE CONDITIONS. SHOULD ANYTHING WITH ALL THESE PLANS AND SUPPORTING DOCUMENTS BE INCONSISTENT WITH THE SITE CONDITIONS THEN THE CONTRACTOR IS TO CONTACT THE ENGINEER IMMEDIATELY BEFORE ANY CONSTRUCTION IS STARTED.



BENCHMARK	
REFERENCE BENCHMARK: NGS PID DN4689	
THE STATION IS LOCATED ABOUT 7.2 MILES (11.6 KILOMETERS) NORTH OF MOKENA, 4.9 MILES (7.9 KILOMETERS) WEST-SOUTHWEST OF PALOS PARK AND 4.2 MILES (6.8 KILOMETERS) SOUTH-SOUTHEAST OF SAG BRIDGE. THE STATION IS IN THE NORTHEAST QUADRANT OF WILLY/COOK ROAD AND WEST 143RD STREET. IT IS 45 FEET (13.7 METERS) EAST OF WILLY/COOK ROAD, 51 FEET (15.5 METERS) NORTH OF WEST 143RD STREET, 1 FOOT (0.3 METERS) EAST OF BACK OF SIDEWALK AND 1 FOOT (0.3 METERS) SOUTHWEST OF AN ORANGE CARSONITE MARKER. NOTE-ACCESS TO THE DATUM POINT IS THROUGH A 6-INCH (15 CENTIMETERS) LOGO CAP AND THE ROD (DATUM POINT) IS SURROUNDED BY A FLOATING BRONZE DISK TO AID IN IDENTIFICATION.	
ELEVATION =	706.18 (NAVD 88 DATUM)
SITE BENCHMARK:	
SQUARE CUT ON THE EAST SIDE OF TRAFFIC LIGHT LOCATED AT NORTHWESTERLY CORNER OF BELL ROAD AND GLENGARY DRIVE.	
ELEVATION =	705.83 (NAVD 88 DATUM)

LEGEND			
EXISTING		PROPOSED	
SYMBOL	DESCRIPTION	DESCRIPTION	SYMBOL
□	UTILITY PEDESTAL	CATCH BASIN	□
○	UTILITY POLE	INLET	○
○	LIGHT STANDARD	STORM MANHOLE	○
○	TRAFFIC SIGNAL CONTROL VAULT	SANITARY MANHOLE	○
○	TRAFFIC SIGNAL MANHOLE	VALVE VAULT	○
○	TELEPHONE SERVICE VAULT	FIRE HYDRANT	○
○	FIRE HYDRANT	FLAMED END SECTION	○
○	WATER SERVICE SHUTOFF VALVE	ELECTRICAL POWER POLE	○
○	WATER VALVE VAULT	OVERHEAD TRAFFIC SIGNAL	○
○	CLOSED COVER DRAINAGE STRUCTURE	TRAFFIC SIGNAL MANHOLE	○
○	OPEN COVER DRAINAGE STRUCTURE	CABLE TELEVISION PEDESTAL	○
○	CURB INLET/CATCH BASIN	COMMONWEALTH EDISON MANHOLE	○
○	SANITARY SEWER MANHOLE	LIGHT POLE	○
○	GAS SERVICE SHUTOFF VALVE	BOLLARD POLE	○
○	CLEANOUT	GAS MARKER	○
○	WATER SHUTOFF VALVE	ELECTRIC MARKER	○
○	WITH 8" CASING	TELEPHONE MARKER	○
○	ELECTRIC SERVICE VAULT	ELECTRIC LINE	—
○	BOLLARDS	TELEPHONE LINE	—
○	FLAG POLE	CABLE TV LINE	—
○	TREE	SANITARY SEWER	—
○	STORM SEWER	STORM SEWER	—
○	SANITARY SEWER	WOOD FENCE	—
○	UNDERGROUND ELECTRIC	CHAIN LINK FENCE	—
○	UNDERGROUND TELEPHONE	METAL GUARDRAIL	—
○	GAS MAIN	CONCRETE SURFACE	—
○	WATER MAIN	CONTOUR LINE	—
○		FRESHED FLOOR ELEVATION	FF
○		PAVEMENT ELEVATION	P
○		GROUND ELEVATION	G
○		TOP OF WALK ELEVATION	TW
○		TOP OF RETAINING WALL ELEVATION	TRW
○		FLOW LINE ELEVATION	FL
○		TOP OF CURB ELEVATION	TC
○		TOP OF FINISH ELEVATION	TF
○		DOWNSPOUT LOCATION	ME
○		RIM ELEVATION	R
○		PERMANENT AREA SLOPE DIRECTION	D.S.
○		PAVEMENT SLOPE DIRECTION	P.S.
○		OVERLAND OVERFLOW DIRECTION	O.O.
○		INLET PROTECTION	IP
○		INLET BASKET FILTER	IBF



STATE OF ILLINOIS) SS
COUNTY OF KANE)

JEFFREY C. MILLER
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

I, JEFFREY C. MILLER, A LICENSED PROFESSIONAL ENGINEER OF ILLINOIS, HEREBY CERTIFY THAT THESE CIVIL ENGINEERING PLANS, NOT THE SUPPORTING DOCUMENTS, AS LISTED IN THE INDEX, HAVE BEEN PREPARED BY WATERMARK ENGINEERING RESOURCES, LTD. UNDER MY PERSONAL DIRECTION. THESE PLANS ARE INTENDED TO BE USED AS AN INTEGRAL PART OF THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THE DRAINAGE OF THE SURFACE WATERS WILL NOT BE CHANGED BY CONSTRUCTION OF THIS DEVELOPMENT, OR ON THIS SITE, OR ANY PART THEREOF, OR THAT IF SUCH SURFACE WATER DRAINAGE WILL BE CHANGED, REASONABLE PROVISIONS HAVE BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS OR DRAINS WHICH THE PROPERTY OWNER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL BE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO THE ADJOINING PROPERTY BECAUSE OF CONSTRUCTION OF THE DEVELOPMENT, OR THIS SITE.

Jeffrey C. Miller DATE: 11/22/2024

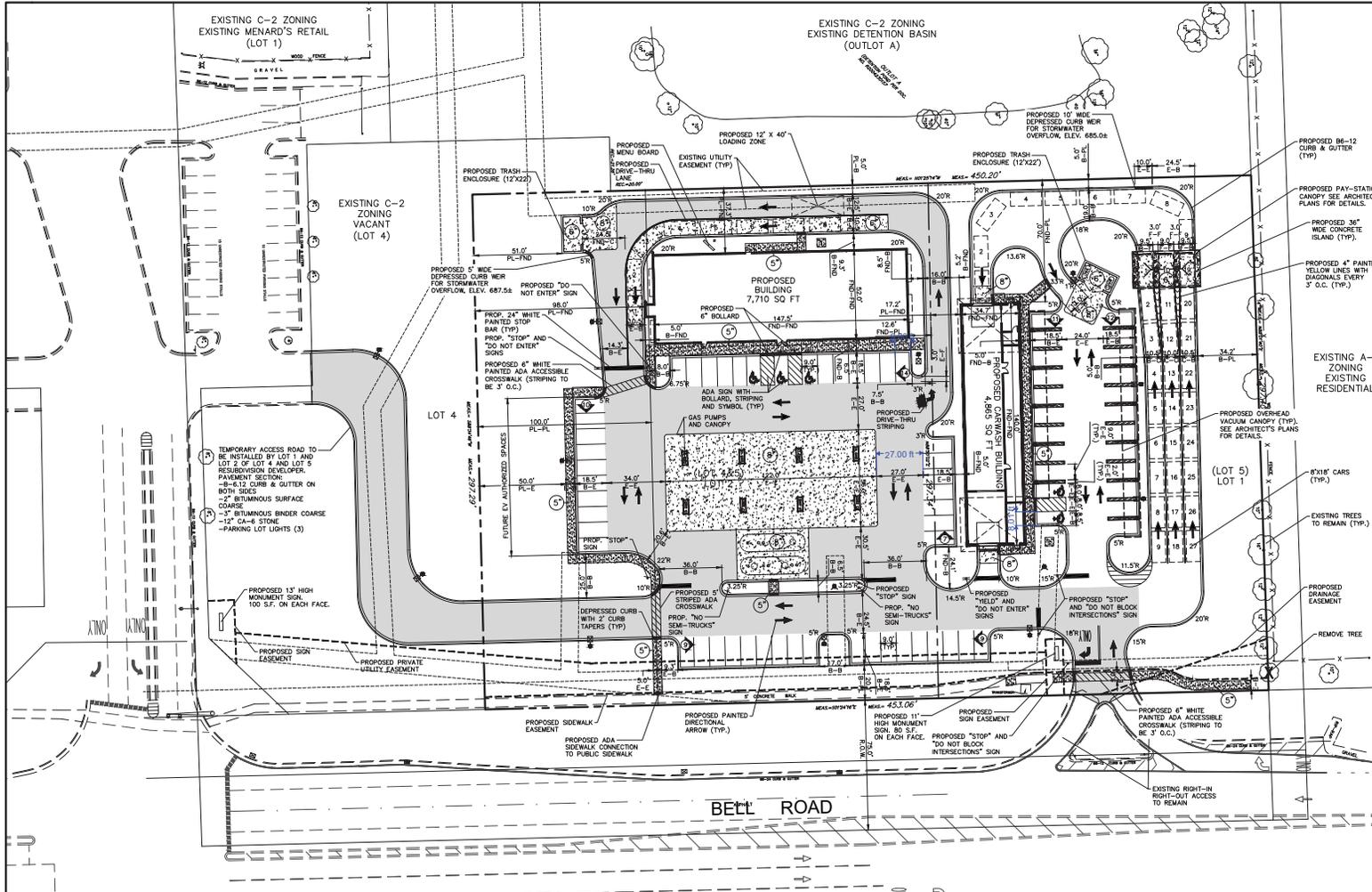
ILLINOIS LICENSE NO. 062-045658
MY LICENSE EXPIRES ON 11-30-2025

UNLESS THIS DOCUMENT BEARS ORIGINAL SIGNATURE AND EMBOSSED SEAL OF THE DESIGN ENGINEER, IT IS NOT A VALID DOCUMENT.
ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 194-002989



COVER SHEET

DATE: 11/22/2024 PROJECT NO.: 21-074	DRAWN BY: JOSH MILLER DATE: JANUARY 10, 2024 SCALE: NONE	CHECKED BY: J. MILLER DESIGN BY: A. NELSON	PROJECT NO.: 21-074
 WATERMARK ENGINEERING RESOURCES, LTD. 2831 Ginger Woods Pkwy Aurora, IL 60502 (800) 374-1900 www.watermark-engineering.com		HOMER GLEN DEVELOPMENT NWC of Bell Road and Glengary Drive - Lot 5 Homer Glen, Illinois	

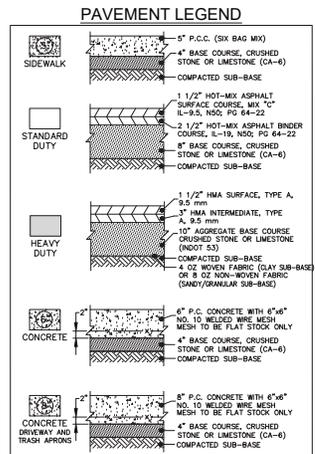


GENERAL NOTES:

1. THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY (SURVEY PROJECT #22-22484 DATED 09/11/2023) PREPARED BY: GENTLE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6262.
2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

GEOMETRIC PLAN NOTES:

1. PROPOSED IMPROVEMENTS ARE PARALLEL AND PERPENDICULAR TO THE EASTERN PROPERTY LINE.
2. ALL RADIUS DIMENSIONS ARE TO BACK OF CURB.
3. SEE ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS.
4. ALL STRIPING TO BE DOUBLE COATED 4" YELLOW PAINT UNLESS OTHERWISE NOTED.
5. WHERE PEDESTRIANS HAVE TO CROSS A TAPERING RAMP OR CURB RAMP THE FACE AND TOP OF CURB ARE TO BE PAINTED USING YELLOW, SLIP RESISTANT PAINT.



NOTES:

1. REFERENCE I.D.O.T. STANDARD SPECIFICATIONS (LATEST EDITION) SECTION 406 FOR BINDER & SURFACE COURSES AND SECTION 351 FOR AGGREGATE BASE COURSE.
2. THE APPLICATION RATES FOR THE PRIME COAT AND TACK COAT ARE TO BE 0.30 AND 0.10 GALLONS PER SQUARE YARD, RESPECTIVELY.
3. SEE PROJECT SPECIFICATIONS FOR SUB-BASE AND BASE COURSE COMPACTION.
4. ALL CONCRETE FLATWORK TO INCLUDE A JOINTING PATTERN SUBMITTAL TO THE CONSTRUCTION MANAGER. CONTRACTOR TO STAY AS CLOSE TO 8"x8" SQUARE PANELS IN LARGE CONCRETE FLATWORK AREAS AS POSSIBLE.
5. FOR SIDEWALKS, PROVIDE TOOLED JOINTS AT 5' O.C., CONTRACTION JOINTS AT 15' O.C., EXPANSION JOINTS AT 45' O.C.
6. PROVIDE AN EXPANSION JOINT ADJACENT TO ALL STRUCTURES. THESE JOINTS SHOULD BE SEALED WITH A TOOL-FINISHED SILICONE SEALANT PER I.D.O.T. STANDARD.

DIMENSION LEGEND

F = FACE	FNC = FENCE
FND = FOUNDATION	R = RADIUS
B = BACK	C = CENTER
E = EDGE	PL = PROPERTY LINE

SITE DATA	LOT 1 DATA	LOT 2 DATA
TOTAL PROPERTY AREA = 134,282 S.F. (3.08 AC.)	LOT 1 AREA = 56,156 S.F. (1.29 AC.)	LOT 2 AREA = 78,126 S.F. (1.79 AC.)(INC. ADDITIONAL 100 FT.)
TOTAL IMPERVIOUS AREA = 90,386 S.F. (2.07 AC.) (67.1%)	IMPERVIOUS AREA = 35,863 S.F. (0.82 AC.) (63.9%)	IMPERVIOUS AREA = 54,426 S.F. (1.25 AC.) (69.7%)
PARKING REQUIRED = 39 (SEE P.I.D. DOCUMENTS)	VACUUM SPACES (INCLUDES 1 ADA SPACE)	ADA ACCESSIBLE SPACES
PARKING PROPOSED = 88 (INCLUDES 16 PUMP AND 22 VACUUM SPACES)	1	3
	22	16
	1	43
	26	62
	BUILDING AREA = 4,865 S.F.	BUILDING AREA = 7,710 S.F.
	1 STORY, 30'-11" TALL BUILDING	1 STORY, 22'-1" TALL BUILDING
	F.A.R. = 0.09	F.A.R. = 0.10

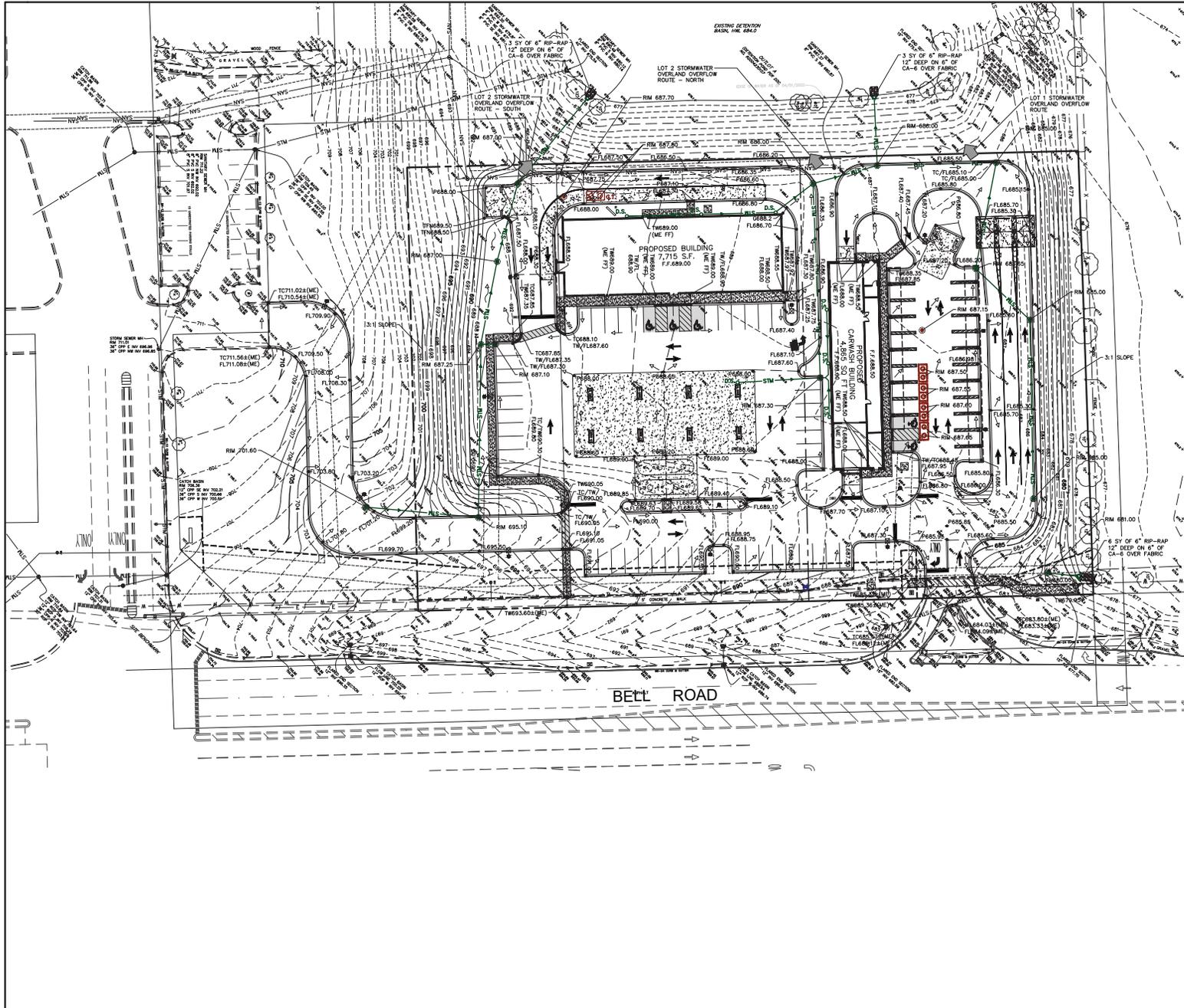


GEOMETRIC PLAN

PREPARED FOR: **HOMER GLEN DEVELOPMENT**
 8060 Lawlode Skokie, IL 60077
 NWC of Bell Road and Glangary Drive - Lot 5
 Homer Glen, Illinois

CHECKED BY: J. MILLER
 DESIGN BY: R. NELSON
 DRAWN BY: JOSH MULLER
 DATE: JANUARY 10, 2024
 SCALE: 1" = 30'
 PROJECT NO.: 21-074

Watsonmark Engineering Resources
 2631 Ginger Woods Pkwy | Aurora, IL 60001 | (800) 374-1800
 watsonmark-engineering.com



GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY (SURVEY PROJECT #22-22484 DATED 09/11/2023) PREPARED BY GENTLE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6262.
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

REFERENCE BENCHMARK: NGS PID DN4689
 THE STATION IS LOCATED ABOUT 7.2 MILES (11.4 KILOMETERS) NORTH OF MOKENA, 4.9 MILES (7.9 KILOMETERS) WEST-SOUTHWEST OF PALOS PARK AND 4.2 MILES (6.8 KILOMETERS) SOUTH-SOUTHWEST OF SAG BRIDGE. THE STATION IS IN THE NORTHEAST QUADRANT OF WILL/COOK ROAD AND WEST 143RD STREET. IT IS 45 FEET (13.7 METERS) EAST OF WILL/COOK ROAD, 51 FEET (15.5 METERS) NORTH-WEST OF WEST 143RD STREET, 1 FOOT (0.3 METERS) EAST OF BACK OF SIDEWALK AND 1 FOOT (0.3 METERS) SOUTHWEST OF AN ORANGE CARBONITE MARKER. NOTE-ACCESS TO THE DATUM POINT IS THROUGH A 6-INCH (15 CENTIMETERS) LOGO CAP AND THE ROD (DATUM POINT) IS SURROUNDED BY A FLOATING BRONZE DISK TO AID IN IDENTIFICATION.
 ELEVATION = 706.16 (NAVD 88 DATUM)

SITE BENCHMARK:
 SQUARE CUT ON THE EAST SIDE OF TRAFFIC LIGHT LOCATED AT NORTHWESTLY CORNER OF BELL ROAD AND GLENGARY DRIVE.
 ELEVATION = 705.83 (NAVD 88 DATUM)

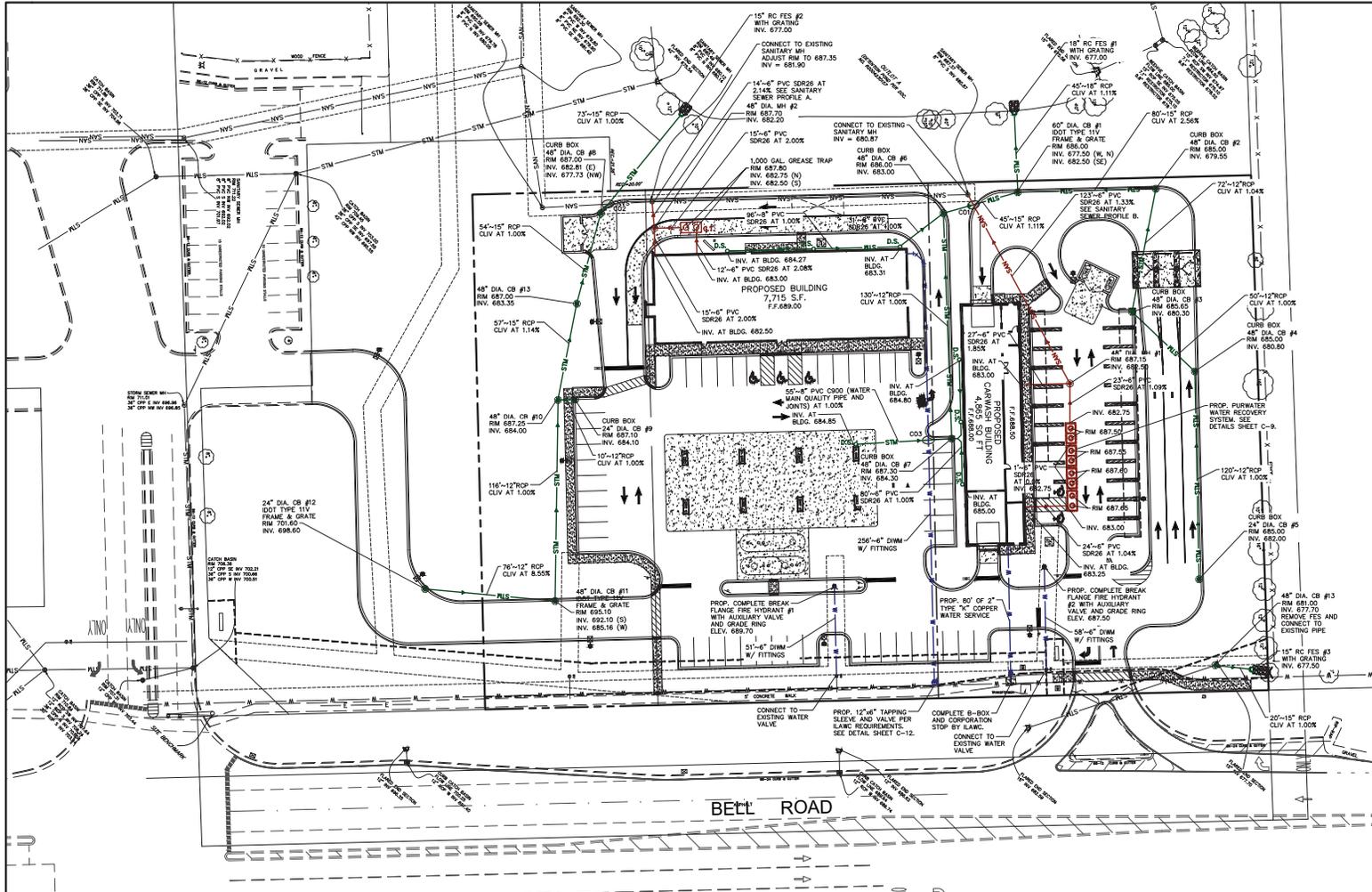
GRADING PLAN NOTES:
 1. UNLESS OTHERWISE SPECIFIED, TOP OF CURB (TC) AND/OR TOP OF WALK ELEVATIONS ARE 0.5' HIGHER THAN THE ADJACENT FLOW LINE (FL) OR PAVEMENT (P) ELEVATIONS.
 2. IN ALL LOCATIONS WHERE ELEVATIONS ARE SHOWN AS ±, THE ELEVATION HAS BEEN DETERMINED BASED ON INTERPOLATED GRADES FROM THE SURVEY. CONTRACTOR IS TO VERIFY THESE GRADES PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS WITHIN THE PROXIMITY OF THESE INTERPOLATED GRADES AND REPORT THEM TO THE DESIGN ENGINEER FOR VERIFICATION OF PROPOSED SLOPES PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. DESIGN ENGINEER IS NOT RESPONSIBLE FOR SLOPES OF PROPOSED IMPROVEMENTS BASED ON THESE ± GRADES WITHOUT CONFIRMATION OF EXISTING ELEVATIONS AT TIME OF CONSTRUCTION.
 3. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUFFERS OR PIPES, AND LANDSCAPING.
 4. ALL RETAINING AND/OR DECORATIVE LANDSCAPE WALLS OR CURBS SHOWN ON THIS PLAN, INCLUDING DETAILS AND SECTIONS, ARE TO ILLUSTRATE GENERAL LOCATION, LENGTH, AND HEIGHT. STRUCTURAL DESIGN, INCLUDING PROPER DRAINAGE, TIE-BACKS, AND SHORING AS WELL AS CONSTRUCTION MEANS ARE NOT ADDRESSED AS PART OF THESE PLANS. A STRUCTURAL ENGINEER SHOULD BE ENGAGED BY THE GENERAL CONTRACTOR AS THEY DEEM NECESSARY. WATERMARK ENGINEERING RESOURCES ASSUMES NO LIABILITY FOR THE DESIGN OR CONSTRUCTION OF ANY STRUCTURAL ELEMENT.
 5. ANY FIELD TILES ENCOUNTERED DURING CONSTRUCTION SHALL BE DIVERTED AROUND THE CONSTRUCTION AREA AND RECONNECTED. THE FIELD TILES SHALL NOT BE ABANDONED, PLUGGED OR CONNECTED TO THE STORM SEWER SYSTEM.

ADA ACCESSIBLE ROUTE.
 SEE SHEET C-4 AND C-41 FOR GRADING AND ADA DETAILS.



GRADING PLAN

DATE: 12/20/24	SCALE: 1" = 30'
PROJECT NO.: 21-074	PROJECT NO.: 21-074
CHECKED BY: J. MILLER	DESIGNED BY: R. NELSON
DRAWN BY: JOSH MULLER	DATE: JANUARY 10, 2024
	
Watermark Engineering Resources	
HOMER GLEN DEVELOPMENT NWC of Bell Road and Glengary Drive - Lot 5 Skokie, IL 60077 Homer Glen, Illinois	
Prepared For:	Prepared By:
C-3	



GENERAL NOTES:

- THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY (SURVEY PROJECT #22-22484 DATED 09/11/2023) PREPARED BY: GENTLE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6262.
- PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

REFERENCE BENCHMARK: NGS PD N4689
 THE STATION IS LOCATED ABOUT 7.2 MILES (11.6 KILOMETERS) NORTH OF MOKENA, 4.9 MILES (7.9 KILOMETERS) WEST-SOUTHWEST OF PALOS PARK AND 4.2 MILES (6.8 KILOMETERS) SOUTH-SOUTHWEST OF SAG BRIDGE. THE STATION IS IN THE NORTHEAST QUADRANT OF WILL/COOK ROAD AND WEST 1430 STREET. IT IS 45 FEET (13.7 METERS) EAST OF WILL/COOK ROAD, 51 FEET (15.5 METERS) NORTH OF WEST 1430 STREET, 1 FOOT (0.3 METERS) EAST OF BACK OF SIDEWALK AND 1 FOOT (0.3 METERS) SOUTHWEST OF AN ORANGE CARBONITE MARKER. NOTE-ACCESS TO THE DATUM POINT IS THROUGH A 6-INCH (15.2 CENTIMETERS) LOGO CAP AND THE ROD (DATUM POINT) IS SURROUNDED BY A FLOATING BRONZE DISK TO AID IN IDENTIFICATION.

ELEVATION = 706.16 (NAVD 88 DATUM)

SITE BENCHMARK:
 SQUARE CORNER ON THE EAST SIDE OF TRAFFIC LIGHT LOCATED AT NORTHWESTERLY CORNER OF BELL ROAD AND GLENGARY DRIVE.
 ELEVATION = 705.83 (NAVD 88 DATUM)

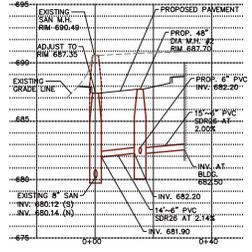
UTILITY PLAN NOTES:

- PRIOR TO CONSTRUCTION OF ANY UTILITIES, CONTRACTOR IS TO VERIFY THAT THE PROPOSED UTILITIES SHOWN ON THIS PLAN THAT ENTER THE PROPOSED BUILDING(S) CORRESPOND WITH THE UTILITIES ON THE PLUMBING PLANS AS THEY EXIT THE BUILDING(S). CONTRACTOR TO REPORT IN WRITING ANY DISCREPANCIES IN SIZE, LOCATION, OR INVERT ELEVATION TO THE DESIGN ENGINEER IMMEDIATELY FOR RESOLUTION OF THE CONFLICT IN WRITING.
- GENERAL CONTRACTOR TO COORDINATE THE INSTALLATION AND PERMITTING OF THE PUBLIC UTILITIES, SUCH AS GAS, ELECTRIC, TELEPHONE, CABLE AND FIBER OPTICS, WITH THE PUBLIC UTILITY COMPANIES AND ARCHITECT PRIOR TO CONSTRUCTION. THE INSTALLATION OF THE PUBLIC UTILITIES AND NECESSARY SLEEVING TO BE INCLUDED AS PART OF GENERAL CONTRACTOR'S SCOPE OF WORK FOR THIS PROJECT.
- ANY FIELD TILES ENCOUNTERED DURING CONSTRUCTION SHALL BE DIVERTED AROUND THE CONSTRUCTION AREA AND RECONNECTED. THE FIELD TILE(S) SHALL NOT BE ABANDONED, PLUGGED OR CONNECTED TO THE STORM SEWER SYSTEM.

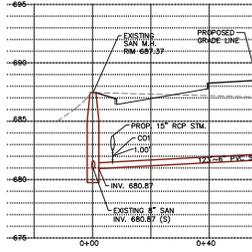
UTILITY CROSSINGS

NO.	DESCRIPTION	ELEVATION	REMARKS
001	BOTTOM OF 15" STM	662.63	> 1.00' CLEARANCE
	TOP OF 6" SAN	661.63	
002	BOTTOM OF 8" SAN	679.92	> 0.84' CLEARANCE
	TOP OF 15" STM	679.08	
003	BOTTOM OF 6" STM	664.27	> 2.17' CLEARANCE
	TOP OF 6" WATERLINE	662.10	

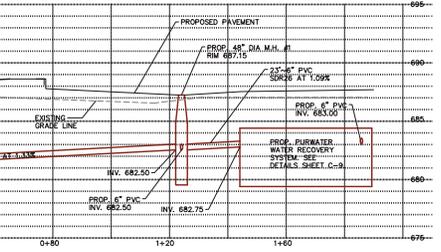
**SANITARY SEWER PROFILE A
 LOT 2 - GAS STATION**
 SCALE: 1"=20' HORIZ. / 1"= 5' VERT.



**SANITARY SEWER PROFILE B
 LOT 1 - CAR WASH**
 SCALE: 1"=20' HORIZ. / 1"= 5' VERT.



**SANITARY SEWER PROFILE C
 LOT 1 - CAR WASH**
 SCALE: 1"=20' HORIZ. / 1"= 5' VERT.



UTILITY PLAN

DATE: 12/20/24
 PROJECT NO.: 21-074
 PREPARED FOR: HOMER GLEN DEVELOPMENT
 8060 Lawndale
 Skokie, IL 60077
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

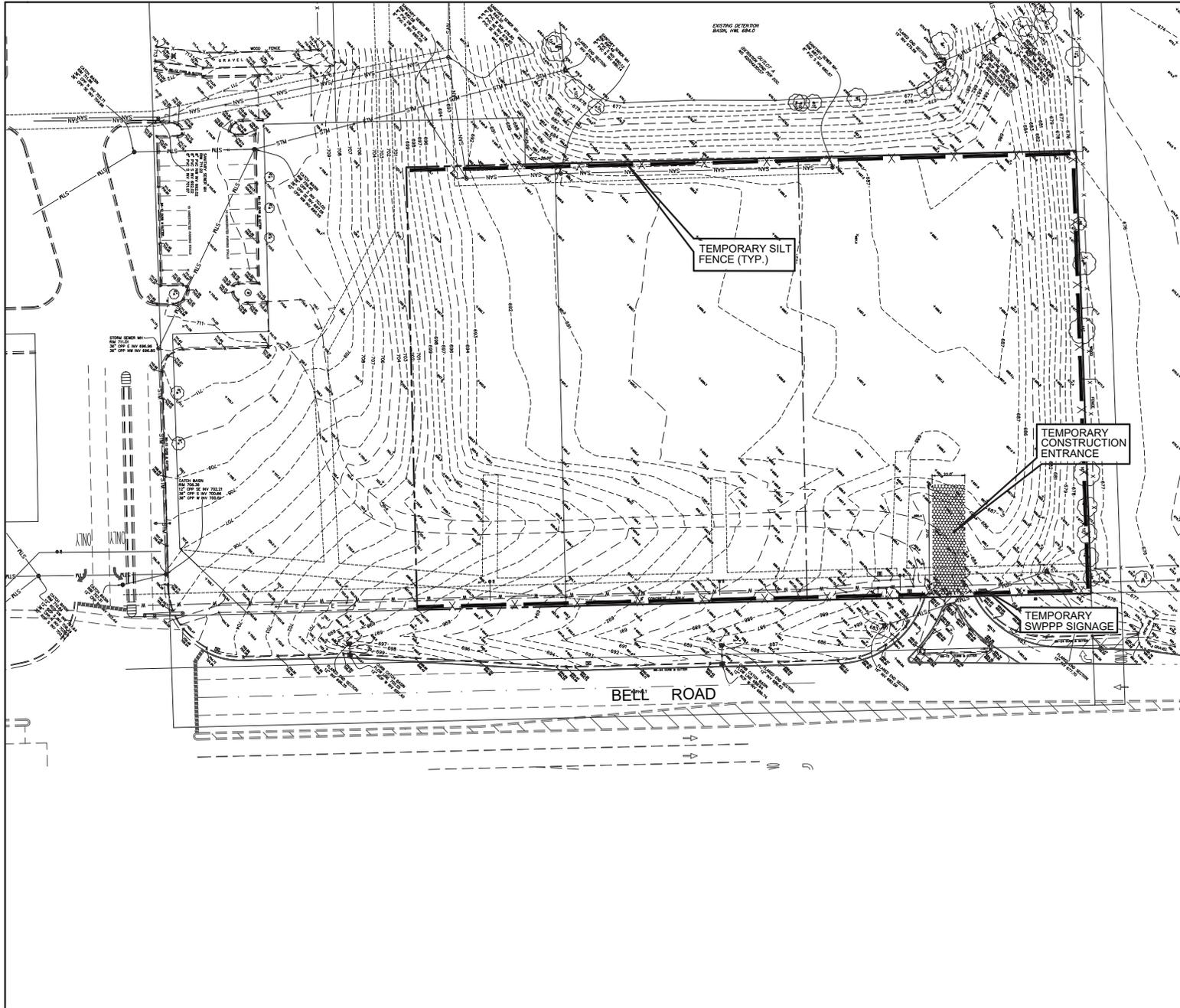
DESIGNED BY: J. MILLER
 CHECKED BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: JANUARY 10, 2024
 SCALE: 1"= 30'

watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60002 | (800) 374-1600

Watermark engineering resources

C-5

UTILITY PLAN



GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY (SURVEY PROJECT #22-22484 DATED 09/11/2023) PREPARED BY: GENTLE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6262.
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

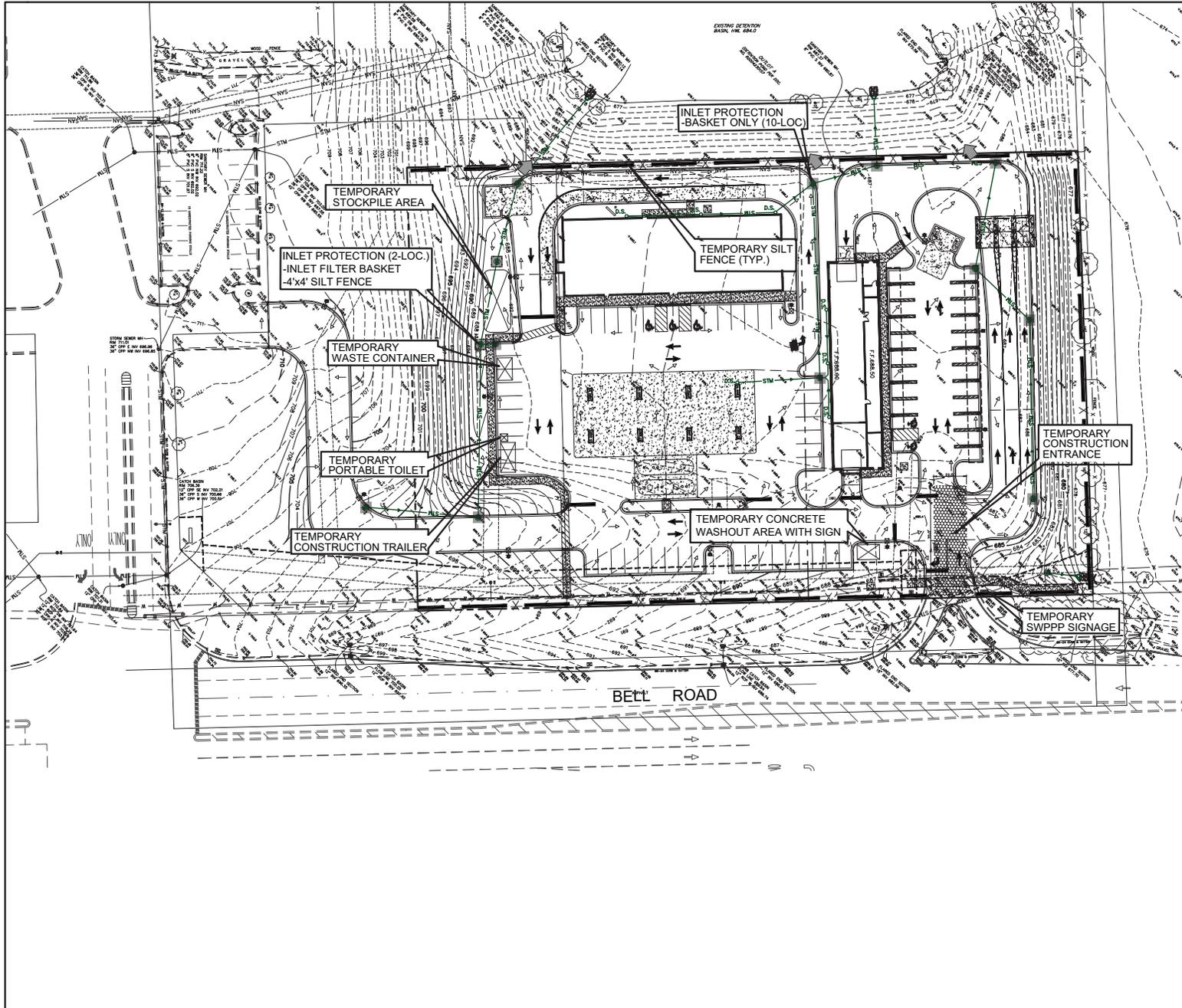
SEE EROSION CONTROL NOTES AND DETAILS ON SHEET C-8

LEGEND	
	SILT FENCE
	INLET FILTER BASKET
	INLET PROTECTION



PHASE I SOIL EROSION CONTROL PLAN
 C-6

DATE: 11/20/24	PROJECT NO.: 21-074
DESIGNED BY: J. MILLER	CHECKED BY: J. MILLER
DRAWN BY: JOSH MULLER	DESIGN BY: R. NELSON
DATE: JANUARY 10, 2024	SCALE: 1" = 30'
<p>WatOrmark Engineering Resources watermark-engineering.com 2631 Ginger Woods Pkwy Aurora, IL 60002 (800) 374-1600</p>	
<p>Prepared For: NSS GROUP, INC. 8080 Lawndale Skokie, IL 60077 HOMER GLEN DEVELOPMENT NWC of Bell Road and Glangary Drive - Lot 5 Homer Glen, Illinois</p>	



GENERAL NOTES:
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SEE EROSION CONTROL NOTES AND DETAILS ON SHEET C-8

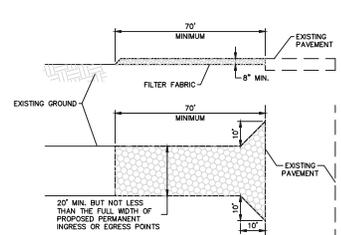
LEGEND

	SILT FENCE
	INLET FILTER BASKET
	INLET PROTECTION



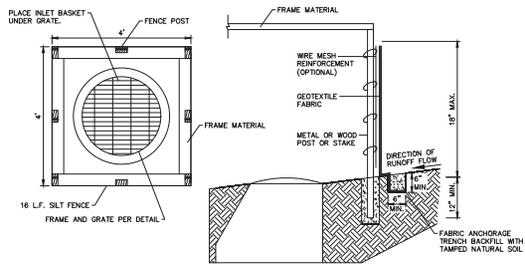
PHASE II SOIL EROSION CONTROL PLAN
 C-7

DATE: 12/20/24	PROJECT: HOMER GLEN DEVELOPMENT
DRAWN BY: JOSH MULLER	DATE: JANUARY 10, 2024
CHECKED BY: J. MILLER	SCALE: 1" = 30'
DESIGN BY: R. NELSON	PROJECT NO.: 21-074
W&S Engineering Resources <small>watermark-engineering.com 2831 Ginger Woods Pkwy Aurora, IL 60502 (800) 374-1800</small>	
Prepared For: NSS GROUP, INC. 8060 Lawndale Skokie, IL 60077 HOMER GLEN DEVELOPMENT NWC of Bell Road and Glangary Drive - Lot 5 Homer Glen, Illinois	



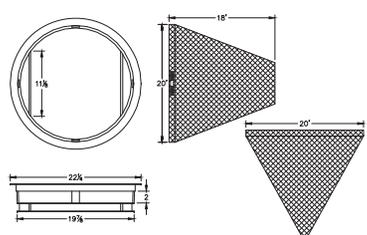
- NOTES:
- STONE SIZE - IDOT COARSE AGGREGATE GRADATIONS: CA-1, CA-2, CA-3 OR CA-4
 - THICKNESS - NOT LESS THAN EIGHT (8) INCHES.
 - FILTER FABRIC SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL CLASS I, II OR IV IN THE ILLINOIS URBAN MANUAL.
 - STONE PLACEMENT - THE STONES IN THE ENTRANCE SHALL BE PLACED ACCORDING TO ILLINOIS URBAN MANUAL CONSTRUCTION SPECIFICATION 25 (ROCKFILL). PLACEMENT WILL BE BY METHOD 1 AND COMPACTION WILL BE CLASS III.
 - MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED IMMEDIATELY.
 - WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING STORM DRAINS, DITCHES, WATERCOURSES, OR SURFACE WATERS INCLUDING WETLANDS.
 - PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

STABILIZED CONSTRUCTION ENTRANCE



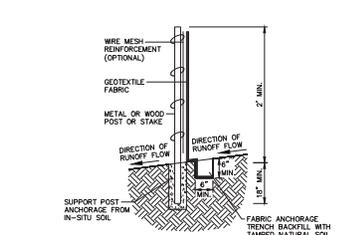
- NOTES:
- FENCE POSTS SHALL BE EITHER STANDARD STEEL POST OR WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.
 - STEEL POSTS SHALL BE STANDARD T AND U SECTIONS WEIGHING NOT LESS THAN 1.33 POUNDS PER LINEAR FOOT OR OTHER STEEL POSTS HAVING EQUIVALENT STRENGTH AND BENDING RESISTANCE.
 - GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFICATION 592 (GEOTEXTILE) TABLE 1 OR 2, CLASS I IN THE ILLINOIS URBAN MANUAL, CURRENT EDITION. THE FABRIC SHALL HAVE AN AOS OF AT LEAST 30 FOR NONWOVEN AND 50 FOR WOVEN MATERIAL.
 - STAKES ARE TO BE PLACED A MAXIMUM OF 3 FEET APART.
 - JOINTS IN GEOTEXTILE FABRIC ARE TO BE MADE AT STAKES.

INLET PROTECTION



- NOTES:
- TOP FLANGE FABRICATED FROM 1 1/4" x 1 1/4" x 1/8" ANGLE BASE RIM FABRICATED FROM 1 1/2" x 1/2" x 1/8" CHANNEL. HANDLES AND SUSPENSION BRACKETS FABRICATED FROM 1 1/4" x 1/4" FLAT STOCK
 - ALL DOMESTIC STEEL IS TO CONFORM TO ASTM-A36.
 - SEDIMENT BAG IS TO BE FABRICATED FROM 4 OZ./SQ.YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH WITH A MINIMUM FLOW RATE OF 145 GAL/MIN/SQ. FT. BAG IS TO BE DESIGNED FOR A MINIMUM SILT AND DEBRIS CAPACITY OF 2 CU. FT. BAG IS TO BE SECURED TO BASE RIM WITH A STAINLESS STEEL STRAP AND LOCK.

INLET BASKET FILTER FOR TYPE 1 INLETS



- NOTES:
- TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
 - FENCE POSTS SHALL BE EITHER STANDARD STEEL POST OR WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.
 - STEEL POSTS SHALL BE STANDARD T AND U SECTIONS WEIGHING NOT LESS THAN 1.33 POUNDS PER LINEAR FOOT OR OTHER STEEL POSTS HAVING EQUIVALENT STRENGTH AND BENDING RESISTANCE.
 - WIRE FENCE SHALL BE A MINIMUM 12 GAGE WIRE WITH A 6 INCH MAXIMUM OPENING.
 - GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFICATION 592 (GEOTEXTILE) TABLE 1 OR 2, CLASS I IN THE FABRIC SHALL HAVE AN AOS OF AT LEAST 30 FOR NONWOVEN AND 50 FOR WOVEN MATERIAL.
- MAINTENANCE NOTES:
- FILTER BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL GREATER THAN 1/2" AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
 - SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL BE NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
 - SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN THE DEPOSITS REACH APPROXIMATELY HALF THE HEIGHT OF THE BARRIER.
 - ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEEDED.
 - AT A MINIMUM, SILT FENCE AND OTHER EROSION CONTROL MEASURES SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PHASE I AND PHASE II SOIL EROSION CONTROL PLANS. THEY SHALL ALSO BE INSTALLED ANYWHERE THAT THEY ARE NEEDED DURING CONSTRUCTION IN ORDER TO PREVENT EROSION AND SEDIMENT FROM BEING CARRIED DOWN STREAM. THIS IS THE GENERAL CONTRACTOR'S RESPONSIBILITY AND SHALL BE INSTALLED, RELOCATED, MAINTAINED, ETC. AS DIRECTED BY THE APPOINTED SWPPP INSPECTOR. EROSION CONTROL INSTALLATION AND MAINTENANCE IS TO BE A PART OF THE CONTRACT AND IS NOT AN EXTRA TO THE OWNER.

EROSION CONTROL

- CONTRACTOR IS TO FOLLOW THE REQUIREMENTS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" CURRENT EDITION AND THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES).
- SOIL EROSION CONTROL SYSTEMS SHALL BE CONSTRUCTED AS SHOWN ON THE SOIL EROSION CONTROL PLANS AND/OR AS SPECIFIED BY THE DESIGN ENGINEER, VILLAGE ENGINEER, APPOINTED SWPPP INSPECTOR, OR MUNICIPAL INSPECTOR.
- PERIMETER EROSION BARRIER SHALL BE PLACED IN A MANNER THAT WILL INTERCEPT WATER BORNE SILT AND PREVENT IT FROM LEAVING THE AREA OF CONSTRUCTION. ALL SILT FENCES SHALL BE PLACED AS CLOSE TO THE CONTOUR AS POSSIBLE WITH THE ENDS EXTENDING UPSLOPE. THE MAXIMUM SPACING OF POSTS SHALL BE 5 FEET. WHEN WIRE OR OTHER FORM OF APPROVED BACKING IS USED THE MAXIMUM SPACING MAY BE INCREASED TO 8 FEET. SPACING MAY NEED TO BE ADJUSTED SO THAT POSTS ARE LOCATED IN LOW AREAS WHERE WATER MAY POND. THE FILTER FABRIC AND WIRE SUPPORT, IF USED, MUST BE SECURELY FASTENED TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY DUTY WIRE STAPLES AT LEAST ONE INCH LONG OR THE WIRES (10 GAGE MINIMUM). THE FABRIC SHALL NOT BE STAPLED OR WIRED TO THE WIRE SUPPORT OR TO EXISTING TREES. GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFICATION 592 (GEOTEXTILE) TABLE 1 OR 2, CLASS I IN THE ILLINOIS URBAN MANUAL, CURRENT EDITION. THE FABRIC SHALL HAVE AN AOS OF AT LEAST 30 FOR NONWOVEN AND 50 FOR WOVEN MATERIAL.
- INLET FILTERS SHALL BE CONSTRUCTED OF A REPLACEABLE REINFORCED FILTER BAG SUSPENDED FROM A RETAINER RING OR FRAME. INLET FILTER SYSTEMS SHALL BE THE CATCH-ALL WITH OVERFLOW. CARE SHOULD BE TAKEN WHEN MAINTAINING OR REMOVING THIS FILTER FABRIC BAG TO NOT ALLOW THE PREVIOUSLY TRAPPED DEBRIS TO ENTER THE STORM SEWER SYSTEM.
- THE BED FOR RIP RAP SHALL BE TRIMMED AND SHAPED TO ALLOW THE FINISHED SURFACE TO CONFORM TO THE LINES SPECIFIED. AT THE TOP OF THE SLOPE, THE RIP RAP SHALL COMMENCE ON A CONTINUATION OF THE SLOPE AFTER EXCAVATION TO ACCOMMODATE THE FULL DEPTH OF FABRIC, BEDDING LAYER, AND RIP RAP SPECIFIED.
- FILTER FABRIC IS REQUIRED UNDER STONE RIP RAP GRADATION 4, 5, 6 AND 7 FOR ALL USES, AND UNDER CONCRETE BLOCK, BROKEN CONCRETE, AND STONE OR BROKEN CONCRETE DUMPED RIP RAP WHEN USED FOR SOIL EROSION PROTECTION.
- STREETS ARE TO BE CLEARED OF DEBRIS, AND SWEEPED CLEAN OF SILT AND MUD DAILY.
- SOIL EROSION CONTROL MEASURES ARE TO BE CHECKED BY QUALIFIED PERSONNEL AT LEAST ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.5 INCHES OR GREATER OR EQUIVALENT SNOWFALL AND REPAIRED IF NECESSARY.
- ALL EROSION CONTROL PROTECTION SHALL BE KEPT IN PLACE UNTIL THE GROUND HAS BEEN STABILIZED AND THE PAVEMENT HAS BEEN INSTALLED.
- ANY DISTURBED AREAS ON WHICH CONSTRUCTION ACTIVITIES HAVE STOPPED (PERMANENTLY OR TEMPORARILY) FOR 7 DAYS, MUST BE STABILIZED IN ACCORDANCE WITH NPDES REQUIREMENTS.
- BUILT UP SEDIMENT SHALL BE REMOVED FROM THE SILT FENCE WHEN IT HAS REACHED ONE THIRD THE HEIGHT OF THE FENCE.
- SILT FENCES SHALL BE INSPECTED FOR DEPTH OF SEDIMENT, TEARS, ETC., TO SEE IF FABRIC IS SECURELY ATTACHED TO THE FENCE POSTS, AND THAT THE FENCE POSTS ARE SECURELY IN THE GROUND.
- THE SEDIMENT BASIN, IF PRESENT, SHALL BE INSPECTED FOR DEPTH OF SEDIMENT AT LEAST ONCE A WEEK. BUILT UP SEDIMENT SHALL BE REMOVED WHEN IT REACHES 25 PERCENT OF THE DESIGN CAPACITY.
- CONTRACTOR TO COMPLY WITH FINAL STABILIZATION AND TERMINATION REQUIREMENTS OF THE SWPPP.
- AT A MINIMUM, SILT FENCE AND OTHER EROSION CONTROL MEASURES SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PHASE I AND PHASE II SOIL EROSION CONTROL PLANS. THEY SHALL ALSO BE INSTALLED ANYWHERE THAT THEY ARE NEEDED DURING CONSTRUCTION IN ORDER TO PREVENT EROSION AND SEDIMENT FROM BEING CARRIED DOWN STREAM. THIS IS THE GENERAL CONTRACTOR'S RESPONSIBILITY AND SHALL BE INSTALLED, RELOCATED, MAINTAINED, ETC. AS DIRECTED BY THE APPOINTED SWPPP INSPECTOR. EROSION CONTROL INSTALLATION AND MAINTENANCE IS TO BE A PART OF THE CONTRACT AND IS NOT AN EXTRA TO THE OWNER.

SOIL PROTECTION CHART

	JAN	FEB	MAR	APR	MAY	JUNE	JULY	AUG	SEP	OCT	NOV	DEC
PERMANENT SEEDING												
SODDING												
TEMPORARY SEEDING												
MULCHING												

- NOTES:
- PERMANENT VEGETATION SHALL BE PLANTED ACCORDING TO THE APPROVED LANDSCAPE PLAN AND SHALL FOLLOW ILLINOIS URBAN MANUAL PRACTICE STANDARD 880 FOR PERMANENT SEEDING AND 925 FOR SODDING AT A MINIMUM.
 - TEMPORARY SEEDING SHALL BE APPLIED ACCORDING TO THE ILLINOIS URBAN MANUAL PRACTICE STANDARD 965. THIS PRACTICE APPLIES TO ALL CLEARED, UNVEGETATED, OR SPARSELY VEGETATED SOIL SURFACES WHERE VEGETATIVE COVER IS NEEDED FOR LESS THAN 1 YEAR.
 - WHERE THE PH OF THE SOIL IS BELOW 5.5, APPLY ONE AND ONE HALF TO TWO TONS PER ACRE OF FINELY GROUND AGRICULTURAL LIMESTONE. IF THE SEEDING PERIOD IS LESS THAN 30 DAYS, LIMING WILL NOT BE REQUIRED.
 - APPLY 500 POUNDS PER ACRE OF 10-10-10 FERTILIZER OR EQUIVALENT. INCORPORATE LIME AND FERTILIZER INTO THE TOP 2-4 INCHES OF SOIL. IF THE SEEDING PERIOD IS LESS THAN 30 DAYS, FERTILIZER WILL NOT BE REQUIRED.
 - PREPARE A TOPSOIL SEEDBED OF LOOSE SOIL TO A DEPTH OF 3 TO 4 INCHES. IF RECENT TILLAGE OR GRADING OPERATIONS HAVE RESULTED IN A LOOSE SURFACE, ADDITIONAL TILLAGE OR ROUGHENING MAY NOT BE REQUIRED EXCEPT TO BREAK UP LARGE CLODS. IF RAINFALL CAUSED THE SURFACE TO BECOME SEALED OR CRUSTED, LOOSEN IT JUST PRIOR TO SEEDING BY DISKING, RAKING, HARROWING, OR OTHER SUITABLE METHODS. GROVE OR FURROW SLOPES STEEPER THAN 3:1 ON THE CONTOUR BEFORE SEEDING.
 - SEED SHALL BE EVENLY APPLIED WITH A CYCLONE SEEDER, DRILL, CULTIPACKER SEEDER OR HYDROSEEDER. SMALL GRAINS SHALL BE PLANTED NO MORE THAN ONE INCH DEEP. GRASSES SHALL BE PLANTED NO MORE THAN ONE HALF INCH DEEP.
 - COVER BROADCAST SEEDINGS BY CULTIPACKING, DRAGGING A HARROW, OR RAKING.
 - OATS SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO JULY 1.
 - CEREA RYE SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - WHEAT SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - PERENNIAL RYE GRASS SHALL BE APPLIED AT 25 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - TEMPORARY MULCHES ARE TO BE APPLIED TO:
 - AREAS THAT HAVE BEEN SEEDING TO PROVIDE A TEMPORARY OR PERMANENT SEEDING; C AREAS THAT CANNOT BE SEEDING BECAUSE OF THE SEASON OF THE YEAR AND NEED FOR SOIL SURFACE PROTECTION;
 - FOR MUD AND DUST CONTROL;
 - WHERE PROTECTION THROUGHOUT CONSTRUCTION OR SEEDING CANNOT BE DONE, AND SHALL BE CONSTRUCTED ACCORDING TO THE ILLINOIS URBAN MANUAL PRACTICE STANDARD 875.

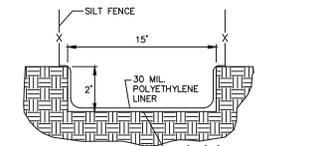
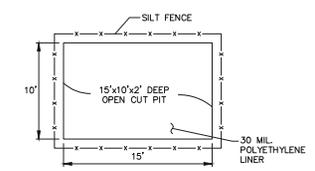
PHASE I CONSTRUCTION SEQUENCE:

- INSTALL STABILIZED CONSTRUCTION ENTRANCE.
- PREPARE TEMPORARY PARKING AND STORAGE AREA.
- CONSTRUCT THE SILT FENCES ON THE SITE.
- INSTALL INLET PROTECTION AROUND ALL EXISTING STORM SEWER STRUCTURES.
- CONSTRUCT THE SEDIMENTATION BASINS.
- CONSTRUCT DIVERSION DITCHES AND AGGREGATE DITCH CHECKS TO DIRECT WATER TO THE SEDIMENTATION BASINS.
- HOLD PRE-CONSTRUCTION MEETING TO DISCUSS THE STORM WATER POLLUTION PLAN WITH ENGINEER, ALL CONTRACTORS AND JURISDICTIONAL INSPECTION AGENCIES.
- CLEAR AND GRUB THE SITE.
- START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- BEGIN MASS GRADING OPERATIONS FOR THE SITE.

PHASE II CONSTRUCTION SEQUENCE:

- TEMPORARILY SEED EXPOSED AREAS.
- INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
- INSTALL RIP RAP AROUND OUTLET STRUCTURES.
- INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
- PREPARE SITE FOR PAVING.
- PAVE SITE.
- INSTALL INLET PROTECTION DEVICES.
- COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
- REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED).

SILT FENCE DETAIL



- NOTES:
- OPTIONAL USE OF A PORTABLE CONCRETE WASHOUT CONTAINER IS ACCEPTABLE WITH 30 MIL POLYETHYLENE LINER.
 - CONCRETE WASHOUT SHOULD BE CONTAINED AT ALL TIMES. WASHOUT MATERIAL SHOULD NOT BE ALLOWED TO ENTER THE STORM SEWER SYSTEM. WASHOUT MATERIAL SHOULD NOT BE STORED UNDER ANY CIRCUMSTANCES. ANY WASTE SHOULD BE DISPOSED OF PROPERLY AND THE LOCATION OF THE WASHOUT SHOULD BE DESIGNATED WITH PROPER SIGNAGE. FAILURE TO COMPLY COULD RESULT IN AN INCIDENT OF NONCOMPLIANCE (ION).

CONCRETE WASHOUT AREA DETAIL

SOIL EROSION CONTROL DETAILS AND SPECS

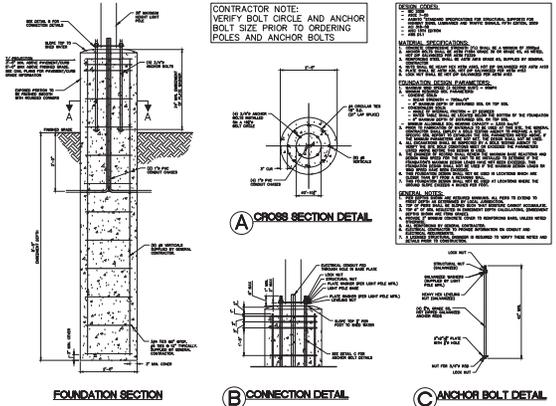
DATE: 12/20/24	SCALE: NONE
PROJECT NO.: 21-074	

Prepared For:
 NCS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
 HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glangary Drive - Lot 5
 Homer Glen, Illinois

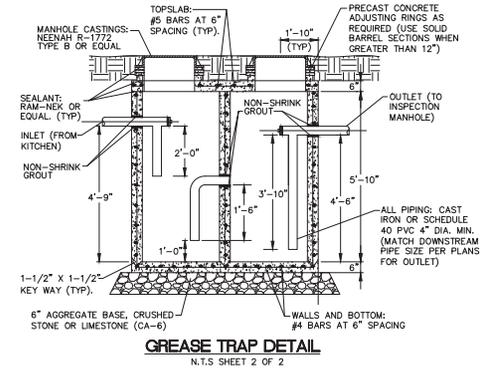
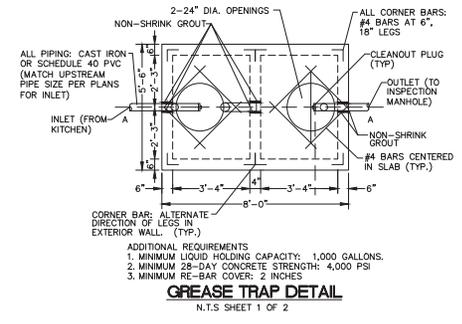
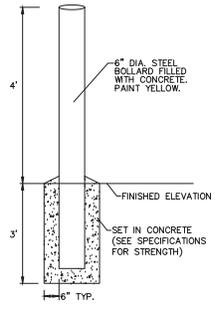
Prepared By:
 NatOrmark
 engineering
 resources
 www.natormarkengineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60002 | 800.375.1600

CHECKED BY: J. MILLER	DATE: JANUARY 10, 2024
DESIGNED BY: R. NELSON	SCALE: NONE
DRAWN BY: JOSH MILLER	PROJECT NO.: 21-074

C-8



PARKING LOT LIGHT POLE DETAIL



PROJECT DETAILS #2

DATE: 1/22/2024	REVISIONS:	NO.	DESCRIPTION:
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Prepared For:

NSS GROUP, INC.
8060 Lawndale
Skokie, IL 60077

HOMER GLEN DEVELOPMENT
NWC of Bell Road and Glangary Drive - Lot 5
Homer Glen, Illinois

Prepared By:

WatOrmark Engineering Resources
watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60002 | (800) 374-1800

CHECKED BY: J. MILLER
DESIGN BY: R. NELSON
DRAWN BY: JOSH MILLER
DATE: JANUARY 10, 2024
SCALE: NONE
PROJECT NO.: 21-074

C-10

PROJECT DETAILS #2

SANITARY SEWER SPECIFICATIONS - ILLINOIS AMERICAN WATER

MATERIAL SPECIFICATIONS FOR REINFORCE COLLECTION

- Pipe Material for Sewers shall be as specified on the plans.
 - Polymer Chloride (PVC) sewer pipe shall conform to ASTM D-3034, SDR35 and shall be manufactured and installed in accordance with the Illinois-American Water Association's. Unions otherwise called for all pipe through 18" shall meet the specification. Joints shall be rubber gasketed in accordance with ASTM D-3212.
 - Class 62 ductile iron pipe (AWWA C151), with AWWA C-111 joints shall be used where the minimum penetration cannot be maintained between water and sewer lines, either casing pipe installations, or for other special conditions as directed by Illinois-American.

- Manhole Material

Sanitary sewer manholes shall be of precast concrete and construction (ASTM C-407) with an effluent cover and joints sealed with butyl based material. Concrete adjustment rings shall be used where adjustment is required. Adjustment rings shall not exceed 1/2" vertically. All joints shall be sealed with Rubberoid, or approved equal butyl based material. Cement grouting of the seams and joints should not be completed.

Butyl material shall have a minimum width of 2" when applied, in two pieces. Manholes shall be 48" inside diameter on sewers 24" and smaller, and shall be 60" inside diameter for sewers 27" through 48".

- Sanitary sewer manholes shall have ground-in-place or precast manholes. The manhole shall conform to the sewer pipe with smooth, non-corrosive joints and manholes. A flexible gasket between the pipe and manhole with meeting ASTM D-633, cast integrally into the manhole wall, shall be provided for each pipe connection to the manhole. Unions shall be the approved Lock Joint Flexible Manhole Series, the A-100 Manhole from Contractor or approved equal. Such unions shall be selected and installed in accordance with the manufacturer's specifications for the specific type of pipe used. Sanitary manhole castings shall be finished in 1772 or approved equal and shall be covered with zinc. Lock shall be finished in 1772 or approved equal. Manholes shall be finished in 1772 or approved equal. Manholes located in areas subject to flooding or installed in areas which shall be finished with waterproof frames and lid, National Foundry F-1100 complete stainless steel anchor bolts or approved equal.

- Drop Manhole

Drop manhole pipe shall be provided for a sewer entering a manhole at an elevation of 24" or more above the manhole invert. The entire drop assembly shall be cast in concrete monolithically with the manhole base material. The diameter of the drop pipe shall be smaller than the diameter of the entering sewer by 2" diameters, provided that the diameter of the drop pipe shall not be less than 8" diameter.

- Service Level Material

Service shafts shall be a minimum of 6" diameter. Pipe shall be PVC SDR35, Resin #124548, or ductile iron. Where ductile iron pipe is called for on the plans, the wall shall be Class 52 ductile iron push-on joints (AWWA C-111).

All pipes for shafts that include and stoppers for all solids capable of withstanding a standard test pressure are not to be run above without stoppers. Stoppers may be an integral one-piece construction.

Joining of dissimilar pipes will be with use of a "Band Seal" using a flexible rubber sleeve tightly fitted to each pipe with a stainless steel ring.

INSTALLATION SPECIFICATIONS FOR REINFORCE COLLECTION

- Protection of Water Main Lines from Sanitary Sewers and Storm Sewers

Water mains shall be protected from horizontal and vertical encroachment in accordance with the requirements of the "Technical Policy Statement", or the requirements of MURCOG, whichever applies.

Water mains shall be protected from horizontal and vertical encroachment in accordance with the requirements of the "Technical Policy Statement", or the requirements of MURCOG, whichever applies.
- Depth of Pipe Cover

NOTE: THE MINIMUM SANITARY SEWER DEPTH IS THE SAME AS THE MINIMUM SANITARY SEWER DEPTH FOR REINFORCE COLLECTION STANDARDS BY SD-107-07.

A minimum depth of 2' shall be maintained for sanitary sewer pipes. The 2' depth shall be from proposed ground elevation to the crown of the pipe.
- Pipe Installation

PVC plastic pipe shall be installed in accordance with the requirements of ASTM Standard D-2321. Only Class 1 embedment materials shall be allowed.
- Unsanitary Soil

If unsanitary soil conditions are encountered under sanitary sewer manholes which require removal of unsanitary material below the depth of the standard bedding, the material removed shall be replaced with granular material of the gradation approved by Illinois-American.
- Service Lines

All sanitary sewer service lines must be installed by Illinois-American Water Company prior to back filling. Illinois-American must be provided with a minimum of 48 hours advance notice (800-777-8555) to arrange for inspection. Notice is not considered valid until acknowledged by the Illinois-American representative. Sanitary sewer service lines shall be a minimum of 6" diameter unless otherwise noted on the plans.

The top of pipe fittings shall be provided on the sanitary sewer main for all sanitary sewer service connections, except those which are made directly to a manhole. Hyres shall be set so that material discharge is compatible with the direction of flow in the main.

- Service connections into manholes shall be made with uniform straight flow channels at the crown elevation of the sewer main, using a flexible sleeve (ASTM C-823), or approved equal. The service connections shall enter the manhole from the main sewer main but shall have a minimum of 24" clearance. The cover shall rest on the crown of the main sewer when the main is larger than 24" inch.
- Risers shall be provided on service lines when the sewer main is greater than 12" below the proposed ground elevation and shall extend from the sewer main to a point 8" below the proposed ground elevation. All sanitary sewer service lines must have a clean-out installed at the property line or right-of-way, to be installed per Clean-out detail.

- Connecting to an Existing Man

When it is necessary to connect a new sewer service to an existing sewer main, by means other than an existing man, tee or an existing manhole, one of the following methods shall be used:

 - Save the sewer main by means of a Sewer Tee or similar device, and properly install a hub-vane manhole or hub-to-hub tees, using a two-part epoxy glue compound.
 - With pipe cutter, neatly and accurately cut out desired length of pipe for insertion of proper size fitting using "Band Seal" or similar couplings to hold it in place.
 - When a manhole is required to be installed on an existing man, a section of the man to be removed and the new manhole to be "spliced" into the line using manhole couplings. The manhole to be installed with flexible material, except in accordance with a "tagman" style manhole is not allowed.

- Conversion to an Existing Manhole

A direct connection to an existing manhole is to be made by capping the manhole and connecting the new man to the manhole by use of a flexible sleeve (ASTM C-823), or approved equal. The service connections shall enter the manhole from the main sewer main but shall have a minimum of 24" clearance. The cover shall rest on the crown of the main sewer when the main is larger than 24" inch.

- Cleaning

In preparation for final inspection, all sanitary sewers shall be and not be cleaned. Jet cleaning may also be required prior to testing.

- Unsanitary Conditions

Connections to the sanitary sewer system with the following items shall not be permitted: down-spirals or hot plastic; sump pumps; laundry drains or sump pumps; window wells or area drains; downspouts; etc. No storm water or groundwater shall be allowed to enter the sanitary sewer system.

TESTING

- Testing of Sanitary Sewers

Testing for sanitary sewers for leakage will be witnessed by an Illinois-American representative or approved site inspector after all service connections have been installed to the property line. Infiltration, exfiltration or air pressure tests shall be required by Illinois-American's direction. Note that the sewer shall be clean to the satisfaction of Illinois-American at the time of testing. Additional cleaning is required, the cost for this will not be borne by Illinois-American Water Company, and shall be paid by the contractor.

All sanitary sewers, including manholes, will be tested for leakage and shall satisfactorily meet the tests and requirements prior to acceptance of the work. Illinois-American or approved site inspector may order leakage tests made in sections of sewer as they are completed. Right is reserved to order the tests performed before back fill is placed over the pipe joints.

The Developer / Contractor shall furnish all labor, testing materials, and equipment such as plugs and stopgops, and shall perform the tests described herein under the supervision and to the satisfaction of Illinois-American. There are two tests which will be employed for gravity sewers:

- Infiltration test, infiltration test or air pressure test
- Deflection testing for thermoplastic pipe
- Service Trenching
- Manhole weeping test

- Air Pressure Tests

The section of sewer to be tested shall have been trench back filled and cleaned. The sewer pipe shall be pressurized to 2 PSIIG above the average back pressure of groundwater over the sewer pipe and the air pressure allowed to stabilize for at least two hours.

After the stabilization period, the line shall be pressurized to 3.5 PSIIG and the time in minutes required for a pressure drop to 2 PSIIG. If ground water is present, the air pressure shall be increased to 1.5 PSIIG above the level of the groundwater at the highest elevation of the sewer and the drop of one pound of air pressure measured in minutes (2.31 ft. of water = 1 PSIIG).

An leakage test results shall not be less than the rate of water per inch diameter per length of sewer pipe as specified in the table entitled "Air Test Table" as shown in Standard Specifications for Water and Sewer Main Construction in Illinois, and as shown within the standard details.

- Infiltration Test

This test may be used only when groundwater levels are at least 2.0 feet above the crown of the pipe for the entire length of the section to be tested during the entire period of the test. Groundwater levels may be measured on an open trench, in standpipes previously placed in back, from trenches during the back filling operation, or with piezometers installed in the manhole wall. Infiltration through joints shall be measured by using a watertight seal or any other approved device for volumetric measurement installed at the base end of the section under the test.

The maximum allowable rate of infiltration shall not exceed 100 gallons per twenty-four (24) hours per mile per inch diameter of the sewer pipe, for any section of the system.

Manholes will be tested by saturating the soil around the manhole. Test will be satisfactory if there are no visible leaks.

- Deflection Testing for Flexible Thermoplastic Pipe

The entire length of the man shall be tested for excessive deflection by pulling a "Go-No-Go" mandrel through the man from manhole to manhole.

Wherever possible and practical, the testing shall initiate at the downstream line and proceed toward the upstream lines.

Where deflection is found to be in excess of the allowable testing limits, the developer shall excavate the point of excess deflection and carefully compact around the point where the deflection was found. The soil shall then be retested for deflection. However, after the initial testing, should the deflected pipe fail to return to the original size (inside diameter) the line shall be replaced.

The deflection limits for flexible pipe shall not exceed 5% of the "Base I.D." of the pipe. A period of time will be required for the trench and pipe to come to equilibrium before the deflection test is performed. In all sewer systems the deflection test shall be performed no sooner than 30 days after the trench is back filled over the pipe.

- Testing

The Contractor shall produce a record video using a pan-and-tilt radial viewing, pipe inspection camera that pans 375 degrees and rotates 300 degrees. The inspection camera shall be provided for the inspection shall be specifically designed and constructed for such inspection. The camera shall be operated in 100% humidity conditions. The Contractor shall be provided with an accurate flexible ladder which extends to the inside the exact distance of the camera from the centerline of the starting manhole. The Contractor shall use a camera with camera height adjustment so that the camera lens is always centered at one-half the inside diameter, or higher, in most jobs being inspected. Lighting for the camera shall be suitable to allow a clear picture of the entire length of the pipe. A reflector in front of the camera lens is required to enhance lighting in dark or larger diameter pipe. The video camera shall be capable of showing on the type B City Name, Project Name, Contractor Name, Date, Size and material, line identification pipe manhole numbers of both manhole and ongoing boundary courses. The camera, television monitor, and other components of the video camera shall be capable of producing picture quality to the satisfaction of the Owner, and if unsatisfactory, equipment shall be removed and replaced with adequate equipment. CD video shall be supplied for all manhole surveys. All videos shall be submitted to the Owner, and will become the property of the Owner. Involvement shall be limited with the above referenced information. The camera shall be moved through the line in either direction at a moderate rate, stopping when necessary to permit proper documentation of the sewer's condition. In no case will the television camera be pulled at a speed greater than 10 feet per minute. Manual vehicles, power winches, TV cables, and powered winches or other devices shall not be used for pulling the camera along the line. The nature and location of the sewer conditions shall be noted to the camera view or reflected with proper documentation of the sewer conditions shall be noted by the Contractor. The camera operator shall always be at all service connections and identified by address or lot number. The camera operator shall always be at all beginning and ending manhole connections, each service connection, joints, valve details and all of pipe material transitions. TV inspection shall be continuous for pipe segments between manholes. Do not leave gaps in the video of a segment between manholes and do not show a single segment on more than one video, unless specifically approved by the Owner. The purpose of recording shall be to supply a visual and audio record of problem areas of the lines that may be repaired. Video recordings shall include an audio track recording of the inspection technician during the actual inspection work describing the parameters of the test being inspected (i.e. location, depth, diameter, pipe material), as well as describing connections, defects and unusual conditions observed during the inspection.

ILLINOIS-AMERICAN WATER COMPANY STANDARD

BASED ON EQUATIONS FROM ASTM C638 SPECIFICATION TIME AND SEC. REQUIREMENTS FOR PRESSURE DROP FROM 1.2 TO 1.2 TO 2 PSIIG WHEN TESTING ONE PIPE DIAMETER

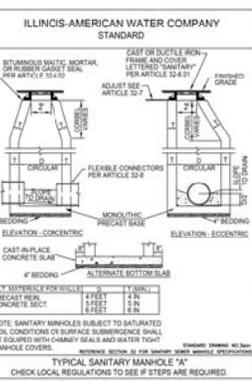
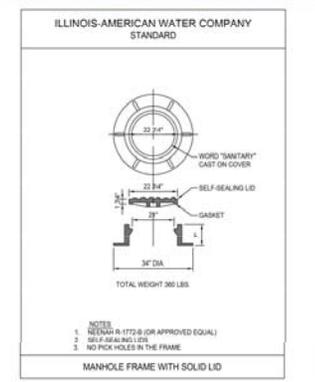
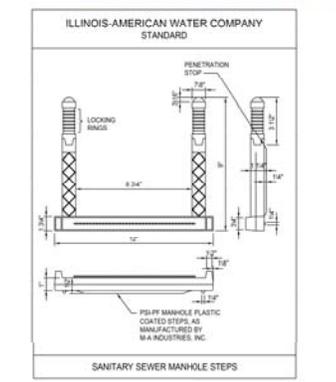
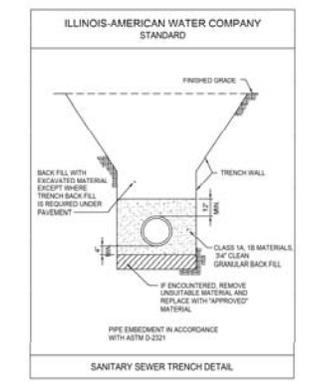
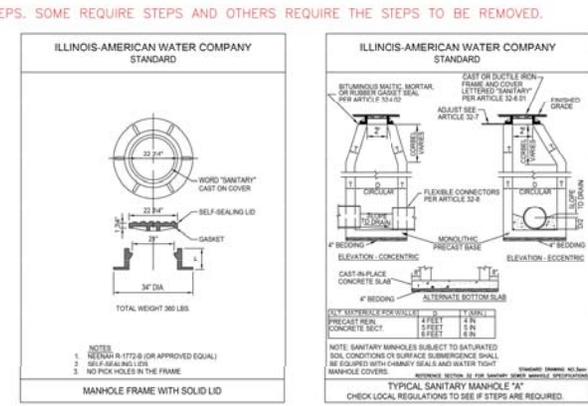
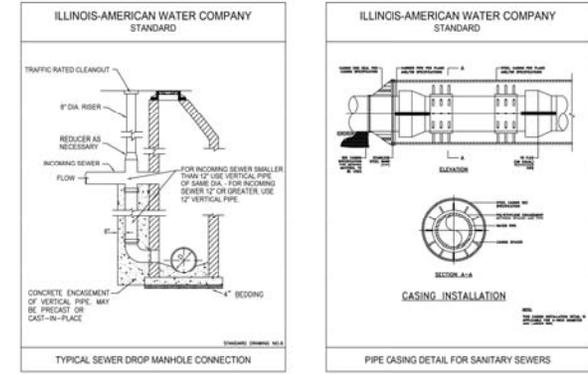
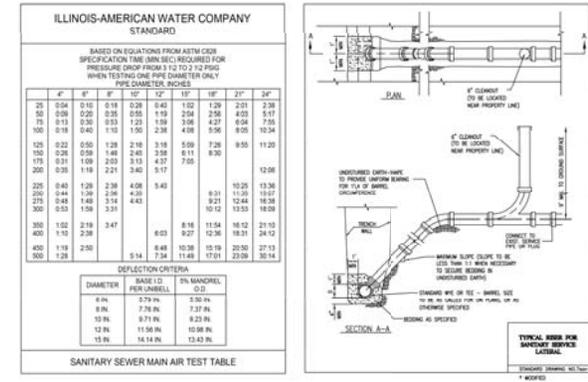
DIAMETER PER INCH	PIPE DIAMETER INCHES					
	4"	6"	8"	10"	12"	14"
25	0.04	0.10	0.19	0.29	0.40	1.02
50	0.09	0.20	0.35	0.55	1.19	2.58
75	0.13	0.30	0.53	1.03	1.98	4.27
100	0.18	0.40	1.10	2.30	4.08	9.56
125	0.22	0.50	1.28	2.78	5.69	12.88
150	0.28	0.60	1.48	2.85	8.11	19.30
175	0.31	1.09	2.03	3.13	4.37	7.05
200	0.35	1.16	2.21	3.40	5.17	
225	0.43	1.28	2.38	4.08	5.40	10.28
250	0.44	1.39	2.56	4.20	6.31	11.38
275	0.48	1.49	3.14	4.43	8.21	12.44
300	0.53	1.58	3.31		10.12	13.53
300	1.02	2.18	3.47		8.18	11.54
400	1.50	2.88		6.03	9.27	10.36
400	1.19	2.00		8.48	10.38	10.19
500	1.28			5.14	7.34	11.41

DEFLECTION CRITERIA

DIAMETER PER INCH	IN MANHOLE O.D.
8 IN.	0.75 IN.
10 IN.	0.75 IN.
12 IN.	1.15 IN.
14 IN.	1.4 IN.
16 IN.	1.4 IN.

- Manhole Testing

Manhole testing of each manhole shall be carried out immediately after assembly, after all connections are made, and prior to backfilling. All manholes shall be plugged with an approved non-toxic grout. No grout will be placed in the horizontal joints before testing. All pipes entering the manhole shall be plugged, taking care to securely brace the pipes from being drawn into the manhole. The test shall be performed by placing a 100 lb. test weight on the top of the frame and the test shall be accompanied with the manufacturer's recommendation using a "globe" style manhole tester, position the globe on the frame. A vacuum of 10 inches of mercury shall be drawn and the vacuum pump shall be kept on until the vacuum is closed, but time shall be measured for the vacuum to drop to 8 inches. The manhole shall pass the test if greater than 80 seconds for a 48" diameter manhole, 75 seconds for 60" diameter and 90 seconds for 72" manhole. If the manhole fails the test, complete necessary repairs and repeat test procedures until a satisfactory test is obtained.



CONTACT LOCAL DISTRICT FOR MANHOLE STEPS. SOME REQUIRE STEPS AND OTHERS REQUIRE THE STEPS TO BE REMOVED.

**IL AMERICAN WATER
DETAILS & SPECS #1**

DATE	12/20/2024
REVISIONS	
NO.	
DESCRIPTION	

Prepared For:

NSS GROUP, INC.
8060 Lawndale
Skokie, IL 60077

HOMER GLEN DEVELOPMENT
NWC of Bell Road and Glangary Drive - Lot 5
Homer Glen, Illinois

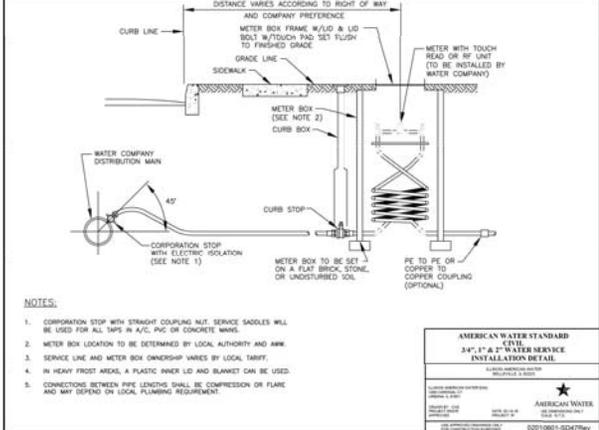
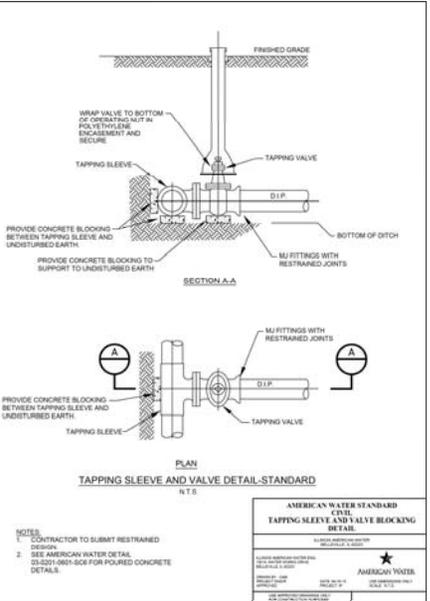
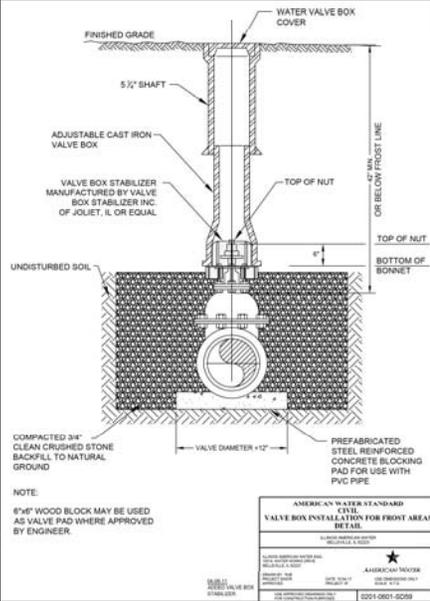
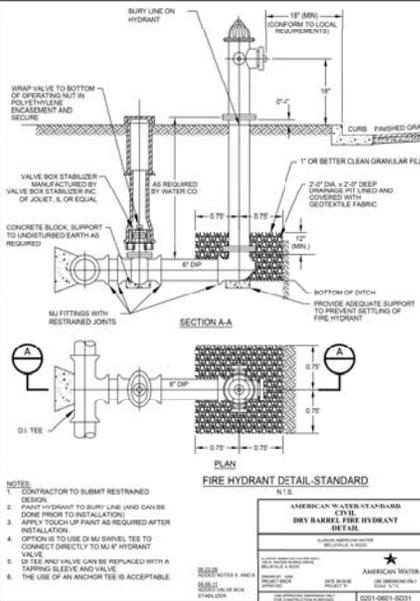
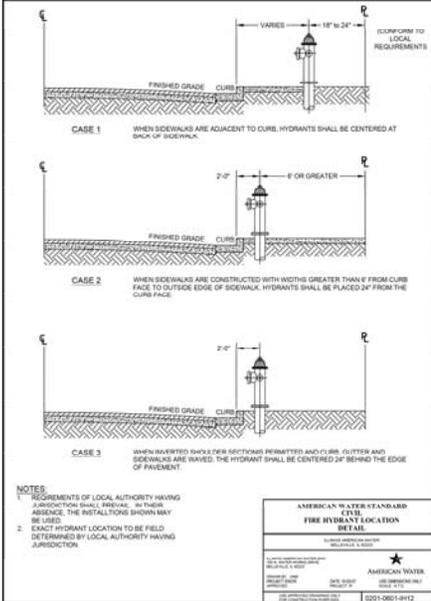
Prepared By:

WatOrmark
Engineering
Resources

www.watormark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (800) 374-1600

CHECKED BY: J. MILLER
DESIGNED BY: M. NELSON
DRAWN BY: JOSH MALLER
DATE: JANUARY 10, 2024
SCALE: NONE
PROJECT NO.: 21-07-4

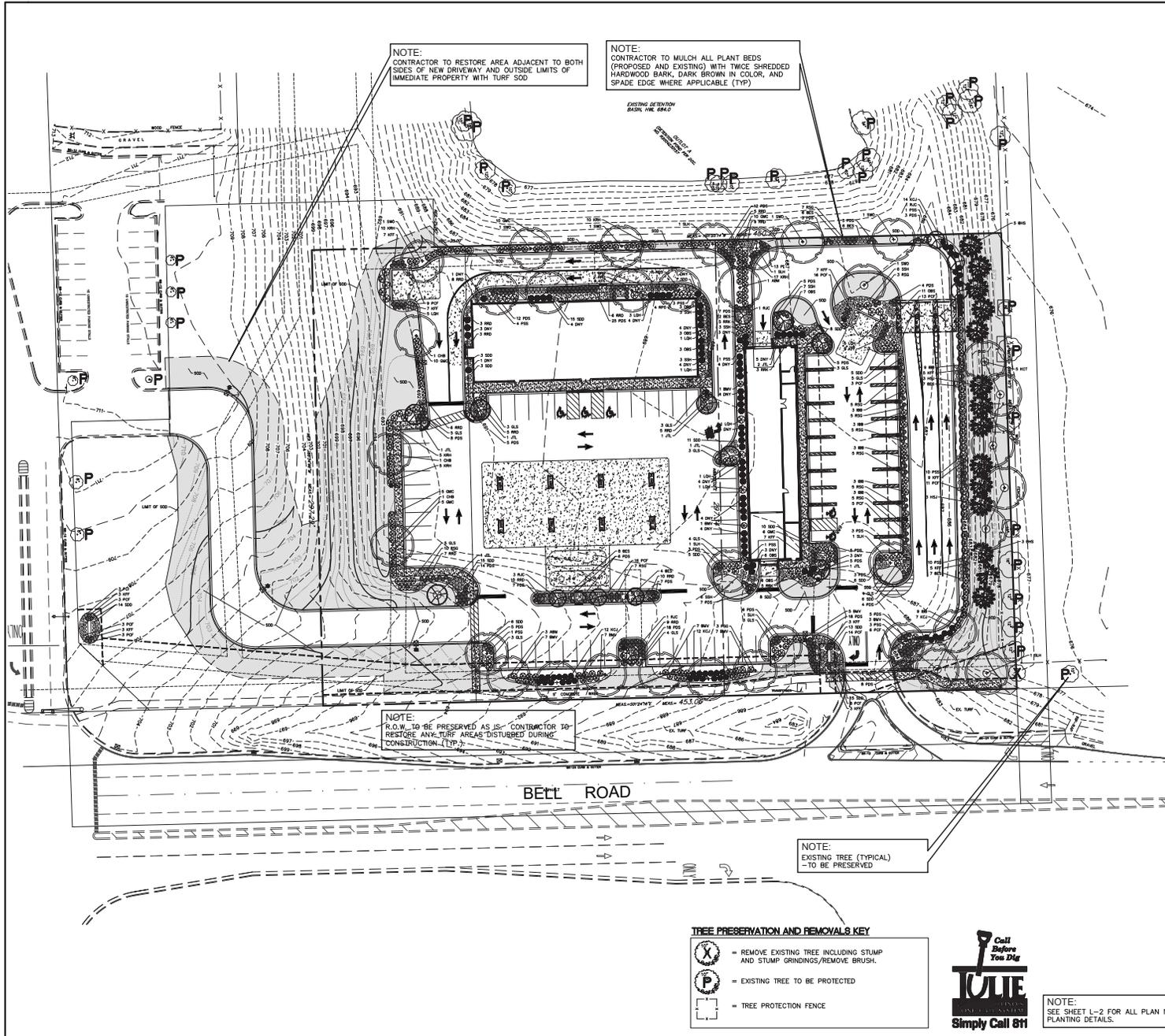
LAW DETAILS & SPECS #1



DATE: 1/12/2024	REVISIONS:	NO.	DESCRIPTION:
	1.		PRELIMINARY COMMENTS DATED 1/12/2024
Prepared For:			
NSS GROUP, INC. 8060 Lawndale Skokie, IL 60077 HOMER GLEN DEVELOPMENT NWC of Bell Road and Glangary Drive - Lot 5 Homer Glen, Illinois			
Prepared By:			
Watermark Engineering Resources watermark-engineering.com 2631 Ginger Woods Pkwy Aurora, IL 60002 (800) 374-1600			
CHECKED BY: J. MILLER	DESIGNED BY: A. NELSON	DRAWN BY: J. MILLER	DATE: JANUARY 10, 2024
PROJECT NO.: 21-07-4		SCALE:	

IL AMERICAN WATER
DETAILS & SPECS #2

C-12



SITE DATA

TOTAL PROPERTY AREA = 134,282 S.F. (3.08 AC.)
 TOTAL IMPERVIOUS AREA = 90,386 S.F. (2.07 AC.)(67.1%)
 PARKING REQUIRES = 39 (SEE P.U.D. DOCUMENTS)
 PARKING PROPOSED = 88 (INCLUDES 16 PUMP AND 22 VACUUM SPACES)

LOT 1 DATA

LOT 1 AREA = 56,156 S.F. (1.29 AC.)
 IMPERVIOUS AREA = 35,863 S.F. (0.82 AC.) (63.9%)
 VACUUM SPACES (INCLUDES 1 ADA SPACE) 22
 ADA ACCESSIBLE SPACES 1
 STANDARD SPACES 45
 TOTAL SPACES 68

BUILDING AREA = 4,865 S.F.
 1 STORY, 30'-11" TALL BUILDING
 F.A.R. = 0.09

LOT 2 DATA

LOT 2 AREA = 78,126 S.F. (1.79 AC.)(INC. ADDITIONAL 100 FT.)
 IMPERVIOUS AREA = 54,426 S.F. (1.25 AC.) (69.7%)
 ADA ACCESSIBLE SPACES 3
 PUMP STATIONS 16
 STANDARD SPACES 62
 TOTAL SPACES 81

BUILDING AREA = 7,710 S.F.
 1 STORY, 22'-1" TALL BUILDING
 F.A.R. = 0.10

PLANT LIST

SHADE, ORNAMENTAL AND CONIFEROUS TREES

QTY.	ABREV.	BOTANICAL NAME	COMMON NAME	SIZE
4	ARM	Aster rubrum Armstrong	Armstrong Maple	3 1/2" Cal.
8	BIS	Prunella sp. var. americana	Black Hills Spruce	4" Ht.
1	CHS	Castanea coccinea 'Chocapuff'	Chocapuff Honeylocust	3 1/2" Cal.
4	HC	Juniperus horizontalis 'Hoopla'	Hoopla Eastern Red Cedar	4" Ht.
8	JL	Juniperus reticulata	Japanese Tree Life	2" Cal.
3	KCT	Gymnocladia dioica 'Tapestry' (Male Only)	Expensive Kentucky Coffee Tree	2 1/2" Cal.
7	PKS	Cornus florida 'Prostrata Sereno' (Male Only)	Prostrata Kerria Shrub	2 1/2" Cal.
8	RUC	Malus 'Red Jewel'	Red Jewel Crabapple	2" Cal.
4	RPD	Quercus robur 'Regal Prince'	Regal Prince Oak	2 1/2" Cal.
4	SJA	Quercus macrocarpa 'Jamaica Bayline'	Shiny-jawed Oak	2 1/2" Cal.
4	SAC	Quercus bicolor	Swamp White Oak	2 1/2" Cal.

FLOWERING AND EVERGREEN SHRUBS

QTY.	ABREV.	BOTANICAL NAME	COMMON NAME	SIZE
36	BAN	Viburnum acerifolium 'Chocofruit'	Black Muffin Viburnum	5 Gal.
48	GLS	Moss-roseholia 'Soleil'	Soleil Lilac	5 Gal.
48	OAC	Ribes alpinum 'Green Mound'	Green Mound Alpine Currant	5 Gal.
39	IBS	Artemisia tridentata 'Morton'	Impatiens Beach Black Chokeberry	5 Gal.
48	IBS	Artemisia tridentata 'Morton'	Kapiti Red Honey-suckle	5 Gal.
18	LPH	Hydrangea paniculata 'SANNIPE'	Little Quick Fire Hydrangea	5 Gal.
48	PSS	Spiraea betulifolia 'COLUMBIA'	Pink Sparler Spruce	5 Gal.
12	RHS	Rosa 'Rhapsody'	Knock Out Rose	5 Gal.
48	DNY	Taxus media 'Davidsonii'	Davidson's Tree	5 Gal.
48	KCC	Juniperus x pfitzeriana 'Kallia's Compact'	Kallia's Compact Juniper	5 Gal.

PERENNIALS, ORNAMENTAL GRASS AND GROUNDCOVERS

QTY.	ABREV.	BOTANICAL NAME	COMMON NAME	SIZE
50	BSL	Rubus idaeus 'Sonnenschein'	Sonnenschein Black and Blue	1 Gal.
35	FF	Callirhoe coccinea 'Karl Foerster'	Karl Foerster Feather Reed Grass	5 Gal.
48	OSB	Heuchera 'Clouds'	Clouds Coralbell	1 Gal.
113	PLP	Erigeron 'Magnus'	Magnus Purple Coneflower	1 Gal.
258	PDS	Spandax 'Spartan'	Spartan Daylily	1 Gal.
300	HRB	Heuchera 'Royal Helios'	Royal Helios Daylily	1 Gal.
57	RSD	Festuca ovina 'Red Sandstone'	Red Sand Stone	5 Gal.
137	HRB	Heuchera 'Soleil'	Soleil de Oro Daylily	1 Gal.
38	SH	Heuchera 'Sun and Sandstone'	Sun and Sandstone Heuchera	1 Gal.
500	SLD	Solidago 'Blazing Star'	Blazing Star	1 Gal.

*These all not installed on 3" pipes or greater.



LANDSCAPE PLAN

SCALE: 1" = 30'

0 15 30 60 90 120

LANDSCAPE PLAN

L-1

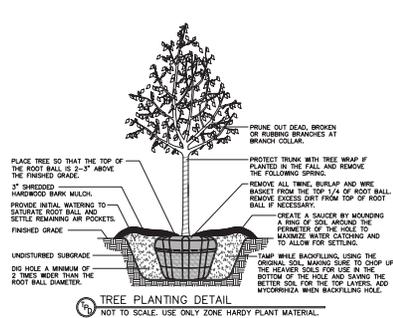
DATE: 10/20/24
 REVISIONS:
 NO. DESCRIPTION
 01
 PREPARED FOR:
 HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glangary Drive - Lot 5
 Homer Glen, Illinois

PREPARED BY:
 WATSON MARK ENGINEERING RESOURCES
 www.watmark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (800) 374-1600

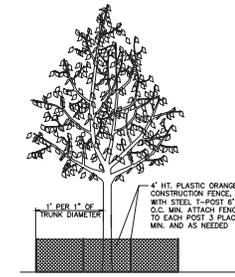
CHECKED BY: J. MILLER
 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MULLER
 DATE: JANUARY 10, 2024
 SCALE: 1" = 30'
 PROJECT NO.: 21-074

LANDSCAPE PLAN

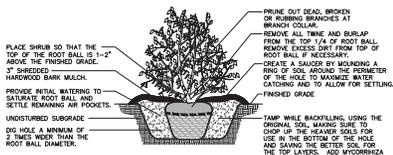
L-1



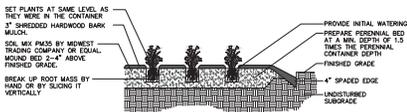
16 TREE PLANTING DETAIL
NOT TO SCALE. USE ONLY ZONE HARDY PLANT MATERIAL.



16 TREE PROTECTION DETAIL
NOT TO SCALE.



16 SHRUB PLANTING DETAIL
NOT TO SCALE. USE ONLY ZONE HARDY PLANT MATERIAL.



16 PERENNIAL PLANTING DETAIL
NOT TO SCALE. USE ONLY ZONE HARDY PLANT MATERIAL.

LANDSCAPE NOTES

1. ALL PLANT MATERIAL SHALL BE HARDY TO THE ZONE IT IS BEING PLANTED IN. ALL TREES AND SHRUBS ARE TO BE BALLED OR BURLAPED UNLESS OTHERWISE NOTED AND SHALL BE GROWN IN ACCORDANCE WITH THE STANDARDS SET FORTH BY THE LATEST EDITION OF AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY AMERICANH&W.
2. PLANT SIZES CALLED OUT ON THIS PLAN ARE THE MINIMUM SIZE REQUIRED. PLANTS WHICH FAIL TO MEET THE SIZES LISTED, SHALL BE REJECTED AT THE EXPENSE OF THE CONTRACTOR.
3. CONTRACTOR MUST VERIFY ALL MATERIAL QUANTITIES AS SHOWN ON THE DRAWING. THE PLANT LIST PROVIDED ON THIS PLAN IS FOR CONVENIENCE ONLY.
4. SUBSTITUTIONS MAY NOT BE MADE WITHOUT THE APPROVAL OF THE LANDSCAPE ARCHITECT/DESIGNER.
5. THE CONTRACTOR SHALL NOTIFY ALL APPROPRIATE AGENCIES AND UTILITY LOCATORS PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOT BEGIN ANY WORK ON-SITE UNTIL ALL UTILITIES HAVE BEEN LOCATED. CONTRACTOR SHALL OBTAIN "AS-BUILT" PLANS FOR ALL IRRIGATION AND LIGHTING PRIOR TO CONSTRUCTION.
6. CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL UTILITIES INCLUDING IRRIGATION AND LIGHTING. ALL DAMAGE SHALL BE REPAIRED TO A NEW CONDITION IN ACCORDANCE WITH ALL CODES AT NO COST TO THE OWNER - SEE NOTE 5.
7. ALL UNSUITABLE MATERIAL (CONCRETE, AGGREGATE, STONE, CRUSHED ASPHALT, BRICK ETC) SHALL BE REMOVED, INCLUDING HAUL OFF, PRIOR TO PLANTING AND SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
8. SOIL MIX PADS BY MONEST TRADING COMPANY OR EQUAL SHALL BE ROTOTILLED INTO ALL PERENNIAL AND ANNUAL PLANTING BEDS PRIOR TO THE INSTALLATION OF THE PLANT MATERIAL. A SLOW RELEASE GRANULAR FERTILIZER SHALL BE APPLIED TO ALL ANNUAL AND PERENNIAL PLANTING BEDS AT THE RECOMMENDED RATE, AND SHALL BE ROTOTILLED IN WITH THE ABOVE SOIL MIXTURE BEFORE THE PLANT MATERIAL IS INSTALLED.
9. CONTRACTOR TO PROVIDE THOROUGH INITIAL WATERING OF ALL PLANTINGS WITHIN 12 HOURS OF INSTALLATION TO ENSURE ALL AIR POCKETS HAVE BEEN REMOVED AROUND ROOT BALL.
10. ALL PLANT BED AREAS ARE TO BE MULCHED WITH 3\"/>

TREE PRESERVATION NOTES

1. ALL GRADING AND CONSTRUCTION EQUIPMENT SHALL BE FORBIDDEN FROM ENCRUACHING WITHIN THE ROOT ZONE OF A TREE TO BE PRESERVED. NO EXCESS SOIL, ADDITIONAL FILL, LIQUIDS OR CONSTRUCTION DEBRIS SHALL BE PLACED WITHIN THE ROOT ZONE OF ANY TREE THAT IS REQUIRED TO BE PRESERVED.
2. CRUSHED LIMESTONE HYDROCARBONS AND OTHER MATERIALS DETRIMENTAL TO TREES SHALL NOT BE DUMPED WITHIN THE ROOT ZONE OF ANY TREE NOR AT ANY HIGHER LOCATION WHERE DRAINAGE TOWARD THE TREE COULD CONSEQUENTLY EFFECT THE HEALTH OF THE TREE.
3. APPROPRIATE FENCING (SEE DETAIL) SHALL TEMPORARILY BE INSTALLED AT THE PERIPHERY OF THE ROOT ZONE OF TREE(S) DESIGNATED FOR PRESERVATION, AS INDICATED ON THE PLAN.
4. ALL REQUIRED FENCING SHALL BE IN PLACE PRIOR TO CONSTRUCTION ACTIVITY. THE FENCING SHALL REMAIN IN PLACE DURING THE ENTIRE CONSTRUCTION PERIOD. ALL FENCING MUST BE SECURED TO 5\"/>



LANDSCAPE DETAILS PLAN

DATE	
REVISIONS	
NO.	
DESCRIPTION	

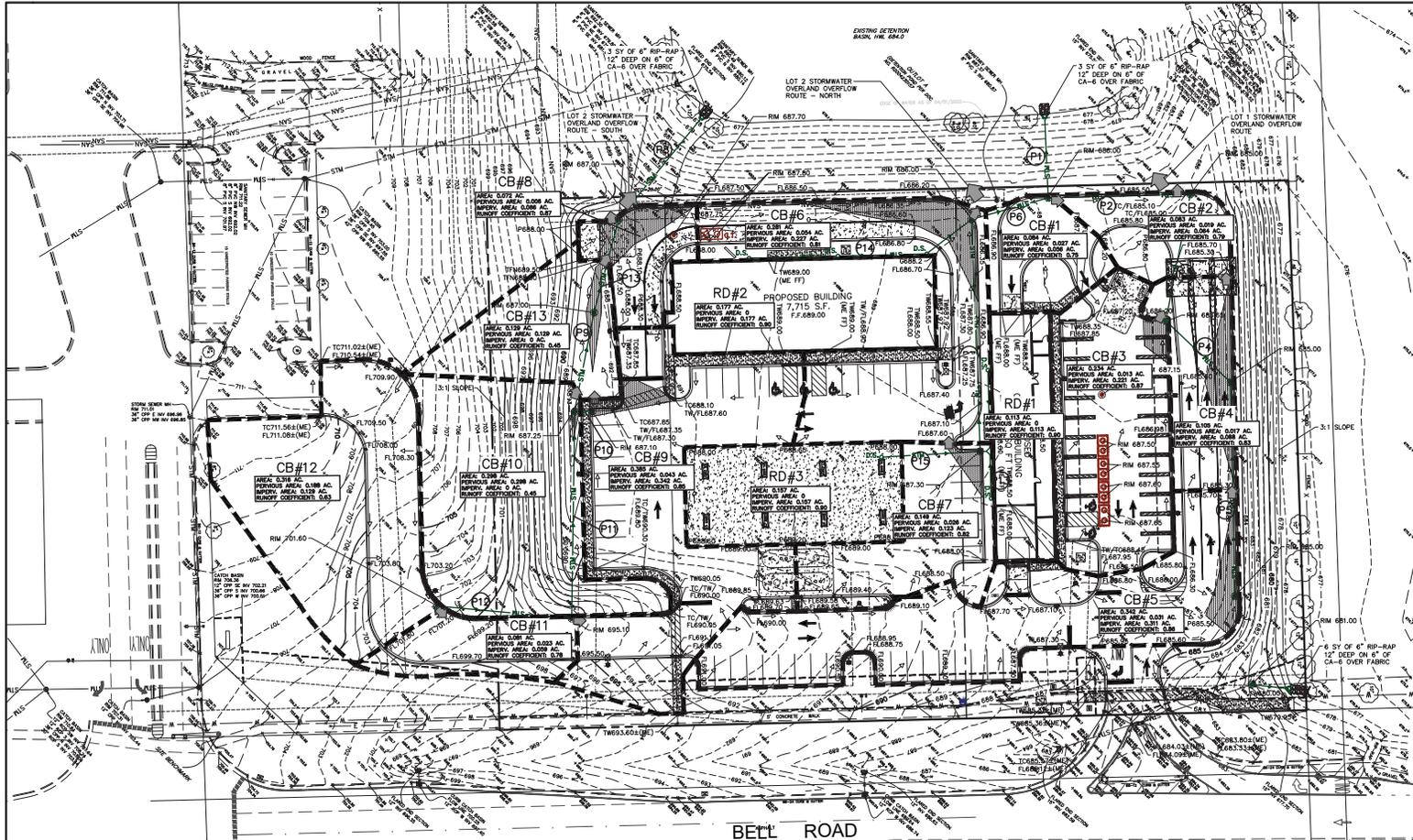
Prepared For:
NSS GROUP, INC.
8060 Lawndale
Skokie, IL 60077
HOMER GLEN DEVELOPMENT
NWC of Bell Road and Glangary Drive - Lot 5
Homer Glen, Illinois

Prepared By:
Wat & Ormark
engineering
resources

LANDSCAPE PLAN
DESIGNED BY
JOSEPH D. DAVITO, P.E.
J. DAVITO DESIGN, INC.
(847) 469-8797

Checked By: J. Miller
Designed By: J. Miller
Drawn By: Joshua Meller
Date: January 10, 2024
Scale: 1" = 30'
Project No.: 21-074

L-2



GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE ALTA/NSPS LAND TITLE SURVEY (SURVEY PROJECT #22-22484 DATED 09/11/2023) PREPARED BY: GENTILE AND ASSOCIATES, INC. 550 E. ST. CHARLES PLACE, LOMBARD, ILLINOIS 60148 (630) 916-6282
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

SITE STORMWATER DATA

PROPOSED SITE DATA
 LOT AREA = 134,282 SF (3.08 AC)
 PROPOSED IMPERVIOUS = 90,289 SF (2.07 AC) (78.3%)
 PROPOSED PERVIOUS = 43,993 SF (1.01 AC) (21.7%)

SITE RUNOFF CURVE NUMBER
 ON IMPERVIOUS AREA (2.07 AC) = 98
 ON PERVIOUS AREA (1.01 AC) = 74
 COMPOSITE CN VALUE = $[(98 \times 2.07) + (74 \times 1.01)] / 3.08 = 90.1$

DETENTION BASIN CN = 94
 90.1 ≤ 94, THEREFORE OK
 NO ADDITIONAL STORAGE REQUIRED
 THERE ARE NO WETLANDS ON THIS PROPERTY.
 THERE IS NO FLOODPLAIN ON THIS PROPERTY.
 THERE WILL BE NO PERMANENT SOIL STOCKPILES ON SITE.

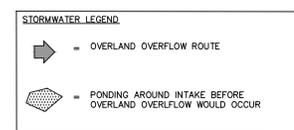
Inlet Capacity Calculations for Car Wash - Homer Glen, IL

F.F. Elev: 688.00 Lot 1 - Carwash
 689.00 Lot 2 - Conveyance Stone

TOWNS Bulletin 75 - 10 min rainfall intensity, I (in/hr):
 10-YR Recurrence Interval: 4.48
 100-YR Recurrence Interval: 10.80

Attachment Label on Stormwater Exhibit	DRAINAGE AREA			Runoff Coefficient, c	Inlet Type	Inlet Open Area (sq ft)	Inlet Rise Length (ft)	Inlet Elev (ft)	Clearflow Elev (ft)	Height over inlet before overflow, (ft)	INLET CAPACITY		10-YEAR FLOW		100-YEAR FLOW		COMMENT		
	Surface Area (AC)	Impervious Area (AC)	Pervious Area (AC)								Flow capacity of inlet, Q (**)	Flow from Contributing Area (cfs)	Flow from Inlet (cfs)	Total Flow (cfs)	Flow from Contributing Area (cfs)	Flow from Inlet (cfs)	Total Flow (cfs)	Inlet Capacity > Total Flow	Inlet Capacity > 100 Year Total Flow
CB#1	0.0837	0.0564	0.0273	0.75	IDOT 11V CB	0.59	2.25	688.00	686.04	0.04	0.69	0.41	0	0.41	0.68	0	0.68	Yes	Yes
CB#2	0.0630	0.0635	0.0195	0.79	Curb box	1.00	2.25	685.09	685.00	0.00	0.00	0.23	0	0.23	0.71	0	0.71	N/A	N/A
CB#3	0.2338	0.2204	0.0134	0.87	Curb box	1.00	2.25	685.60	686.15	0.56	6.43	1.32	0	1.32	2.25	0	2.25	Yes	Yes
CB#4	0.1049	0.0880	0.0169	0.83	Curb box	1.00	2.25	685.09	685.30	0.30	4.17	0.56	0	0.56	0.94	0	0.94	Yes	Yes
CB#5	0.3418	0.3114	0.0304	0.86	Curb box	1.00	2.25	685.09	685.30	0.30	4.17	1.90	0	1.90	3.17	0	3.17	Yes	Yes
CB#6	0.2814	0.2273	0.0541	0.81	Curb box	1.00	2.25	686.09	686.50	0.50	6.43	1.48	0	1.48	2.67	0	2.67	Yes	Yes
CB#7	0.4487	0.1228	0.0529	0.82	Curb box	1.00	2.25	687.39	687.60	0.30	4.17	0.79	0	0.79	1.32	0	1.32	Yes	Yes
CB#8	0.0716	0.0661	0.0055	0.87	Curb box	1.00	2.25	687.09	687.50	0.50	6.43	0.40	0	0.40	0.67	0	0.67	Yes	Yes
CB#9	0.3841	0.3415	0.0426	0.85	Curb box	1.00	2.25	687.31	687.60	0.50	6.43	2.12	0.82	2.94	3.53	1.46	5.24	Yes	Yes
CB#10	0.2977	0	0.2977	0.78	Inlet	0.90	0.00	687.21	687.65	0.40	3.06	0.87	0	0.87	1.45	0	1.45	Yes	Yes
CB#11	0.0813	0.0588	0.0225	0.78	IDOT 11V CB	0.59	2.25	685.10	685.14	0.04	0.69	0.41	0.29	0.69	0.68	0.01	0.69	Yes	Yes
CB#12	0.1361	0.1282	0.0079	0.83	IDOT 11V CB	0.59	2.25	701.39	701.64	0.06	0.81	0.63	0.09	2.16	-1.47	0.69	Yes	Yes	
CB#13	0.1290	0	0.1290	0.45	Inlet	0.90	0.00	687.09	687.50	0.50	3.42	0.38	0	0.38	0.63	0	0.63	Yes	Yes
RD#1	0.1128	0.1128	0	0.90															
RD#2	0.1774	0.1774	0	0.90															
RD#3	0.1568	0.1568	0	0.90															
Total	3.0040	2.1124	0.8726	0.78										12.36		20.87			

Composite runoff coefficient: 0.78



STORMWATER EXHIBIT

Prepared For: **NSS GROUP, INC.**
 8060 Lawndale
 Skokie, IL 60077

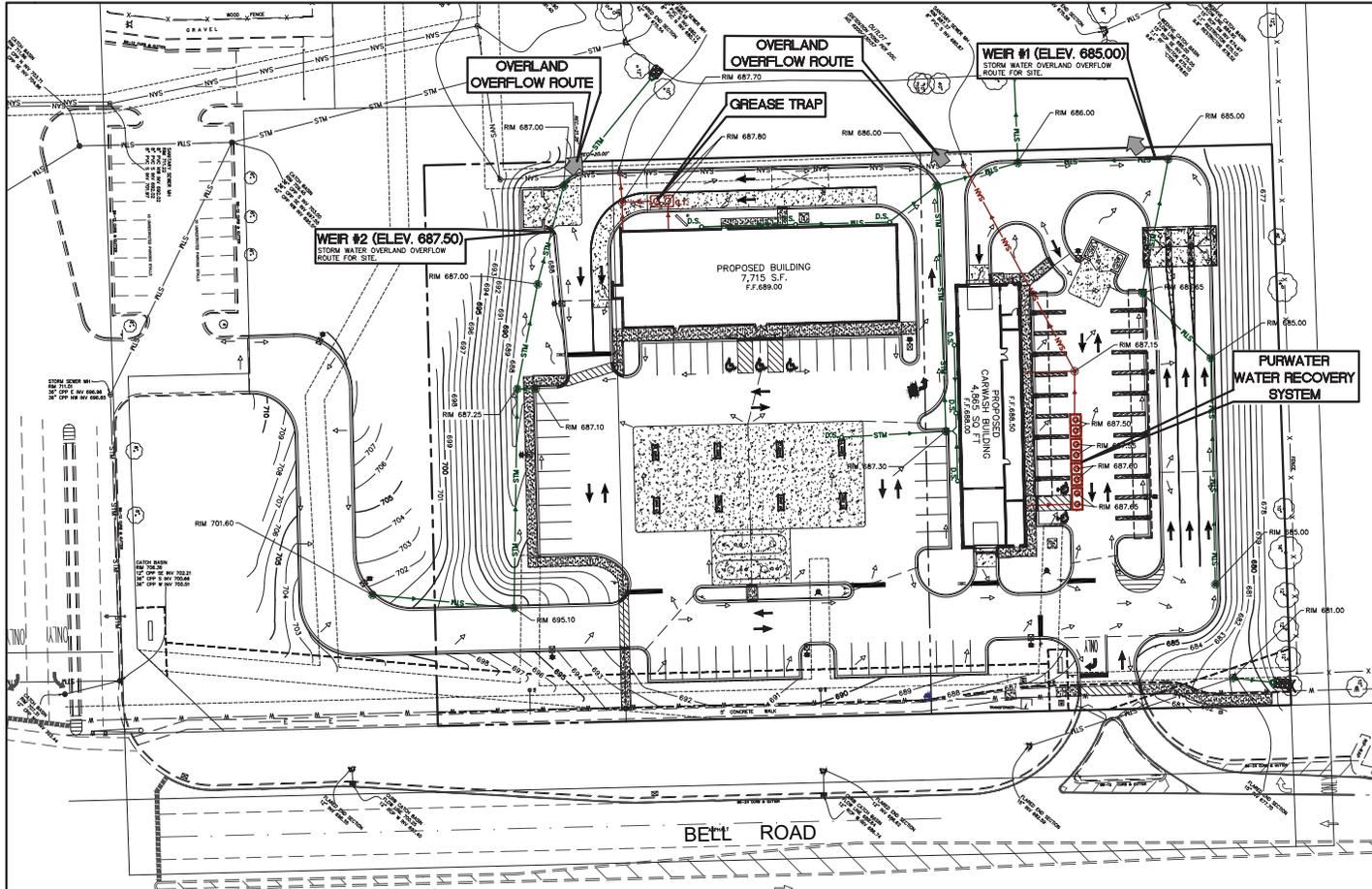
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glangary Drive - Lot 5
 Homer Glen, Illinois

Prepared By: **Wat & Armark engineering resources**
 2631 Ginger Woods Pkwy Aurora, IL 60502 | (815) 374-1900

CHECKED BY: J. MILLER
 DESIGN BY: A. NELSON
 DRAWN BY: JOSH MILLER
 DATE: JANUARY 10, 2024
 SCALE: 1" = 30'
 PROJECT NO.: 21-074

STORM WATER EXHIBIT

1 of 1



THE OWNER SHALL ASSUME RESPONSIBILITY FOR THE FOLLOWING PERPETUAL MAINTENANCE ACTIVITIES

MAINTENANCE PLAN

- GENERAL:**
 - REGULAR INSPECTIONS AND ROUTINE MAINTENANCE OF GENERAL AREAS SHALL BE PERFORMED ON A MONTHLY OR AS-NEEDED BASIS. ITEMS OF CONCERN INCLUDE:
 - LITTER AND DEBRIS SHALL BE CONTROLLED.
 - LANDSCAPED AREAS SHALL BE MAINTAINED WITH REGULAR MOWING AND RESTORED WITH APPROPRIATE SEEDING/VEGETATION AS NECESSARY.
 - ACCUMULATED SEDIMENT SHALL BE REMOVED PROPERLY, ALONG WITH ANY WASTES GENERATED DURING MAINTENANCE OPERATIONS.
 - RRAP AREAS SHALL BE REPAIRED WITH THE ADDITION OF NEW RRAP, AS NECESSARY, OF SIMILAR SIZE AND SHAPE.
 - ROADS SHALL BE SWEEP, VACUUMED AND/OR WASHED ON A REGULAR BASIS.
- STORMWATER COLLECTION SYSTEM:**
 - THE OWNER SHALL PERFORM MONTHLY INSPECTIONS OF ALL COMPONENTS OF THE STORMWATER COLLECTIONS SYSTEM. THE MONTHLY INSPECTION SHALL OCCUR BETWEEN MARCH AND NOVEMBER AND INCLUDE THE FOLLOWING SPECIFIC AREAS OF CONCERN:
 - STORM INLETS/MANHOLES
 - REMOVE ACCUMULATED LEAVES AND OTHER DEBRIS FROM GRATES.
 - RESET COVERS/LIDS ON AS-NEEDED BASIS.
 - REMOVE ACCUMULATED SEDIMENT FROM THE BOTTOM OF MANHOLE WHEN 50% OF SLUMP IS FILLED.
 - STORM SEWERS/CONVERTS
 - VISUALLY INSPECT PIPES BY REMOVING MANHOLE LIDS, MAKE REPAIRS AS NECESSARY.
 - STORM SEWERS AND CONVERTS SHALL BE CHECKED FOR SILTATION DEPOSITS AT INLETS, OUTLETS, AND WITHIN THE CONDUIT, CLEAN OUT AS NECESSARY.
 - RESTORE RRAP AT OUTFALLS IF DISOBN OBSERVED.
 - REPLANT AND RESEED ANY ERODED AREAS.
 - OVERLAND FLOW ROUTES (DITCHES/SWALES)
 - ANNUAL VISUAL INSPECTIONS SHALL BE PERFORMED THAT VERIFY THE DESIGN CAPACITY OF THE OVERLAND FLOW ROUTES IS MAINTAINED, THE SLOPE AND CROSS-SECTIONAL AREA OF THE DITCH/SWALE SHALL BE VERIFIED DURING THIS INSPECTION.
 - REMOVE ANY OBSTRUCTIONS THAT HAVE BEEN PLACED IN THE DRAINAGE PATH.
 - SEED AND SOO ANY ERODED AREAS.
 - RESTORE RRAP AS NECESSARY.
 - RESURFACE TO PROVIDE POSITIVE DRAINAGE AS NECESSARY (A PROFESSIONAL LAND SURVEYOR MAY BE REQUIRED TO CHECK GRADES TO ENSURE POSITIVE DRAINAGE).
 - REGULAR MOWING TO CONTROL VEGETATION.
 - NOTIFY BOTTOM OF DRY SWALES IF NOT DRAWING DOWN WITHIN 48-HOURS.
- VEGETATED AREAS:**
 - NEED FOR PLANTING, RESEEDING OR SOODING. SUPPLEMENT ALTERNATIVE NATIVE VEGETATION IF A SIGNIFICANT PORTION HAS NOT ESTABLISHED (50% OF THE SURFACE AREA AFTER SECOND GROWING SEASON). RESEED WITH ALTERNATIVE NATIVE GRASS SPECIES IF ORIGINAL GRASS COVER HAS NOT SUCCESSFULLY ESTABLISHED.
 - EVIDENCE OF GRAZING, MOTORVOKES, OR OTHER VEHICLES. REPAIR.
 - CHECK FOR INVASIVE VEGETATION. REMOVE WHEN POSSIBLE.
 - REGULAR MOWING TO CONTROL VEGETATION. IT IS RECOMMENDED THAT NATIVE VEGETATION REMAIN UNCUT.
 - DEAD OR DAMAGED NON-NATIVE GRASSY AREAS - REPAIR WITH SEEDING WITH FERTILIZATION OR SEEDING WITH MULCH.
 - COMPACTED STORAGE AREA SHALL BE RESEEDED WITH APPROPRIATE VEGETATION ACCORDING TO THE APPROVED PLANTING PLAN.
- QUALIFIED SEWER CONSTRUCTION:**
 - PERFORM MANHOLE INSPECTIONS ONCE EVERY FIVE YEARS. MAKE REPAIRS AS NECESSARY.
 - PERFORM SEWER INSPECTIONS ONCE EVERY FIVE YEARS. MAKE REPAIRS AS NECESSARY.
 - PERFORM REGULAR SEWER CLEANING SO THAT EVERY SEWER SEGMENT IS CLEANED ONCE EVERY FIVE YEARS.
 - REMOVE ANY OBSTRUCTIONS PLACED IN MAINTENANCE EASEMENTS THAT MAY IMPEDE MAINTENANCE EQUIPMENT ACCESS.
- GREASE TRAP:**
 - SERVICE BY A VACUUM OR PUMPING TRUCK EVERY 90 DAYS OR SOONER IF NECESSARY.
 - THE UNIT MUST BE EMPTIED COMPLETELY. ONCE THE TANK IS EMPTIED, IT SHOULD BE RINSED WITH WATER TO REMOVE ANY OIL, GREASE OR SOLIDS IN THE UNIT.
 - IF TOOLS ARE NECESSARY FOR COMPLETE REMOVAL OF CRUSTED GREASE OR SOLIDS, CARE SHOULD BE TAKEN NOT TO DAMAGE INTERNAL PVC PIPING, BATTERIES AND TANK WALLS.
 - CHECK THE CONDITION OF THE GASKETS AND/OR O-RING UNDER THE ACCESS COVER. CLEAN AND REPLACE AS NECESSARY.
- PURWATER WATER RECOVERY SYSTEM:**
 - CHECK HMI SCREEN FOR FAULT MESSAGES DAILY.
 - CHECK PUMP PRESSURE/LIT VALVE WEEKLY.
 - CLEAN BASKET STRAINER WEEKLY.
 - VERIFY FLOW THROUGH LIT VALVE WEEKLY.
 - PUMP OUT SETTLING TANKS EVERY 6 MONTHS. WASH TANK WALLS DOWN WHILE EMPTY AND PUMP OUT. INSPECT FOOT VALVES AND RECIRCULATION LINES.

PROPERTY INDEX NUMBER (P.I.N.)
05-02-213-002-0000, 05-02-213-003-0000

LEGAL DESCRIPTION AS SURVEYED

PARCEL 1:
LOT 5 IN MENARDS SUBDIVISION, BEING PART OF THE NORTHEAST QUARTER AND THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 36 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2004 AS DOCUMENT NO. R2004230157, IN WILL COUNTY, ILLINOIS.

PARCEL 2:
THE NORTH 100.00 FEET OF LOT 4 LYING EASTERLY OF THE WESTERLY LINE OF LOT 5, AND ITS SOUTHERLY EXTENSION THEREOF MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 4; THENCE SOUTH 01 DEGREE, 24 MINUTES, 16 SECONDS EAST ALONG EASTERLY LINE OF SAID LOT 4, 100.00 FEET; THENCE SOUTH 88 DEGREES 34 MINUTES 46 SECONDS WEST ALONG A LINE 100.00 FEET SOUTH OF AND PARALLEL WITH THE NORTH LINE OF SAID LOT 4, 279.26 FEET; THENCE NORTH 1 DEGREE 25 MINUTES 14 SECONDS WEST ALONG SAID WESTERLY LINE AND SOUTHERLY EXTENSION OF LOT 5, 100.00 FEET TO THE MOST NORTHERLY NORTHWEST CORNER OF SAID LOT 4; THENCE NORTH 88 DEGREES 34 MINUTES 46 SECONDS EAST, ALONG THE NORTH LINE OF SAID LOT 4, 279.29 FEET TO THE POINT OF BEGINNING, IN MENARDS SUBDIVISION, BEING A SUBDIVISION OF PART OF THE NORTHEAST QUARTER AND THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 36 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2004 AS DOCUMENT NO. R2004230157, IN WILL COUNTY, ILLINOIS.

TOTAL AREA: 134,280.54 SQ. FT., 3.08 AC. (MORE OR LESS)



BMP OPERATION & MAINTENANCE PLAN

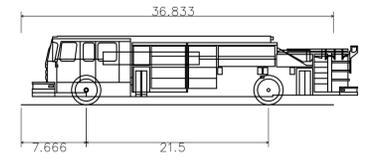
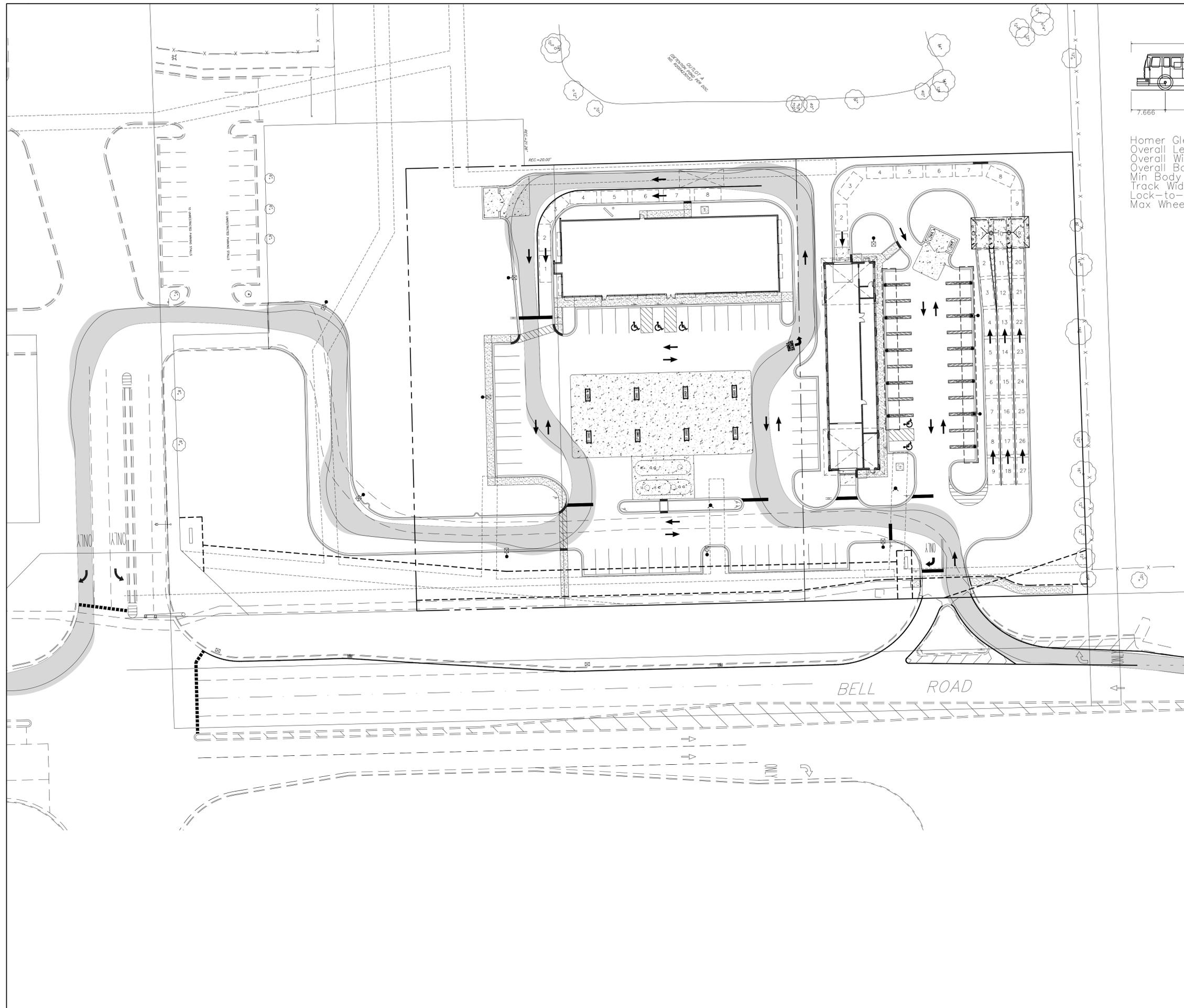
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DATE: 1/10/2024	REVISION: 2	DESIGNED BY: J. MILLER
DATE: 1/10/2024	REVISION: 3	DRAWN BY: JOSH MULLER
DATE: 1/10/2024	REVISION: 4	SCALE: 1" = 30'
DATE: 1/10/2024	REVISION: 5	PROJECT NO.: 21-074

Prepared For: **NSS GROUP, INC.**
8060 Lawlode
Skokie, IL 60077

HOMER GLEN DEVELOPMENT
NWC of Bell Road and Glangary Drive - Lot 5
Homer Glen, Illinois

Prepared By: **WatCrmark engineering resources**
www.watcrmark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (800) 374-1900

BMP O&M PLAN



Homer Glen Fire Truck
 Overall Length 36.833ft
 Overall Width 7.666ft
 Overall Body Height 21.5ft
 Min Body Ground Clearance 0.955ft
 Track Width 5.877ft
 Lock-to-lock time 5:00s
 Max Wheel Angle 40.00°



FIRE TRUCK CIRCULATION PLAN

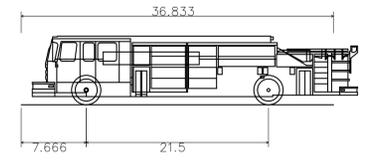
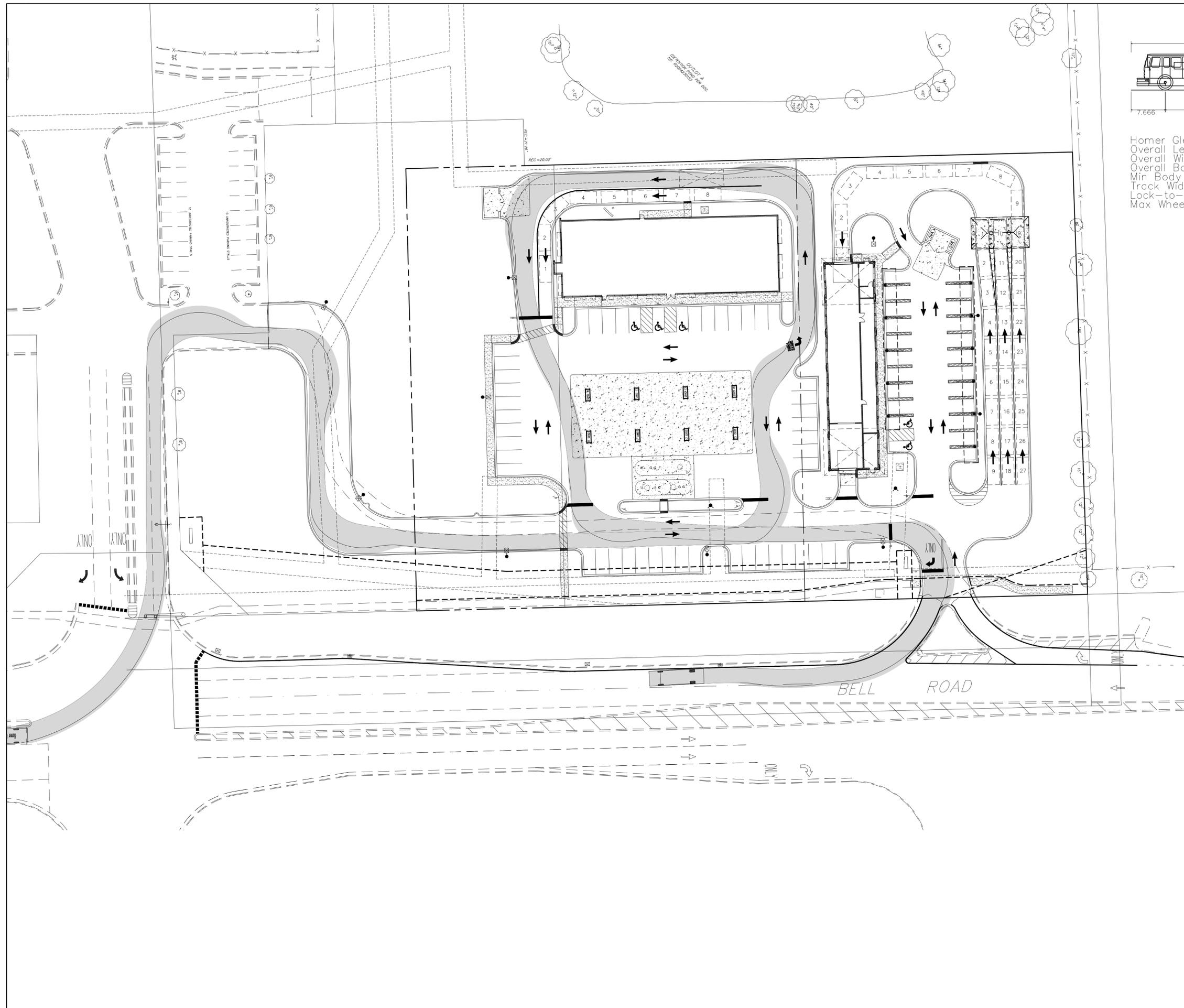
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6	ADDED NEW LAND AREA ON LOT 4	4/14/2023
7	ADDED TWO POLES FOR ACCESS DRIVE	5/6/2023
8	ADDED PROPERTY AND EXPANDED GAS STATION	6/9/2023
9	PER VILLAGE COMMENTS	8/2/2023
10	PER VILLAGE COMMENTS DATED 8/8/2023	8/13/2023

Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

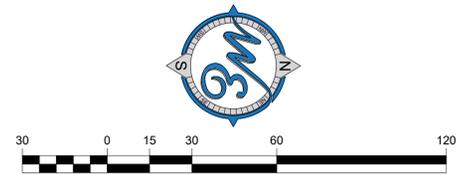
Prepared By:

Watermark Engineering Resources
 watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

CHECKED BY: J. MILLER
 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074



Homer Glen Fire Truck
 Overall Length 36.833ft
 Overall Width 7.666ft
 Overall Body Height 21.5ft
 Min Body Ground Clearance 0.955ft
 Track Width 5.877ft
 Lock-to-lock time 5:00s
 Max Wheel Angle 40.00°



FIRE TRUCK CIRCULATION PLAN

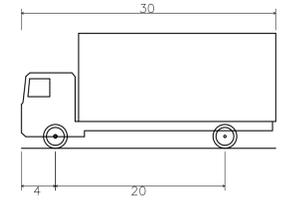
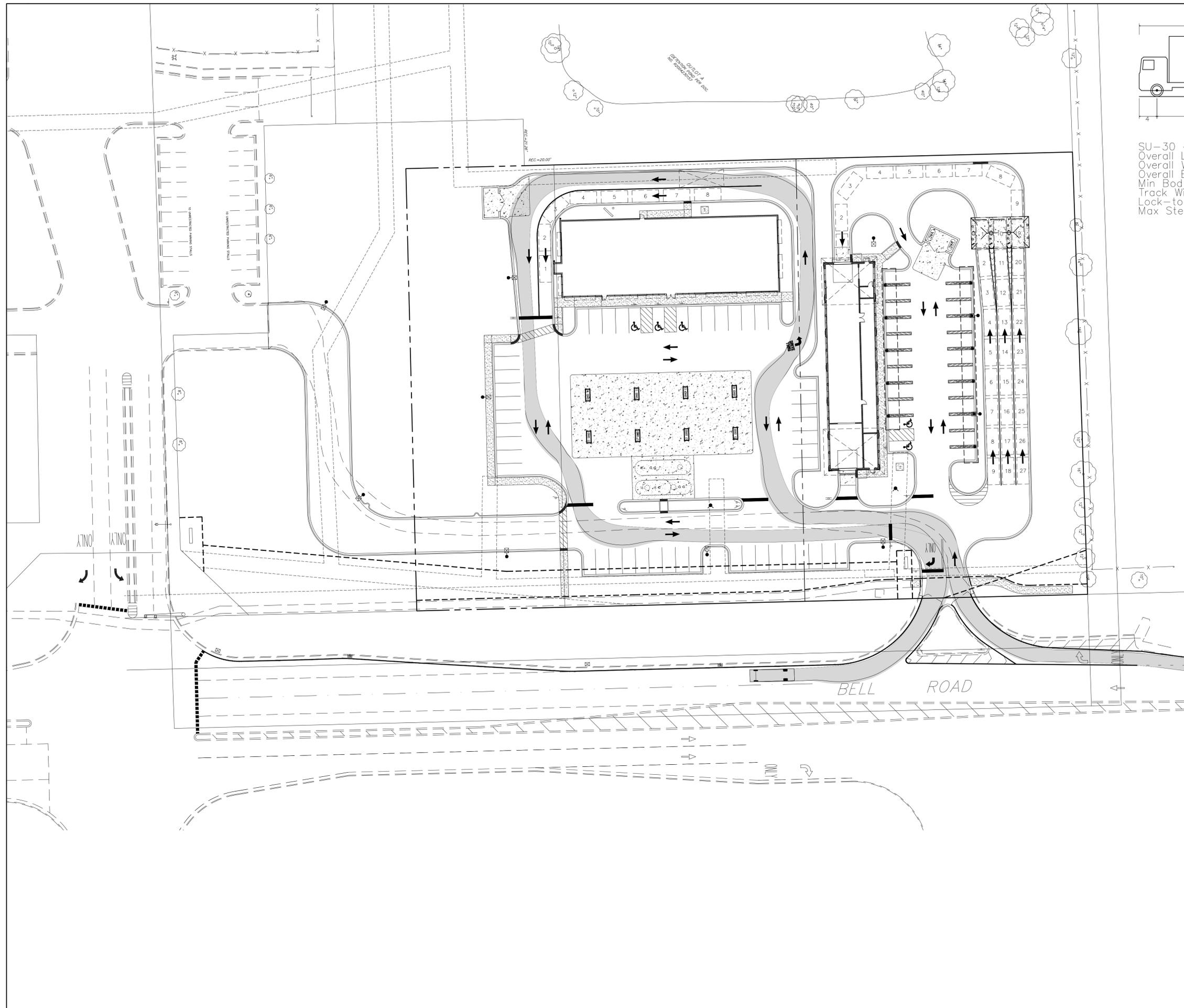
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9	PER VILLAGE COMMENTS	8/2/2023
10	PER VILLAGE COMMENTS DATED 8/8/2023	8/13/2023

Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

Prepared By:



CHECKED BY: J. MILLER
 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074
 watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800



SU-30 – Single Unit Truck
 Overall Length 30.000ft
 Overall Width 8.000ft
 Overall Body Height 13.500ft
 Min Body Ground Clearance 1.367ft
 Track Width 8.000ft
 Lock-to-lock time 5.00s
 Max Steering Angle (Virtual) 31.80°



DELIVERY TRUCK CIRCULATION PLAN

NO.	REVISIONS	DATE
1	PER VILLAGE AND CLIENT REVIEW	5/17/2022
2	PER CLIENT REVIEW	6/03/2022
3	PER VILLAGE COMMENTS	7/28/2022
4	PER CLIENT AND REVISIONS	10/21/2022
5	PER CLIENT AND REVISIONS	11/14/2022
6	ADDED NEW LAND AREA ON LOT 4	4/14/2023
7	ADDED TWO POLES FOR ACCESS DRIVE	5/6/2023
8	ADDED PROPERTY AND EXPANDED GAS STATION	6/9/2023
9	PER VILLAGE COMMENTS	8/2/2023
10	PER VILLAGE COMMENTS DATED 8/8/2023	8/13/2023

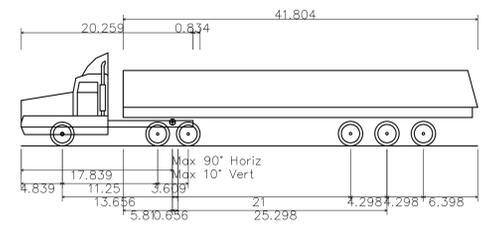
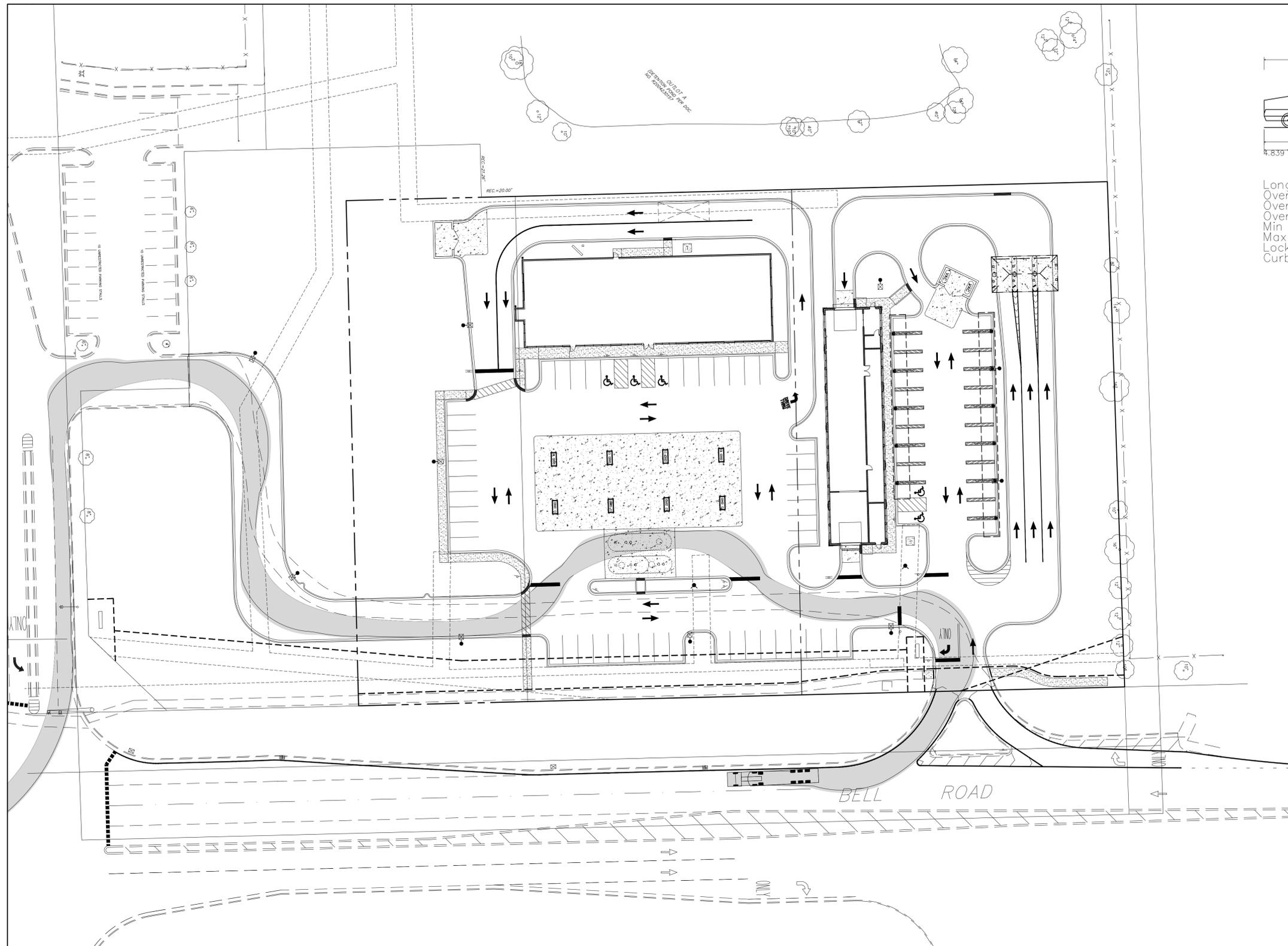
Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

Prepared By:



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CHECKED BY: J. MILLER
 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074



Long Nose Oil Tanker
 Overall Length 53.833ft
 Overall Width 8.202ft
 Overall Body Height 10.920ft
 Min Body Ground Clearance 1.329ft
 Max Track Width 8.202ft
 Lock-to-lock time 4.00s
 Curb to Curb Turning Radius 21.325ft



TANKER TRUCK CIRCULATION PLAN

NO.	REVISIONS	DATE
1	PER VILLAGE AND CLIENT REVIEW	5/17/2022
2	PER CLIENT REVIEW	6/03/2022
3	PER VILLAGE COMMENTS	7/26/2022
4	PER CLIENT AND REVISIONS	12/1/2022
5	ADDED NEW LAND AREA ON LOT 4	4/14/2023
6	ADDED TWO POLES FOR ACCESS DRIVE	5/6/2023
7	ADDED PROPERTY AND EXPANDED GAS STATION	6/9/2023
8	PER VILLAGE COMMENTS	8/2/2023
9		
10	PER VILLAGE COMMENTS DATED 8/2/2023	8/13/2023

Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

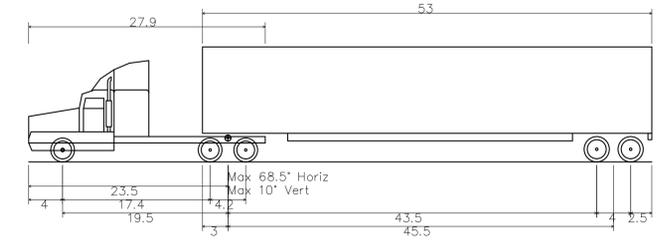
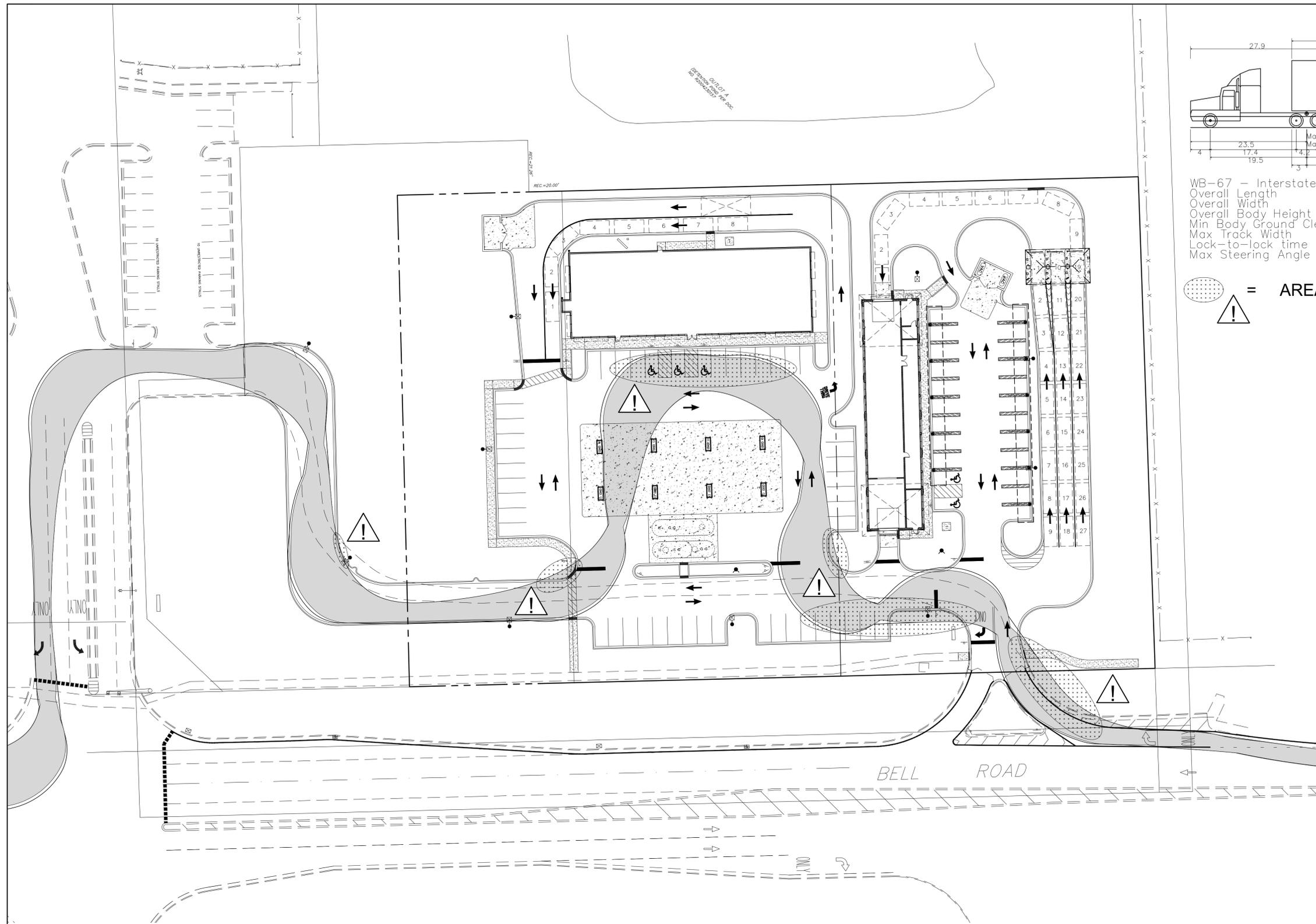
Prepared By:



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 DESIGN BY: J. MILLER
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 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074

1 of 1

TANKER TRUCK CIRCULATION PLAN



WB-67 - Interstate Semi-Trailer
 Overall Length 73.50ft
 Overall Width 8.500ft
 Overall Body Height 13.500ft
 Min Body Ground Clearance 1.334ft
 Max Track Width 8.500ft
 Lock-to-lock time 6.00s
 Max Steering Angle (Virtual) 28.40°

= AREAS OF TRUCK ENCROACHMENT



WB-67 CIRCULATION PLAN

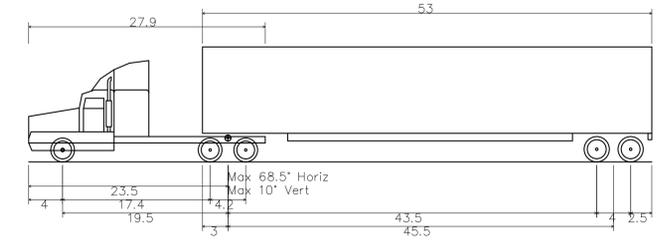
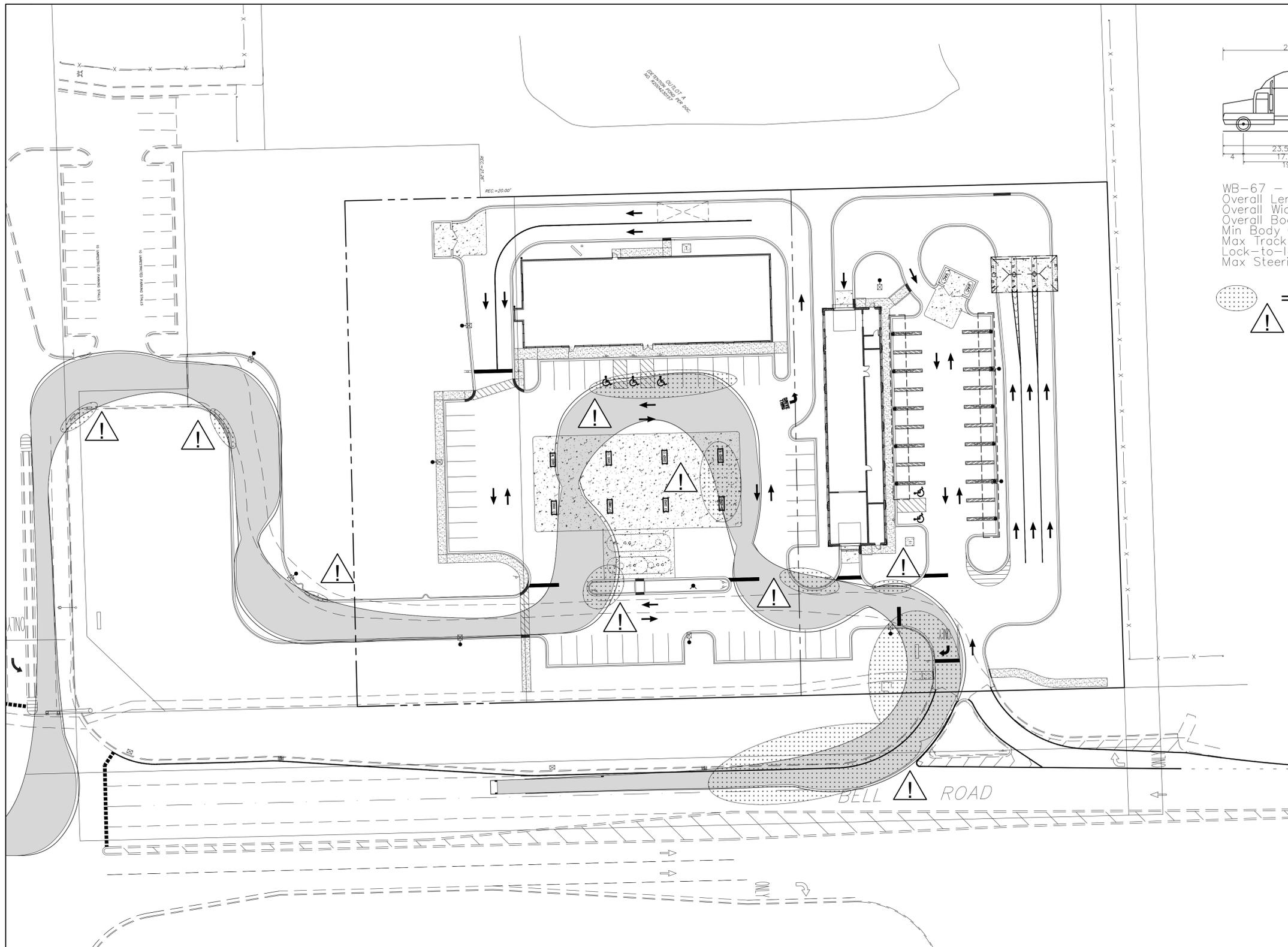
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8	ADDED PROPERTY AND EXPANDED GAS STATION	8/6/2023
9	PER VILLAGE COMMENTS	8/22/2023
10	PER VILLAGE COMMENTS DATED 8/22/2023	8/13/2023

Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

Prepared By:



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 DESIGN BY: J. MILLER
 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074



WB-67 - Interstate Semi-Trailer
 Overall Length 73.501ft
 Overall Width 8.500ft
 Overall Body Height 13.500ft
 Min Body Ground Clearance 1.334ft
 Max Track Width 8.500ft
 Lock-to-lock time 6.00s
 Max Steering Angle (Virtual) 28.40°

= AREAS OF TRUCK ENCROACHMENT

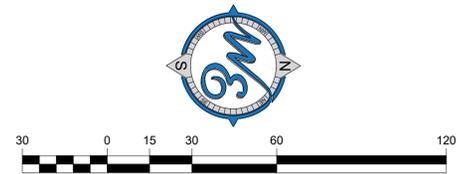
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Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
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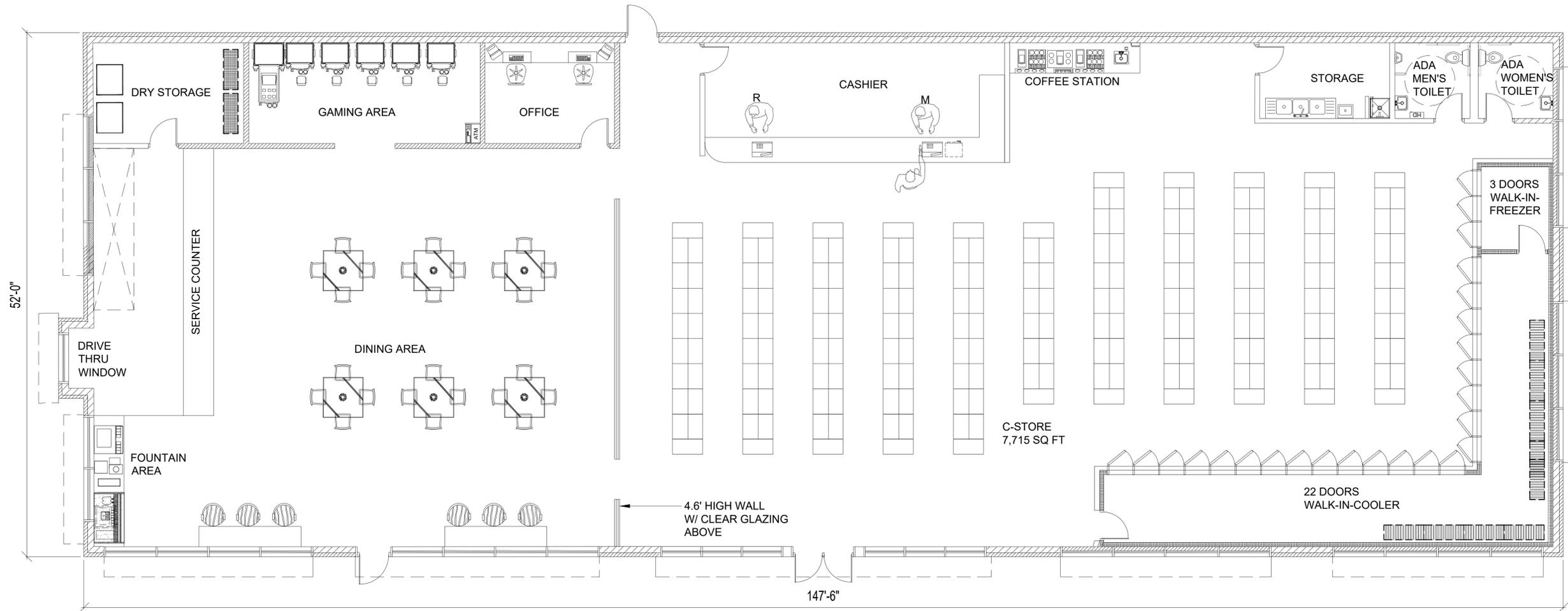
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 DRAWN BY: JOSH MILLER
 DATE: MARCH 7, 2022
 SCALE: 1" = 30'
 PROJECT NO.: 21-074



WB-67 CIRCULATION PLAN

Attachment 16 - Floor Plans

Square Footage of:
C-Store - 4,929.12 sf
Restaurant - 2,781.29 sf



1 PROPOSED C-STORE PLAN
SCALE: 3/16" = 1'-0" NORTH

Document Record:

Issued Date	Description
10.24.2021	PRELIM. DESIGN OWNER REVIEW
01.19.2022	PRELIM. DESIGN OWNER REVIEW
03.23.2022	PRELIM. DESIGN OWNER REVIEW
06.03.2022	STAFF / DESIGN REVISIONS
06.08.2022	STAFF REVISIONS
07.26.2022	STAFF REVISIONS
07.27.2022	STAFF REVISIONS
10.05.2022	MONUMENT SIGN REVISIONS
12.21.2022	SITE PLAN REVISIONS
07.26.2023	PROJECT REVISIONS

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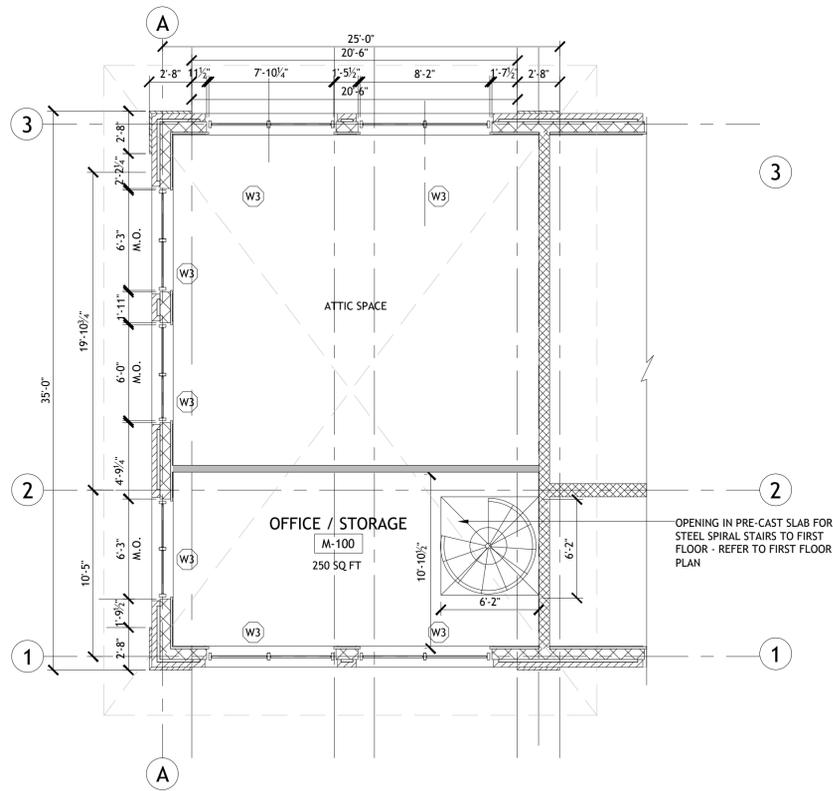
IPA
PUROHIT ARCHITECTS
251 BRADWELL ROAD,
INVERNESS, IL 60010
Tel. (847) 496-5322
Cell. (847) 942-8777
info@purohitarchitects.com
www.purohitarchitects.com

Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL

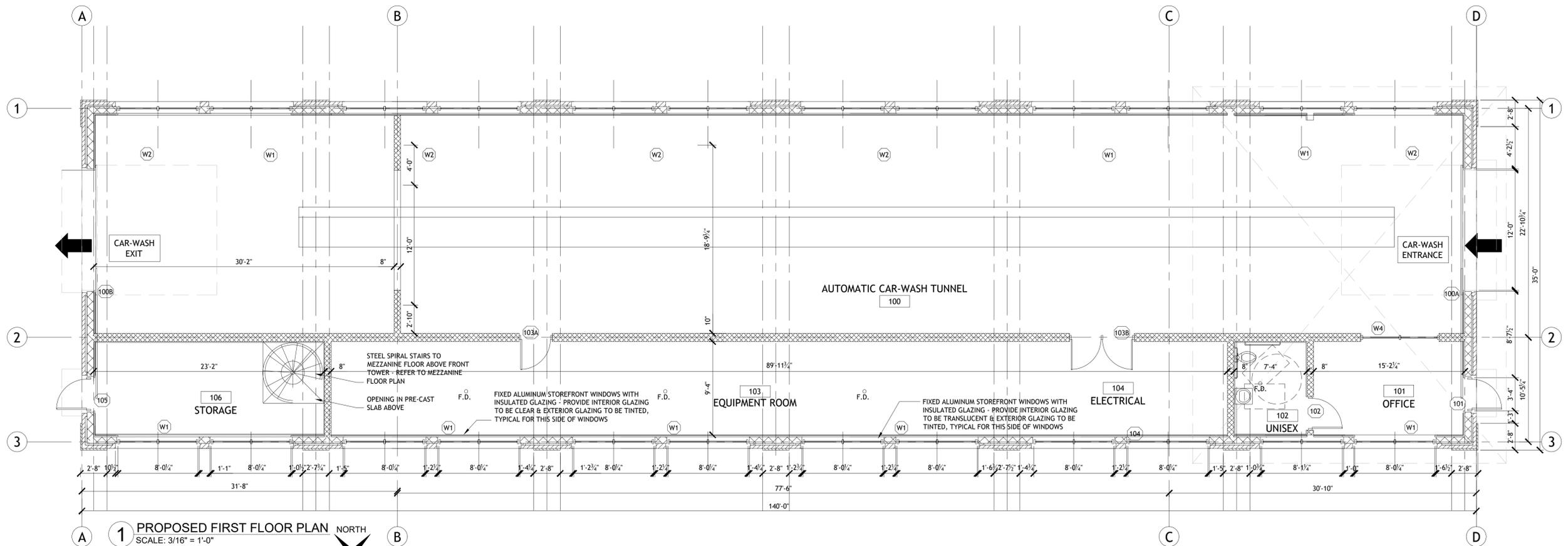
Issue Date: JULY 26, 2023
Drawn By: PAI

Sheet:
**PROPOSED C-STORE
FLOOR PLAN A-2**

PAI PROJECT NUMBER
21171.00



2 PROPOSED MEZZANINE FLOOR PLAN NORTH
SCALE: 3/16" = 1'-0"



1 PROPOSED FIRST FLOOR PLAN NORTH
SCALE: 3/16" = 1'-0"

Document Record:

Issued Date	Description
10.24.2021	PRELIM. DESIGN OWNER REVIEW
01.19.2022	PRELIM. DESIGN OWNER REVIEW
03.23.2022	PRELIM. DESIGN OWNER REVIEW
06.03.2022	STAFF / DESIGN REVISIONS
06.08.2022	STAFF REVISIONS
07.26.2022	STAFF REVISIONS
07.27.2022	STAFF REVISIONS
10.05.2022	MONUMENT SIGN REVISIONS
12.21.2022	SITE PLAN REVISIONS
07.26.2023	PROJECT REVISIONS

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PUROHIT ARCHITECTS
251 BRADWELL ROAD,
INVERNESS, IL 60010
Tel. (847) 496-5322
Cell. (847) 942-8777
info@purohitarchitects.com
www.purohitarchitects.com

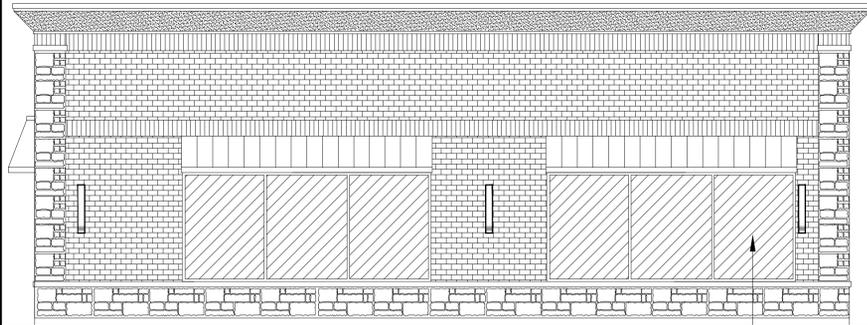
Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL

Issue Date: JULY 26, 2023
Drawn By: PAI

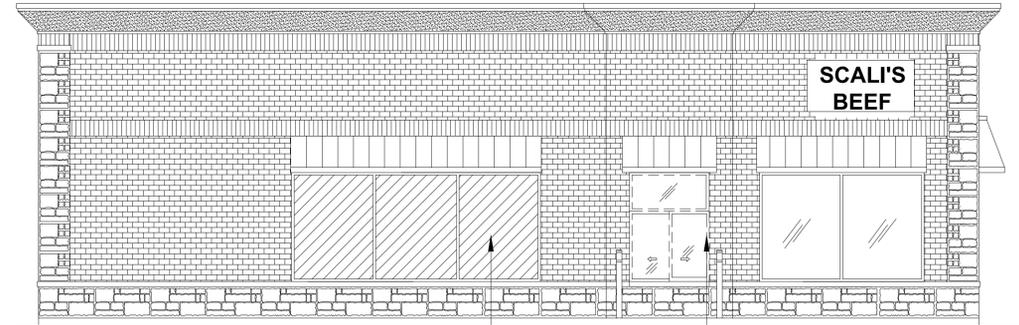
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PROPOSED CARWASH MEZZANINE & FIRST FLOOR A-3

PAI PROJECT NUMBER
21171.00

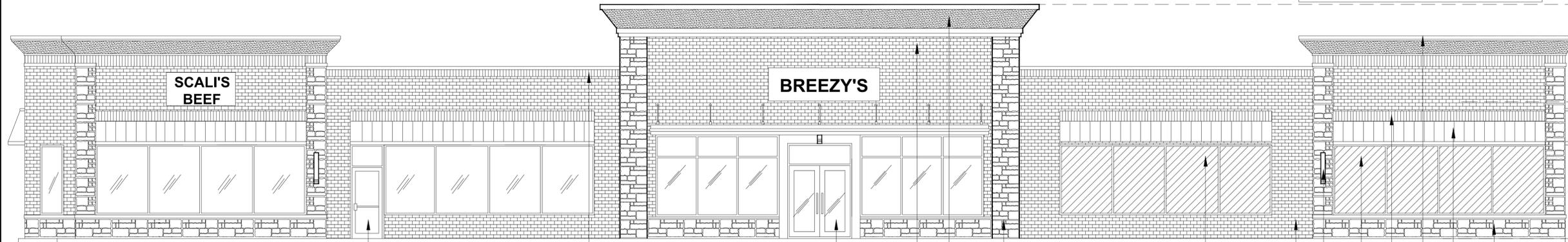
Attachment 17 - Proposed Elevations



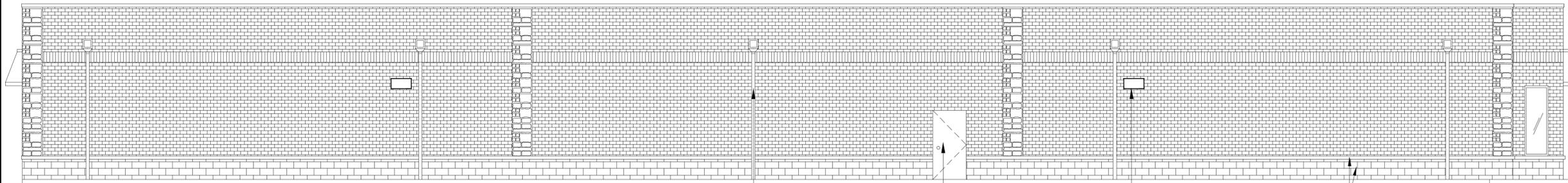
1 PROPOSED NORTH ELEVATION
SCALE: 3/16" = 1'-0"



2 PROPOSED SOUTH ELEVATION
SCALE: 3/16" = 1'-0"



3 PROPOSED EAST ELEVATION
SCALE: 3/16" = 1'-0"



4 PROPOSED WEST ELEVATION
SCALE: 3/16" = 1'-0"

- ELEVATION FINISH**
- 1 ALUMINUM GLASS DOOR AND FRAME GLAZING TO BE TEMPERED SAFETY GLAZING.
 - 2 JUMBO FACE BRICK VENEER.
 - 3 LIME STONE VENEER.
 - 4 JUMBO BRICK VENEER SOLDIER COURSE.
 - 5 ORNAMENTAL EIFS CROWN MOLDING.
 - 6 ORNAMENTAL STONE INSERT.
 - 7 METAL AWNING
 - 8 DECORATIVE DOWN LIGHT FIXTURE - REFER TO PHOTOMETRIC PLAN FOR SPECIFICATIONS.
 - 9 ORNAMENTAL EIFS TRIM BOARD W/ DECORATIVE BRACKET.
 - 10 6" PRE-FINISHED GALVANIZED METAL COPING (COLOR SELECTED BY OWNER)
 - 11 BRONZED ANODIZED ALUMINUM STORE FRONT SYSTEM W/ TEMPERED SAFETY GLAZING.
 - 12 WALL MOUNTED LIGHT FIXTURE - REFER TO PHOTOMETRIC PLAN FOR SPECIFICATIONS
 - 13 CAR WASH DOOR
 - 14 8" THICK CMU BLOCK WALL
 - 15 DRIVE THRU WINDOW
 - 16 DOWNSPOUT
 - 17 METAL DOOR
 - 18 DUMMY WINDOW
 - 19 CAST STONE BASE WITH 4" HIGH MASONRY CAP/BAND AS SHOWN, TYPICAL

- T.O. EIFS CROWN +23'-6"
- T.O. EIFS CROWN +20'-6"
- T. O. JOIST +14'-0"
- FFL LEVEL +0'-6"
- NEW ASPHALT ±0'-0"

Document Record:

Issued Date	Description
10.24.2021	PRELIM. DESIGN OWNER REVIEW
01.19.2022	PRELIM. DESIGN OWNER REVIEW
03.23.2022	PRELIM. DESIGN OWNER REVIEW
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07.26.2022	STAFF REVISIONS
07.27.2022	STAFF REVISIONS
10.05.2022	MONUMENT SIGN REVISIONS
12.21.2022	SITE PLAN REVISIONS
07.26.2023	PROJECT REVISIONS

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251 BRADWELL ROAD,
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Cell. (847) 942-8777
info@purohitarchitects.com
www.purohitarchitects.com

Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL

Issue Date: JULY 26, 2023
Drawn By: PAI

Sheet:
PROPOSED C-STORE ELEVATIONS A-4

PAI PROJECT NUMBER
21171.00

Document Record:

Issued Date	Description
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01.19.2022	PRELIM. DESIGN OWNER REVIEW
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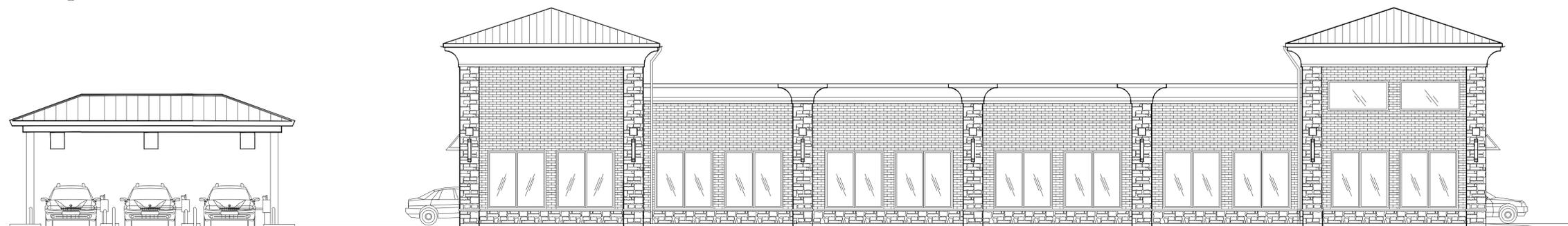


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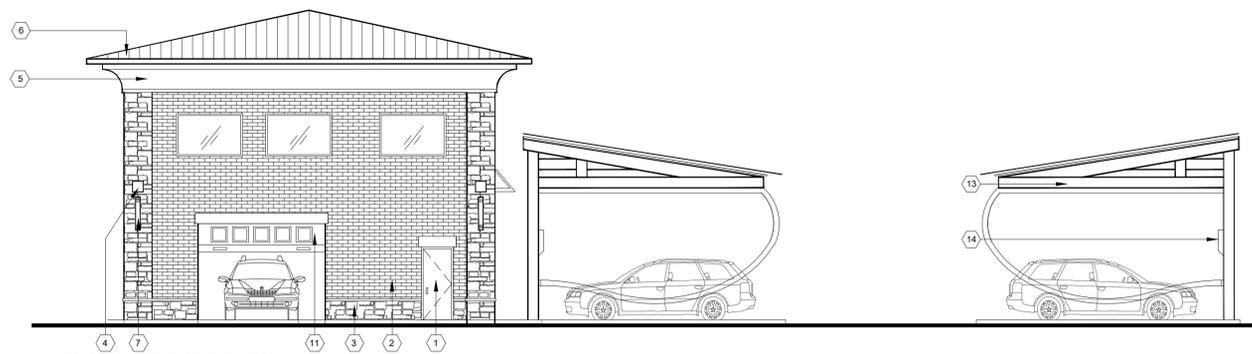
Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL



1 PROPOSED NORTH ELEVATION
SCALE: 1/8" = 1'-0"



2 PROPOSED SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



3 PROPOSED EAST ELEVATION
SCALE: 1/8" = 1'-0"



4 PROPOSED WEST ELEVATION
SCALE: 1/8" = 1'-0"

ELEVATION FINISH

- KEY NOTES:
- 1 HOLLOW METAL DOOR
 - 2 STONE CUT FINISH FACE BRICKS MODULAR SIZE
 - 3 LIME STONE VENEER.
 - 4 ORNAMENTAL STONE INSERT.
 - 5 STUCCO
 - 6 METAL ROOF
 - 7 UP & DOWN EXTERIOR LIGHT FIXTURE.
 - 8 6" PRE-FINISHED GLAZINED METAL COPING (COLOR SELECTED BY OWNER)
 - 9 BRONZED ANODIZED ALUMINUM STORE FRONT SYSTEM W/ TEMPERED SAFETY GLAZING.
 - 10 CAR WASH DOOR 10' X 12'
 - 11 METAL CANOPY
 - 12 VACUUM
 - 13 PREFINISHED SCUPPER AND DOWNSPOUT, MILL ALUMINUM

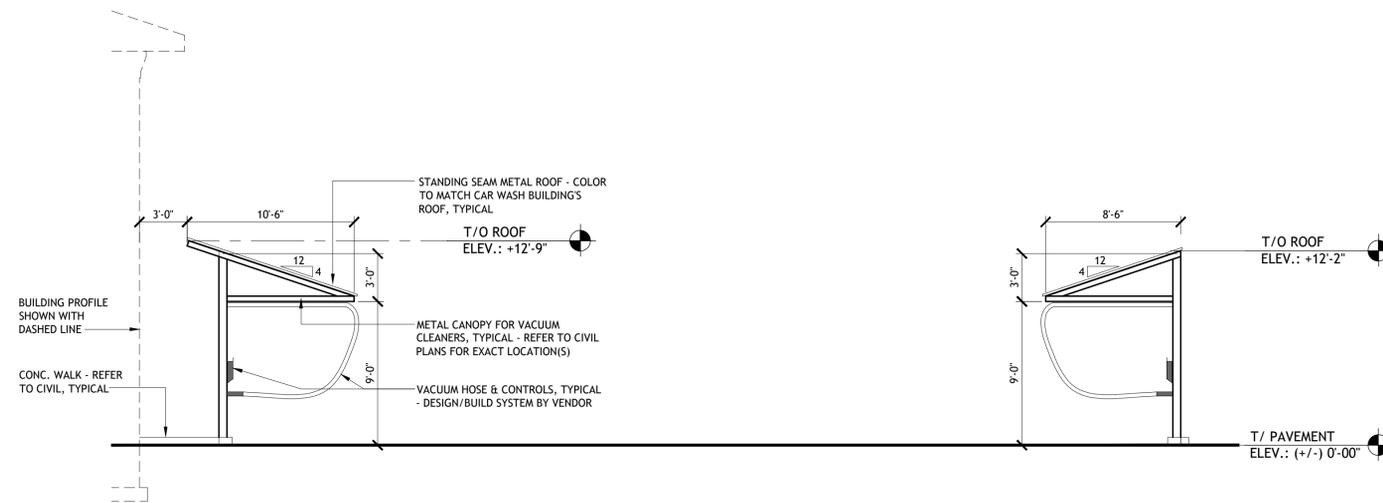
- T.O. TORRET +29'-6"
- T.O. PARAPET WALL +19'-0"
- T.O. JOIST +14'-0"
- F.F.L LEVEL +0'-0"
- NEW ASPHALT ±0'-0"

Issue Date: JULY 26, 2023
Drawn By: PAI

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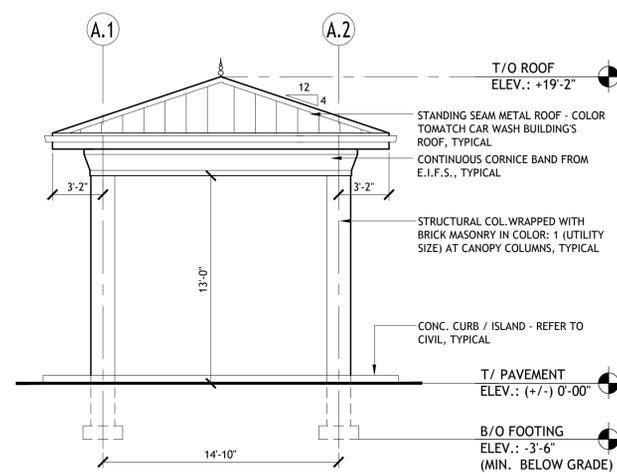
PROPOSED CARWASH ELEVATIONS A-5

PAI PROJECT NUMBER
21171.00

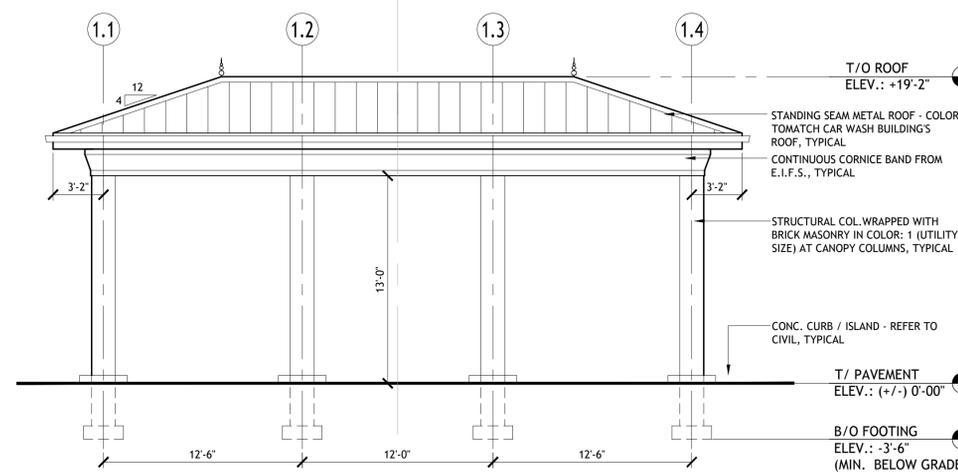


PROPOSED EAST ELEVATION - VACUUM CANOPIES
SCALE: 3/16" = 1'-0"

4



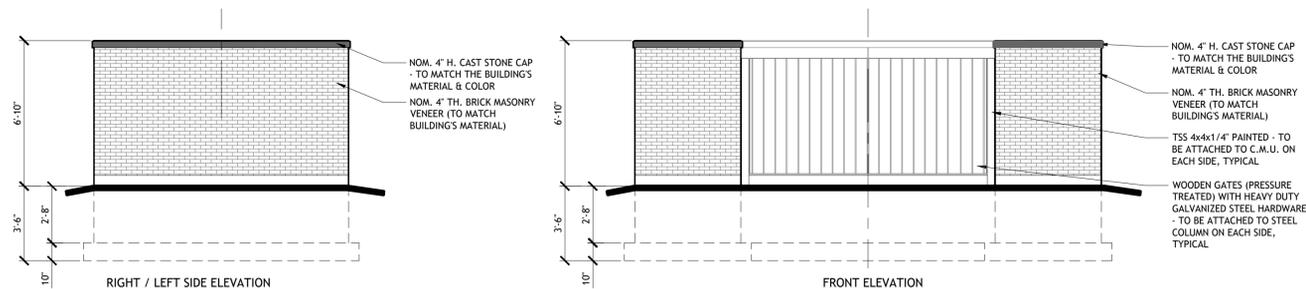
EAST ELEVATION
(WEST ELEVATION: MIRRORED)



NORTH ELEVATION
(SOUTH ELEVATION: MIRRORED)

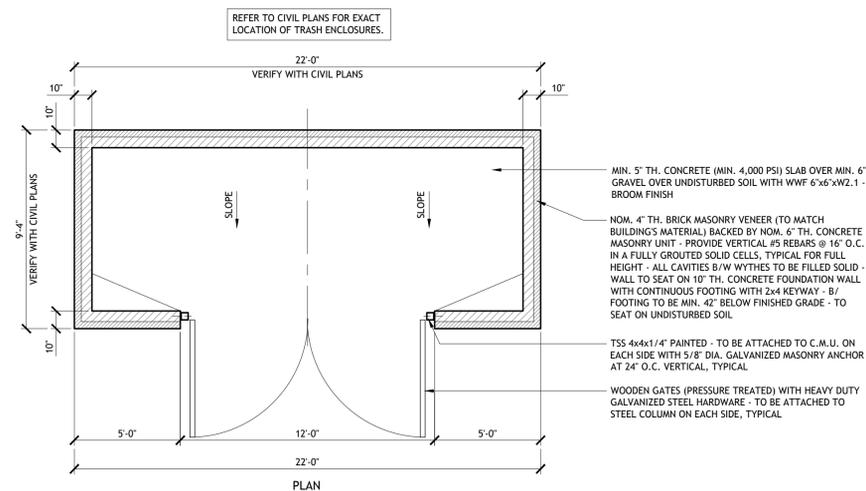
PROPOSED ELEVATIONS - PAY STATION CANOPY
SCALE: 3/16" = 1'-0"

3



PROPOSED TRASH ENCLOSURE ELEVATIONS
SCALE: 1/4" = 1'-0"

2



PROPOSED TRASH ENCLOSURE PLAN 1
SCALE: 1/4" = 1'-0"

1

Document Record:

Issued Date	Description
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Cell. (847) 942-8777
info@purohitarchitects.com
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Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL

Issue Date: JULY 26, 2023

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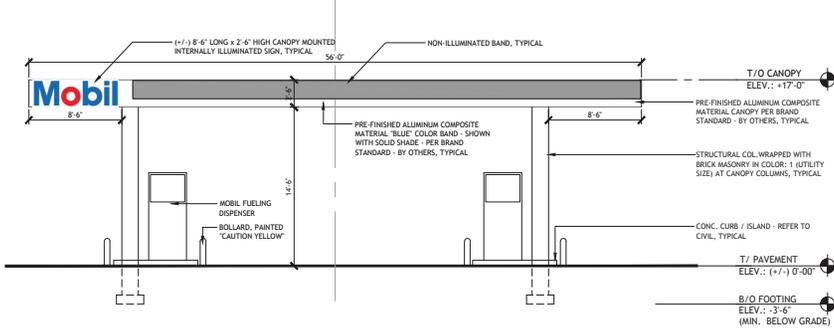
Sheet:

PROPOSED SITE DETAILS

A2.2a

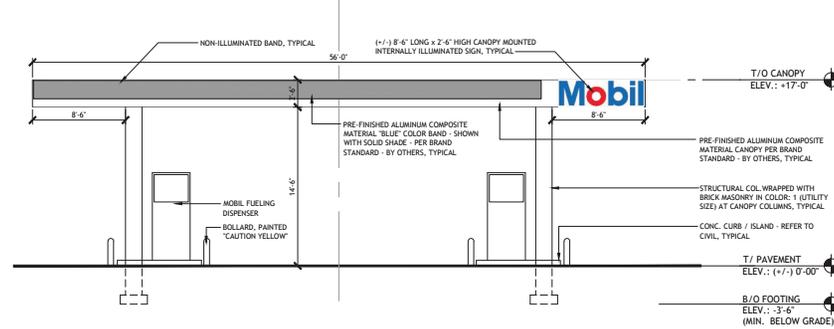
PAI PROJECT NUMBER
21171.00

Attachment 18 - Proposed Elevations, Gas Canopy



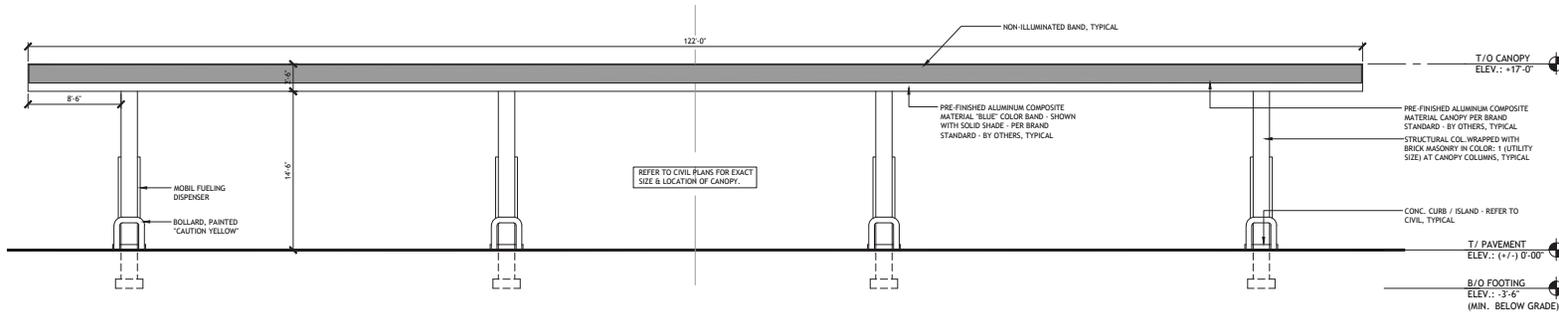
PROPOSED NORTH ELEVATION - GAS STATION CANOPY
SCALE: 3/16" = 1'-0"

3



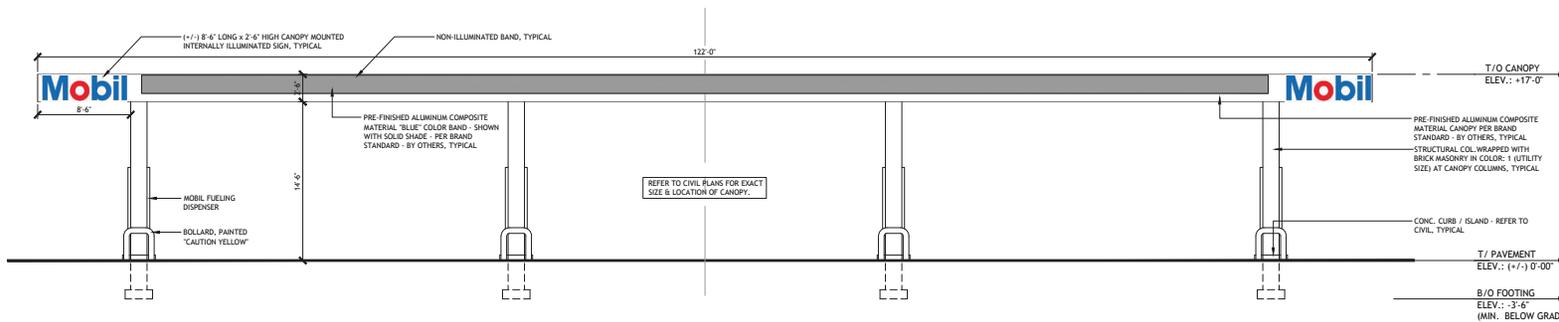
PROPOSED SOUTH ELEVATION - GAS STATION CANOPY
SCALE: 3/16" = 1'-0"

4



PROPOSED WEST ELEVATION - GAS STATION CANOPY
SCALE: 3/16" = 1'-0"

2



PROPOSED EAST ELEVATION - GAS STATION CANOPY
SCALE: 3/16" = 1'-0"

1

Document Record:

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01.19.2022	PRELIM DESIGN OWNER REVIEW
03.23.2022	PRELIM DESIGN OWNER REVIEW
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07.29.2022	STAFF REVISIONS
09.27.2022	STAFF REVISIONS
10.10.2022	MONUMENT SIGN REVISIONS
12.21.2022	SITE PLAN REVISIONS
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Proposed Schematic Site Plan for
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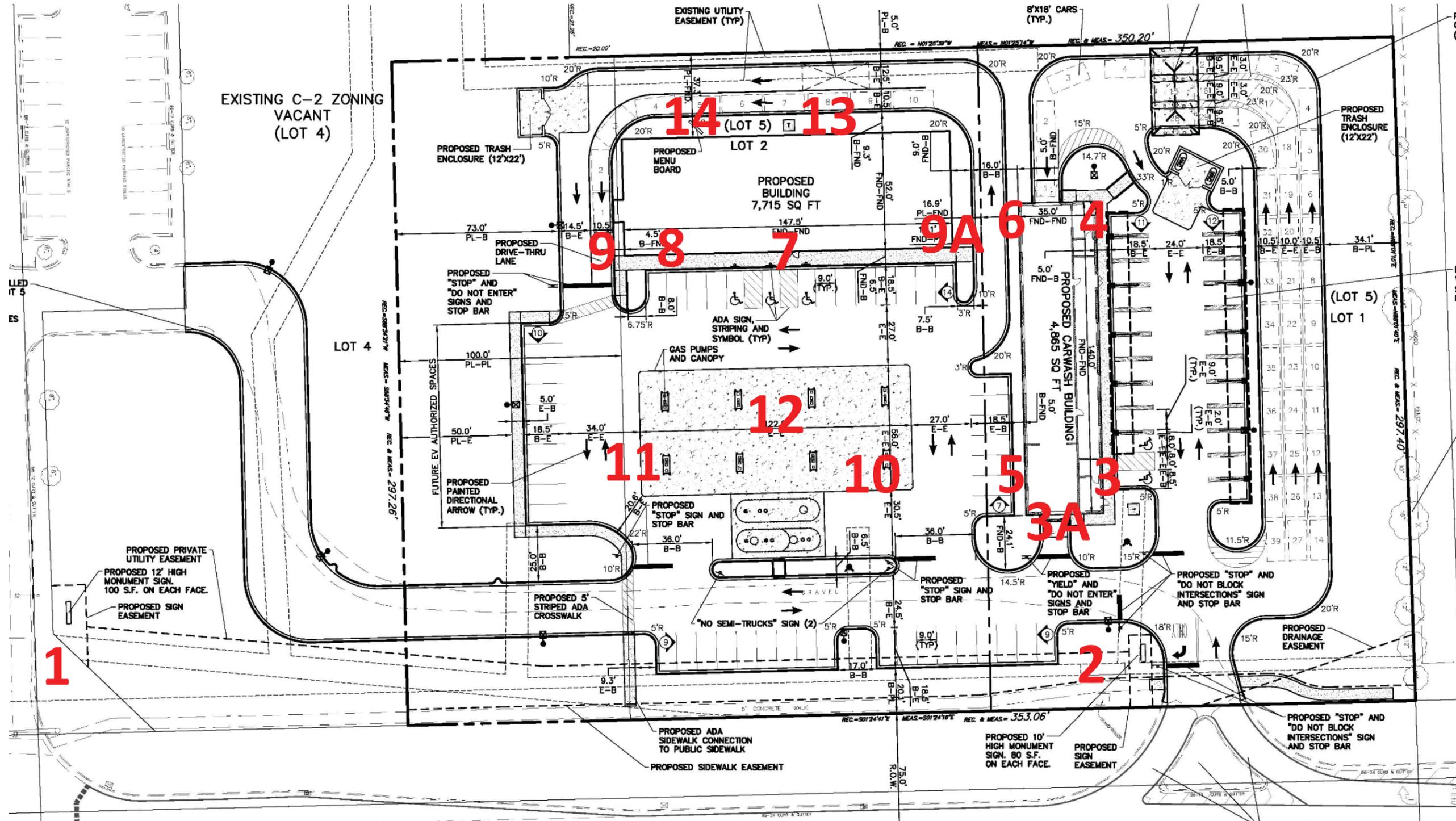
Issue Date: JAN 08, 2025
Drawn By: PAI

Sheet:
PROPOSED SITE DETAILS
A2.2b

PAI PROJECT NUMBER
21171.00

Attachment 19 - Proposed Sign Plan (provided by applicant)

SIGN PLAN LOT 5 (LOT 4) MENARDS SUBDIVISION



Document Record:

Issued Date	Description
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01.19.2022	PRELIM. DESIGN OWNER REVIEW
03.23.2022	PRELIM. DESIGN OWNER REVIEW
05.03.2022	STAFF / DESIGN REVISIONS
06.08.2022	STAFF REVISIONS
07.26.2022	STAFF REVISIONS
07.27.2022	STAFF REVISIONS
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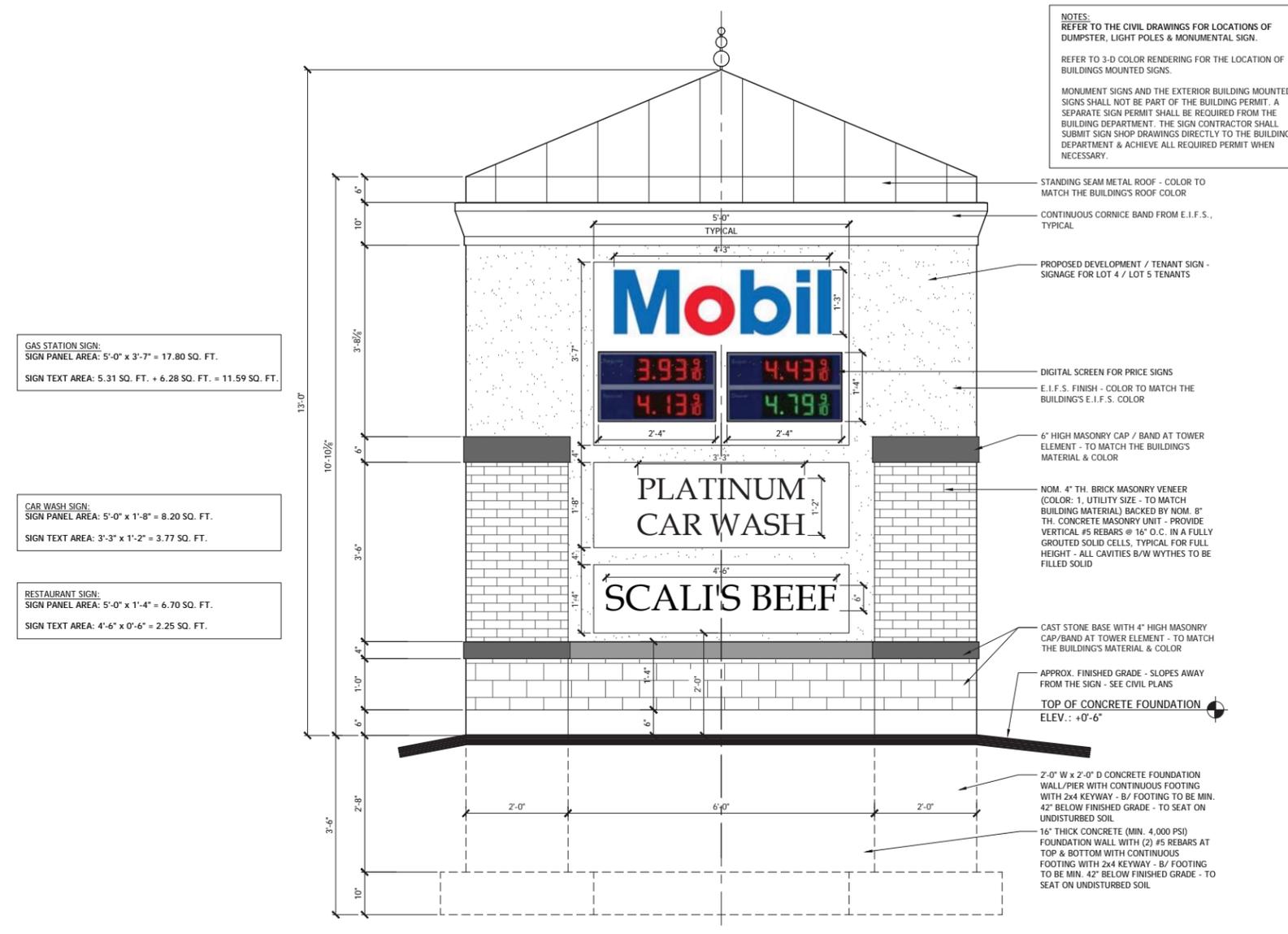
Proposed Schematic Site Plan for:
Mixed-Use Commercial Development
S. Bell Road & W. 143rd Street, Homer Glen, IL

Issue Date: JULY 26, 2023
Drawn By: PAI

Sheet:
PROPOSED
SIGN DETAILS

A2.1b

PAI PROJECT NUMBER
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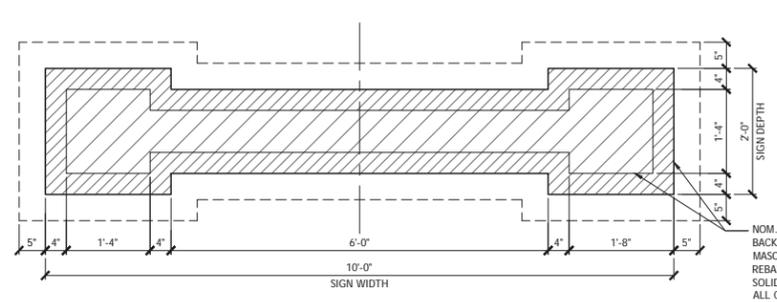


GAS STATION SIGN:
SIGN PANEL AREA: 5'-0" x 3'-7" = 17.80 SQ. FT.
SIGN TEXT AREA: 5.31 SQ. FT. + 6.28 SQ. FT. = 11.59 SQ. FT.

CAR WASH SIGN:
SIGN PANEL AREA: 5'-0" x 1'-8" = 8.20 SQ. FT.
SIGN TEXT AREA: 3'-3" x 1'-2" = 3.77 SQ. FT.

RESTAURANT SIGN:
SIGN PANEL AREA: 5'-0" x 1'-4" = 6.70 SQ. FT.
SIGN TEXT AREA: 4'-6" x 0'-6" = 2.25 SQ. FT.

LOT 4: PROPOSED MONUMENT SIGN - NORTH & SOUTH ELEVATION



LOT 4: PROPOSED MONUMENT SIGN - PLAN

LOT 4: PROPOSED MONUMENT SIGN - SOUTH ELEVATION
SCALE: 3/4" = 1'-0"

1

Document Record:

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Proposed Schematic Site Plan for:
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S. Bell Road & W. 143rd Street, Homer Glen, IL

Issue Date: JULY 26, 2023
Drawn By: PAI

Sheet:
PROPOSED
SIGN DETAILS

A2.1a

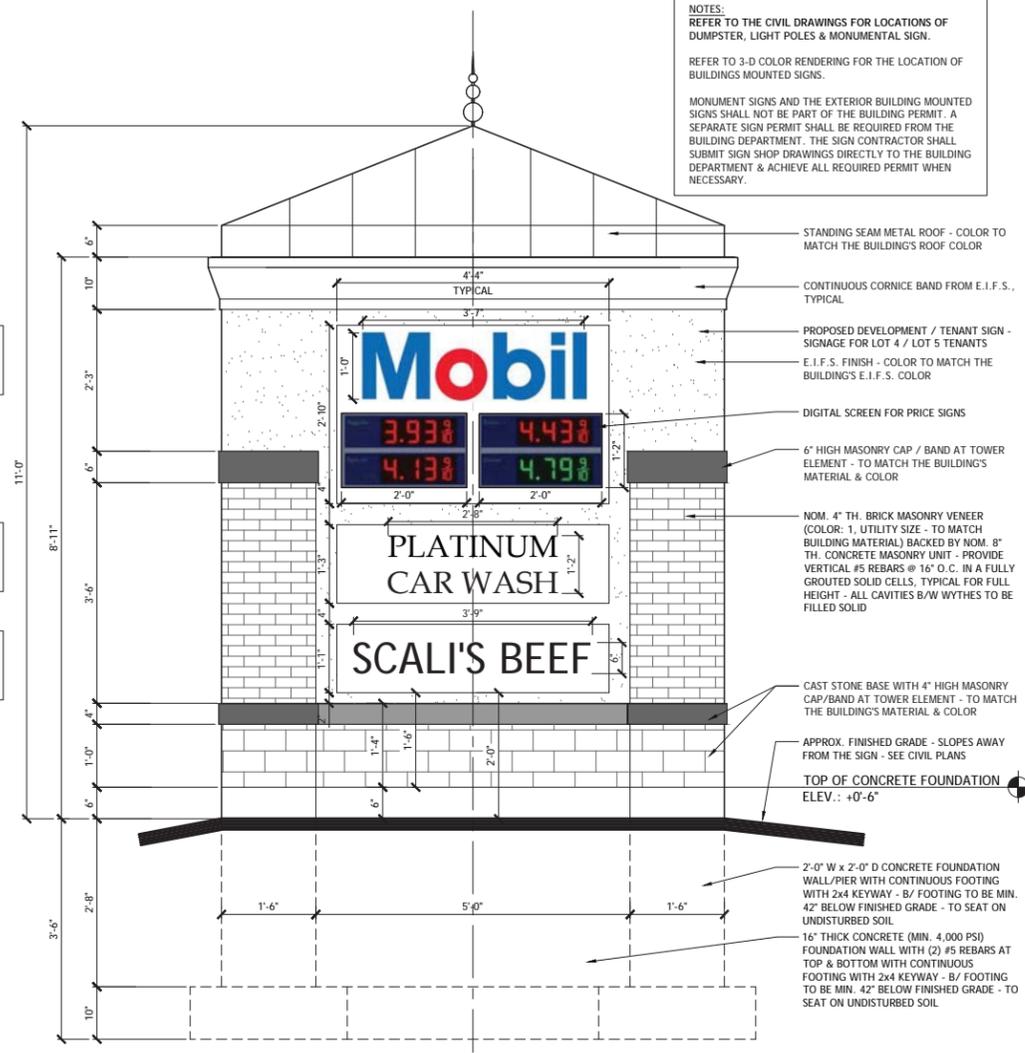
PAI PROJECT NUMBER
21171.00

NOTES:
REFER TO THE CIVIL DRAWINGS FOR LOCATIONS OF DUMPSTER, LIGHT POLES & MONUMENTAL SIGN.
REFER TO 3-D COLOR RENDERING FOR THE LOCATION OF BUILDINGS MOUNTED SIGNS.
MONUMENT SIGNS AND THE EXTERIOR BUILDING MOUNTED SIGNS SHALL NOT BE PART OF THE BUILDING PERMIT. A SEPARATE SIGN PERMIT SHALL BE REQUIRED FROM THE BUILDING DEPARTMENT. THE SIGN CONTRACTOR SHALL SUBMIT SIGN SHOP DRAWINGS DIRECTLY TO THE BUILDING DEPARTMENT & ACHIEVE ALL REQUIRED PERMIT WHEN NECESSARY.

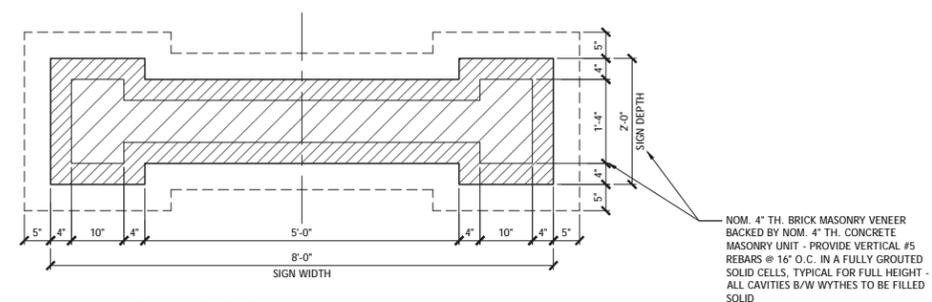
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SIGN TEXT AREA: 3.57 SQ. FT. + 4.64 SQ. FT. = 8.21 SQ. FT.

CAR WASH SIGN:
SIGN PANEL AREA: 4'-4" x 1'-3" = 5.42 SQ. FT.
SIGN TEXT AREA: 2'-8" x 1'-2" = 3.07 SQ. FT.

RESTAURANT SIGN:
SIGN PANEL AREA: 4'-4" x 1'-1" = 4.69 SQ. FT.
SIGN TEXT AREA: 3'-9" x 0'-6" = 1.87 SQ. FT.

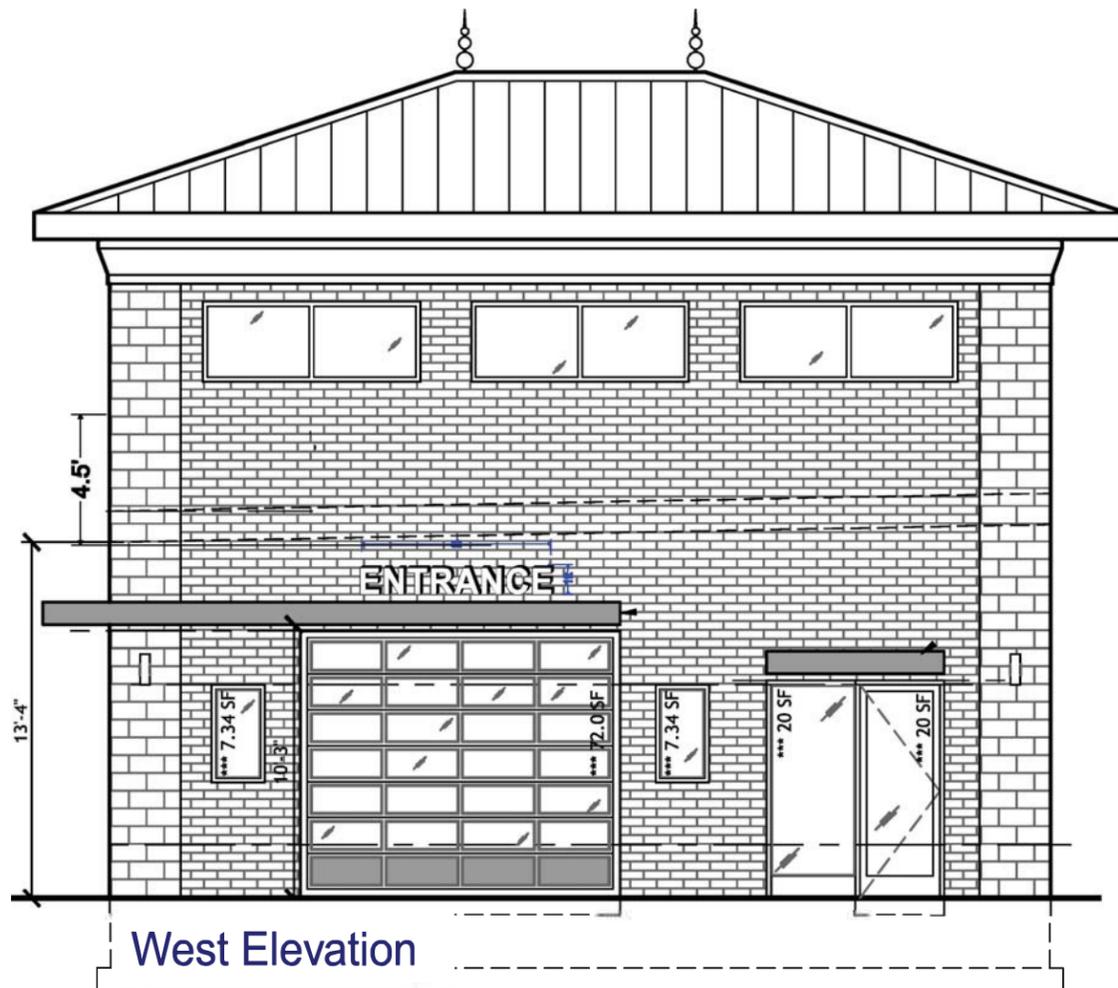


LOT 5: PROPOSED MONUMENT SIGN - NORTH & SOUTH ELEVATION



LOT 5: PROPOSED MONUMENT SIGN - PLAN

LOT 5: PROPOSED MONUMENT SIGN - NORTH & SOUTH ELEVATIONS
SCALE: 3/4" = 1'-0"



Signs
& Graphics Inc.
Whole Sale & Retail Interior / Exterior Signage

1-773-303-0161
5026 N. Broadway, Chicago, IL 60640

CUSTOMER: Platinum Car Wash
LOCATION: S Bell Rd & W 143rd St
Homer Glen, IL 60491

Design By: CN
Date: 06/05/2022

Customer approval signature

____ / ____ / 2022
Date

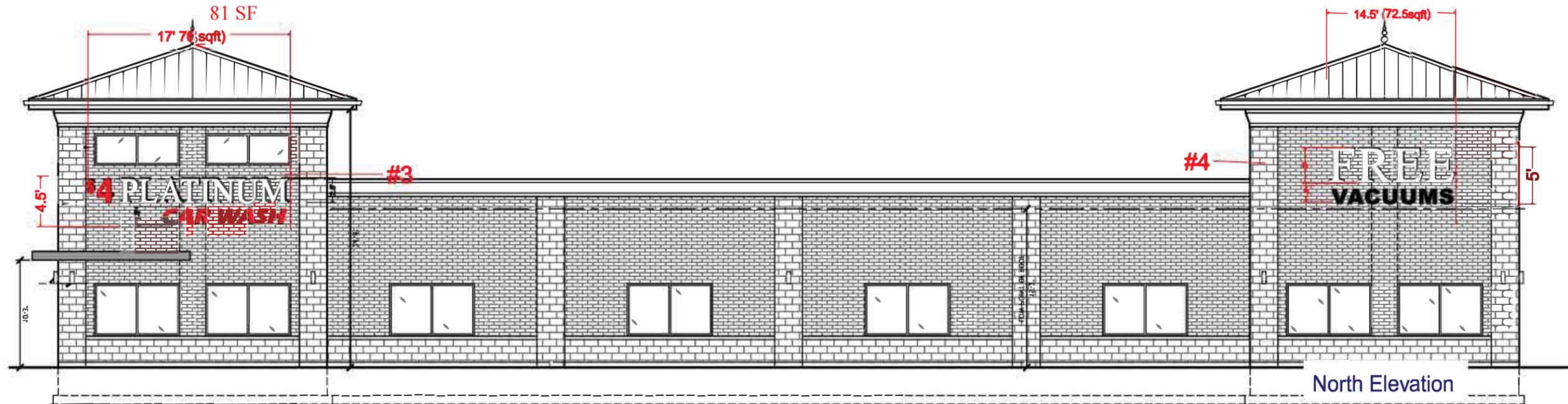
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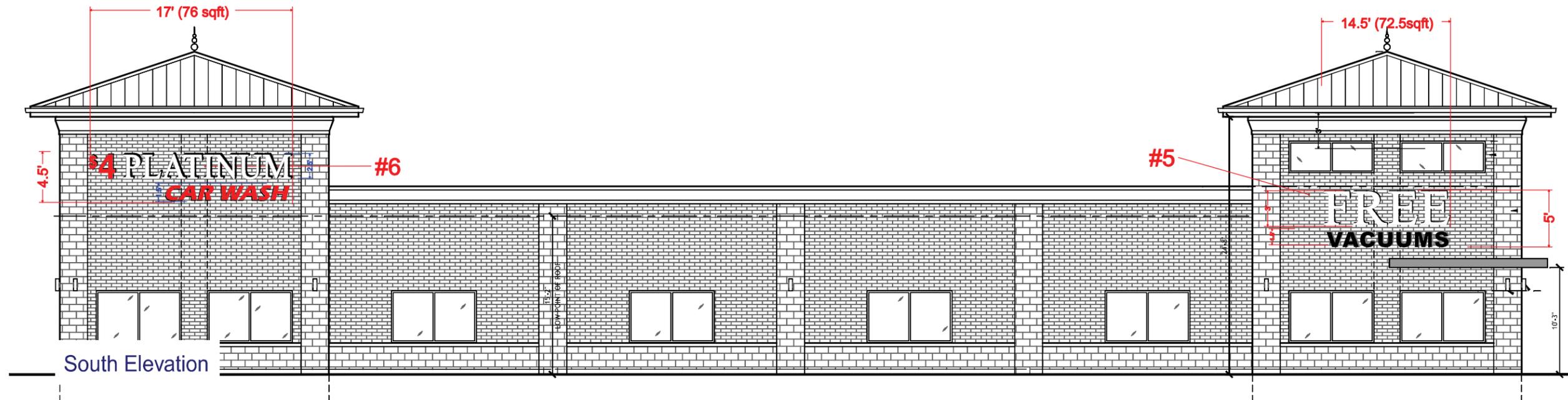
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South Elevation

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 & Graphics Inc.
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Design By: CN
 Date: 06/05/2022

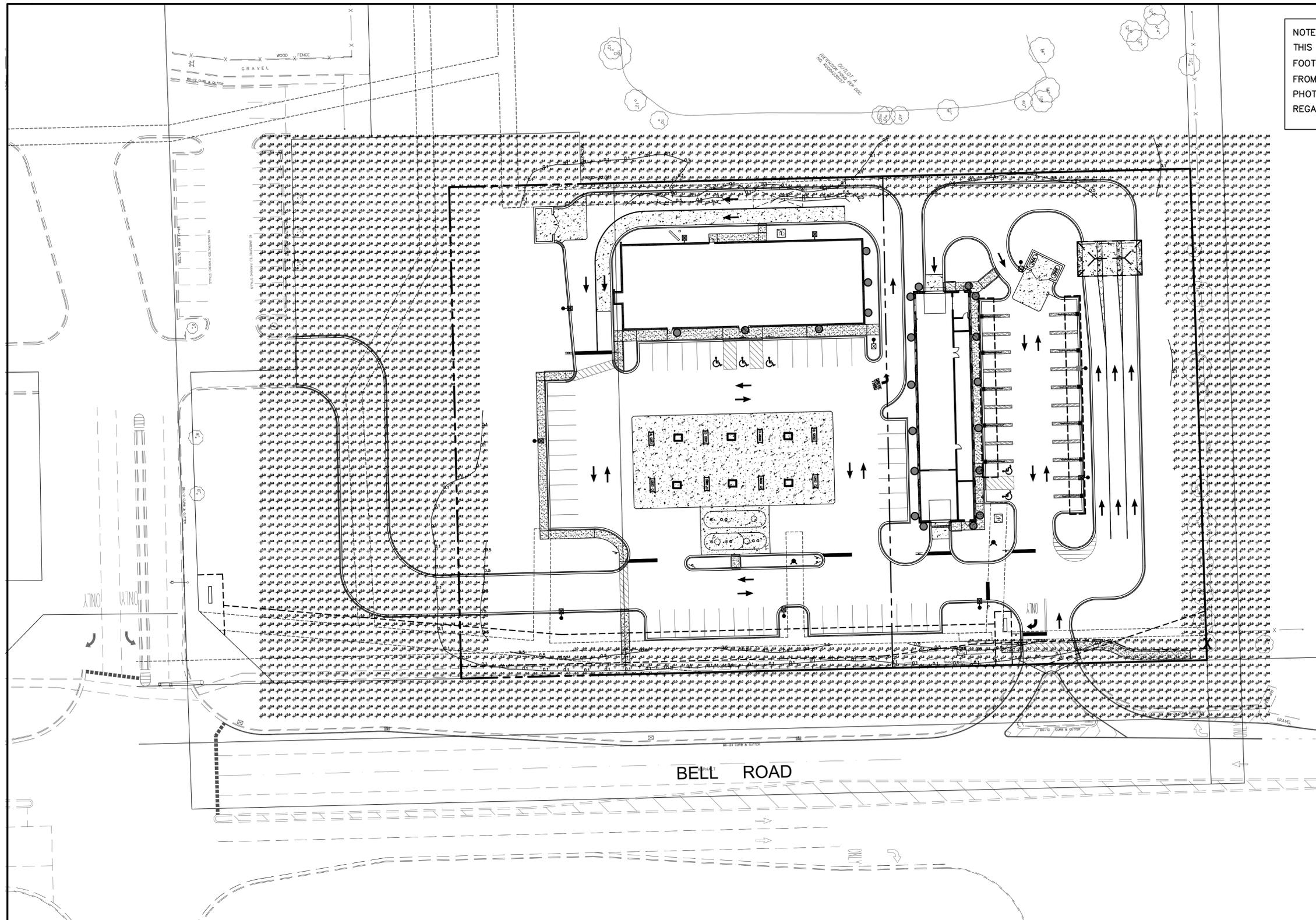
_____/_____/2022
 Customer approval signature Date

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Special Note: #5, #5A

_____/_____/2022
 Landlord approval signature Date



NOTE:
 THIS EXHIBIT HAS BEEN CREATED TO ILLUSTRATE
 FOOT-CANDLES NEAR THE PROPERTY LINE, MODELED 3'
 FROM THE GROUND PER VILLAGE CODE REQUIREMENTS. SEE
 PHOTOMETRIC PLAN 1 of 2 FOR DETAILED INFORMATION
 REGARDING LIGHT FIXTURES, POLES, HEIGHTS, ETC.

RECEIVED
 By Christopher Gruba at 9:42 am, Jan 07, 2026

**PHOTOMETRIC PLAN -
 PROPERTY LINE EXHIBIT**
 (W/O 2 OFF-SITE ACCESS ROAD LIGHTS)

NO.	REVISIONS	DATE
1	NONE	2/27/2024
2	NONE	1/12/2024
3	REMOVED 2 OFF-SITE ACCESS ROAD LIGHTS	0/10/2026

Prepared For:
NSS GROUP, INC.
 8060 Lawndale
 Skokie, IL 60077
HOMER GLEN DEVELOPMENT
 NWC of Bell Road and Glengary Drive - Lot 5
 Homer Glen, Illinois

Prepared By:

Watermark Engineering Resources
 watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

CHECKED BY: J. MILLER
 DESIGN BY: R. NELSON
 DRAWN BY: JOSH MILLER
 DATE: JANUARY 10, 2024
 SCALE: 1" = 30'
 PROJECT NO.: 21-074

2 of 2